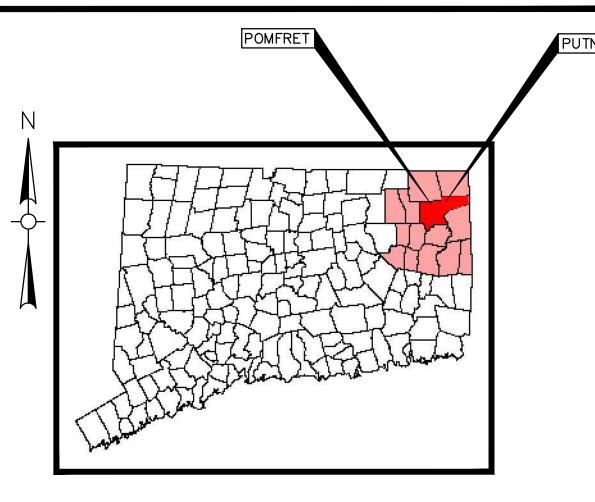
# AIR LINE TRAIL

# RECONSTRUCTION AND ACCESSIBILITY PROJECT A

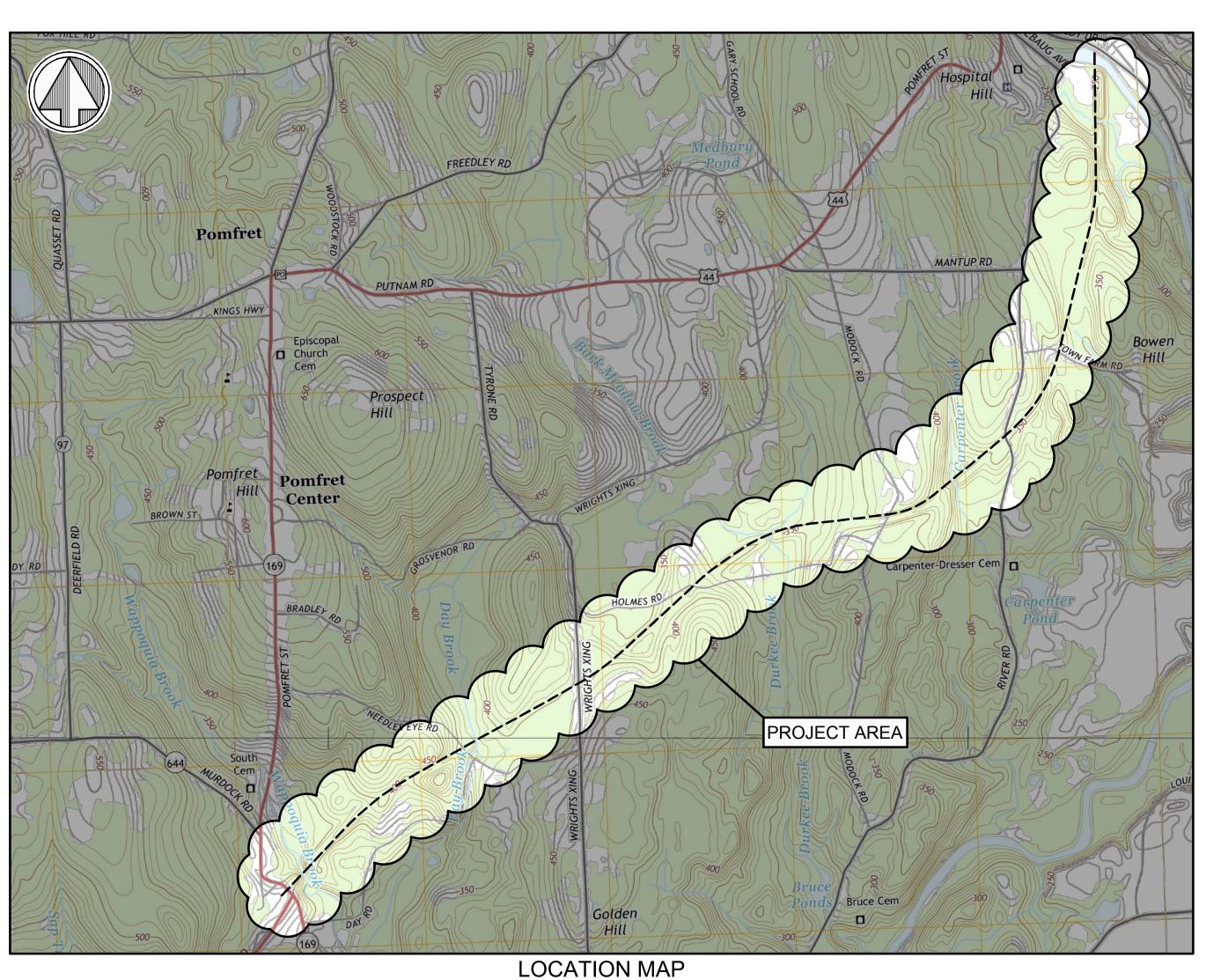
TAP PROJECT NO. 111-126

W&SPROJECT NO. ENG22-1336

TOWNS OF PUTNAM AND POMFRET, CT



NOT TO SCALE



# SHEET INDEX

LOC	1	INDEX PLAN
LOC	2	NOTES AND LEGEND
LOC	3	DETAILED ESTIMATE SHEET
L10	00 - L122	EXISTING CONDITIONS PLANS AND SURVEY TIES
L20	0	TRAIL TYPICAL SECTION
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L60	1-L611	PROFILE SHEETS
L70	0 - L703	BORING LOGS
L80	0-L810	CRITICAL CROSS SECTIONS

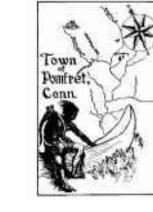
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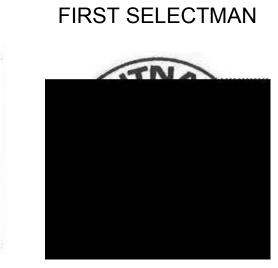
AUGUST 28, 2023
Prepared By



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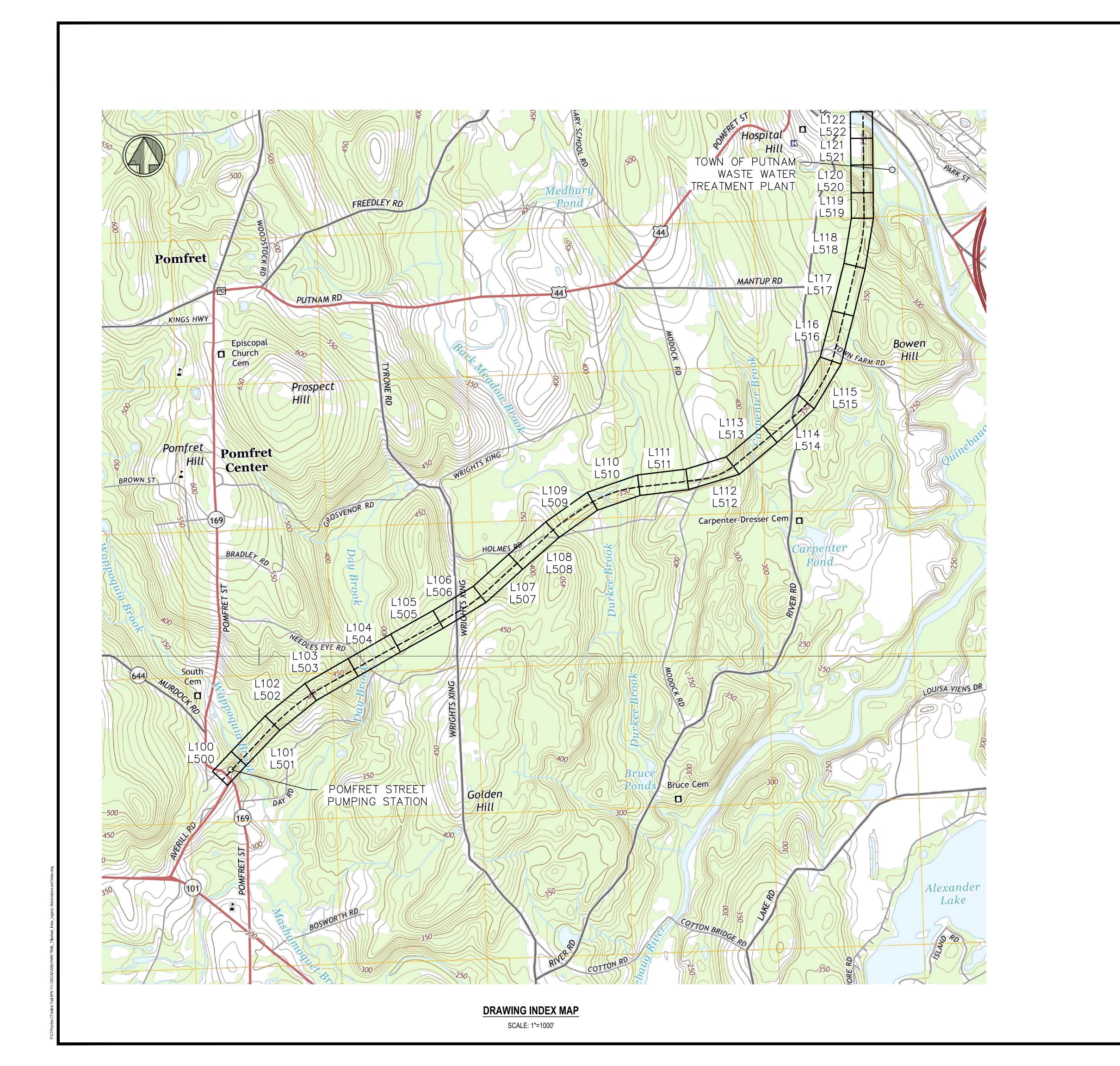




MAUREEN A. NICHOLSON

BARNEY SENEY MAYOR





AIR LINE TRAIL
RECONSTRUCTION AND
ACCESSIBILITY PROJECT
TOWNS OF PUTNAM AND
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Approved By: MJJ

W&S Project No.: ENG22-1336

W&S Project No.: ENG22-1336
W&S File No.: ----

Drawing Title:

INDEX PLAN

neet Number:

L001

LI	EGEND	
DESCRIPTION	EXISTING	PROPOSED
SANITARY SEWER		8"S PVC
FORCE MAIN		6"FM PVC
LOW PRESSURE FORCE MAIN	<u> </u>	6"LP_PVC
WATER MAIN		8"W DI
STORM DRAIN	<u> </u>	— 18"D RCP —
GAS		4"G
ELECTRIC		E
TELEPHONE		T
HOUSE CONNECTION	·	
WATER SERVICE		
GRINDER PUMP	○ GP	<b>⊕</b> GP
SANITARY SEWER MANHOLE	S	● SMH
STORM DRAIN MANHOLE	0	DMH
ELECTRICAL MANHOLE	- E	
TELEPHONE MANHOLE	$\Box$	<b>A</b> A <b>D</b> 1411
AIR RELEASE VALVE MANHOLE		● ARMH
FORCE MAIN CLEANOUT MANHOLE		● FMMH
CLEANOUT		<ul><li>CO</li></ul>
CATCH BASIN		
HYDRANT	X	<b>+</b>
TEMPORARY HYDRANT	<i>γ</i> γ◊	<del>Y</del> ⊕
	₩¥	
GATE VALVE		<b>M</b>
CHECK VALVE	<i>□</i>	<i>7</i>
BUTTERFLY VALVE	N	М
BALL VALVE	M	M
CURB STOP	4€	# <del>2</del> 0
REDUCER	₫	4
CAP OR PLUG	С	С
	gv ⊠	<u> </u>
GAS GATE VALVE		
UTILITY POLE	₽ P	
GUY POLE	Ø	
GUY WIRE		
OVERHEAD WIRES		
LIGHT POST	<b>*</b>	
EDGE OF PAVEMENT		
EDGE OF UNPAVED ROAD		
CURB		
SIDEWALK		Δ .Δ
STONE WALL		000000000
RETAINING WALL	RET WALL	RET WALL
WIRE FENCE	×	
CHAIN LINK FENCE		<u> </u>
DECIDUOUS TREE	$\bigcirc$	$\overline{\odot}$
EVERGREEN TREE	X. X	
		<b>ئ</b> ېد
TREE LINE	~~~~	$\sim$
SURVEY MARKER	□	
CONTROL POINT	Δ	
BENCH MARK	<b>⊕</b>	
PROPERTY LINE	<u> </u>	
EASEMENT LINE		
CONTOUR	56	56
SPOT ELEVATIONS	× 100.2	×101.5
BUILDING/BUILDING NUMBER	#35	
FLOOR ELEVATION	FF=56.7	
SILL ELEVATION	S=56.7	
LIMIT OF WORK		·
WETLAND/WETLAND FLAG	<u> </u>	
EDGE OF WATER		
DRAINAGE DITCH/SWALE		_ · · — · · -
RIP RAP	<del>\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\</del>	<del>*************************************</del>
BOLLARD	ΟВ	
SIGN	-0-	
GUIDE RAIL	1 1 1 1	1 1 1 1
PERCOLATION TEST	PT ⊕PT1	•
TEST PIT	TP	
BORING	⊕B1	<b>⊕</b> B4
PROBE	⊕P-1	<b>⊕</b> P−1
GROUNDWATER MONITORING WELL	⊕ WSE-1	<b>-</b> GW−1
HAY BALES	T	••••••
SILT FENCE	1.	<>
ROCK OUTCROP		
	• <u></u>	

#### **ABBREVIATIONS**

ASBESTOS CEMENT PIPE, TRANSITE ASPHALT COATED CORRUGATED METAL PIPE AMERICAN SOCIETY FOR TESTING AND MATERIALS BITUMINOUS CONCRETE BITUMINOUS BUILDING BENCH MARK C900 POLYVINYL CHLORIDE CABLE TELEVISION

CATCH BASIN CONCRETE CURB

ACCMP

ASTM

BIT

BLDG

C900

CATV

CMP

CNO

CONC

CTDOT

CU FT

DIA, Ø

DINV

DMH

ELEV

EOP

GALV

HDD

HDPE

HYD

CY

CAST IRON CENTERLINE CORRUGATED METAL PIPE COULD NOT OPEN

CONCRETE CONNECTICUT DEPARTMENT OF TRANSPORTATION CUBIC FEET CUBIC YARD

STORM DRAIN DROP INLET, DUCTILE IRON DIAMETER DEPTH TO INVERT OF EXISTING PIPE DRA**I**N MANHOLE

ELECTRIC, EAST EACH **ELEVATION EDGE OF PAVEMENT** EACH WAY EXISTING FORCE MAIN

FEET, FOOT NATURAL GAS GALVANIZED **GRANITE CURB** GAS MAIN GAS SERVICE HOUSE CONNECTION

HORIZONTAL DIRECTIONAL DRILLING HIGH DENSITY POLYETHYLENE PIPE HIGH PRESSURE FIRE HYDRANT INVERT INSIDE DIAMETER IRON PIPE

LINEAR FEET LUMP SUM MAXIMUM MAIL BOX MECH MECHANICAL MANHOLE MINIMUM MISCELLANEOUS

MECHANICAL JOINT MERIDEN-WATERBURY TURNPIKE NORTH NOT FOUND NO OR# NUMBER POLYETHYLENE PROPERTY LINE POLYVINYL CHLORIDE PROPOSED PVMT PAVEMENT REINFORCED CONCRETE ROW RIGHT-OF-WAY SEWER, SOUTH

SEWER COMBINATION AIR VALVE STRUCTURE SERV SERVICE SQUARE FEET SEWER MAIN SPECIFICATIONS SQ FT

SQUARE FEET SEWER SERVICE STATION SIDEWALK TELEPHONE TEMPORARY BENCH MARK TURNPIKE TRAFFIC, TRAFFIC CONDUIT

TYPICAL UTILITY POLE VITRIFIED CLAY VERTICAL WATER, WEST WATER MAIN WATER SERVICE

WM

#### **CONSTRUCTION NOTES:**

- 1. THE CONTRACTOR SHALL CONTACT "CALL BEFORE YOU DIG" (CBYD) AT 1-800-922-4455 AT LEAST 72 HOURS, SATURDAYS, SUNDAYS, AND HOLIDAYS EXCLUDED, PRIOR TO EXCAVATING AT ANY LOCATION. A COPY OF THE CBYD PROJECT REFERENCE NUMBER(S) SHALL BE GIVEN TO THE OWNER PRIOR TO EXCAVATION.
- 2. LOCATIONS OF EXISTING PIPES, CONDUITS, UTILITIES, FOUNDATIONS AND OTHER UNDERGROUND OBJECTS ARE NOT WARRANTED TO BE CORRECT AND THE CONTRACTOR SHALL HAVE NO CLAIM ON THAT ACCOUNT SHOULD THEY BE OTHER
- 3. TEST PITS TO LOCATE EXISTING UTILITIES MAY BE REQUIRED BY THE ENGINEER.
- 4. ALL PAVEMENT OUTSIDE OF THE LIMITS SHOWN WHICH IS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED IN ACCORDANCE WITH THE SPECIFICATIONS AND AS SHOWN ON THE DRAWINGS.
- 5. ALL AREAS DISTURBED BY THE CONTRACTOR BEYOND PAYMENT LIMITS SHALL BE RESTORED AT NO ADDITIONAL COST TO
- 6. THE CONTRACTOR SHALL MAINTAIN SIDE SLOPES AND DRAINAGE SWALES DURING CONSTRUCTION TO PREVENT PONDING
- 7. THE CONTRACTOR SHALL NOT STORE ANY APPARATUS, MATERIALS, SUPPLIES, AND EQUIPMENT ON DRAINAGE STRUCTURES OR WITHIN 100 FEET OF WETLANDS.
- 8. THE CONTRACTOR SHALL INSTALL THE EROSION CONTROL MEASURES BEFORE BEGINNING OTHER WORK ON SITE.
- 9. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET PROTECTION ON ALL EXISTING CATCH BASINS THROUGHOUT THE DURATION OF THE PROJECT.
- 10. THE CONTRACTOR SHALL NOTIFY WATER AND SEWER SERVICE PROVIDERS AT LEAST 72 HOURS PRIOR TO EXCAVATING NEAR SEWER AND WATERLINES.
- 11. ALL STREET EXCAVATIONS SHALL BE COMPLETELY CLOSED AT THE END OF EACH WORKING DAY BY BACKFILLING. COVERING WITH STEEL PLATES MAY BE ALLOWED IF APPROVED BY THE ENGINEER AND CT DOT.
- 12. DURING THE PROCESS OF WORK, THE CONTRACTOR SHALL CONDUCT OPERATIONS AND MAINTAIN THE AREA OF CONSTRUCTION ACTIVITIES, INCLUDING SWEEPING AND SPRINKLING OF STREETS AS NECESSARY OR AS REQUIRED, TO MINIMIZE CREATION AND DISPERSION OF DUST.
- 13. A TRAFFIC CONTROL PLAN SHALL BE FOLLOWED AS STATED IN THE SPECIFICATIONS. SIGNAGE SHALL BE PROVIDED AS NECESSARY OR AS REQUIRED.
- 14. WHERE EXISTING FENCES ARE TO BE REMOVED AND RESET, A TEMPORARY CONSTRUCTION FENCE SHALL BE ERECTED AFTER REMOVAL FOR THE PROTECTION OF THE PUBLIC.
- 15. ALL DOT HIGHWAY LINE MONUMENTATION AND PRIVATE PROPERTY MONUMENTATION WITHIN THE PROJECT LIMITS MUST BE DELINEATED AND PROTECTED FROM DAMAGE OR MOVEMENT. ANY COST ASSOCIATED WITH RESETTING OF THE MONUMENTATION SHALL BE AT THE CONTRACTOR'S EXPENSE.
- 16. THE CONTRACTOR SHALL COMPLETE ALL LAYOUTS, SURVEYS, ETC. REQUIRED FOR CONSTRUCTION OF THE PROJECT AS SHOWN AND AS SPECIFIED.
- 17. ALL WORK PERFORMED WITHIN STATE HIGHWAY LIMITS SHALL CONFORM TO THE PERMIT ISSUED FOR THIS PROJECT.
- 18. LIMITS OF CLEARING SHALL BE FIELD LOCATED WITH OWNERS AND ENGINEER PRIOR TO CONSTRUCTION.
- 19. HISTORICAL RAILROAD MONUMENTS MAY BE ENCOUNTERED DURING CONSTRUCTION. ALL MONUMENTS SHALL BE LOCATED AND ADEQUATELY PROTECTED, IF AN EXISTING MONUMENT CONFLICTS WITH THE PROPOSED WORK IT SHALL BE REMOVED AND THE MONUMENT SHALL BE RESET BY A LICENSED SURVEYOR.

TAP PROJECT NO. 111-126

ACCESSIBILITY PROJECT TOWNS OF PUTNAM AND POMFRET, CT

AIR LINE TRAIL

RECONSTRUCTION AND

Weston & Sampson Engineers, Inc. 712 Brook Street, Suite 103 Rocky Hill, CT 06067 978.532.1900

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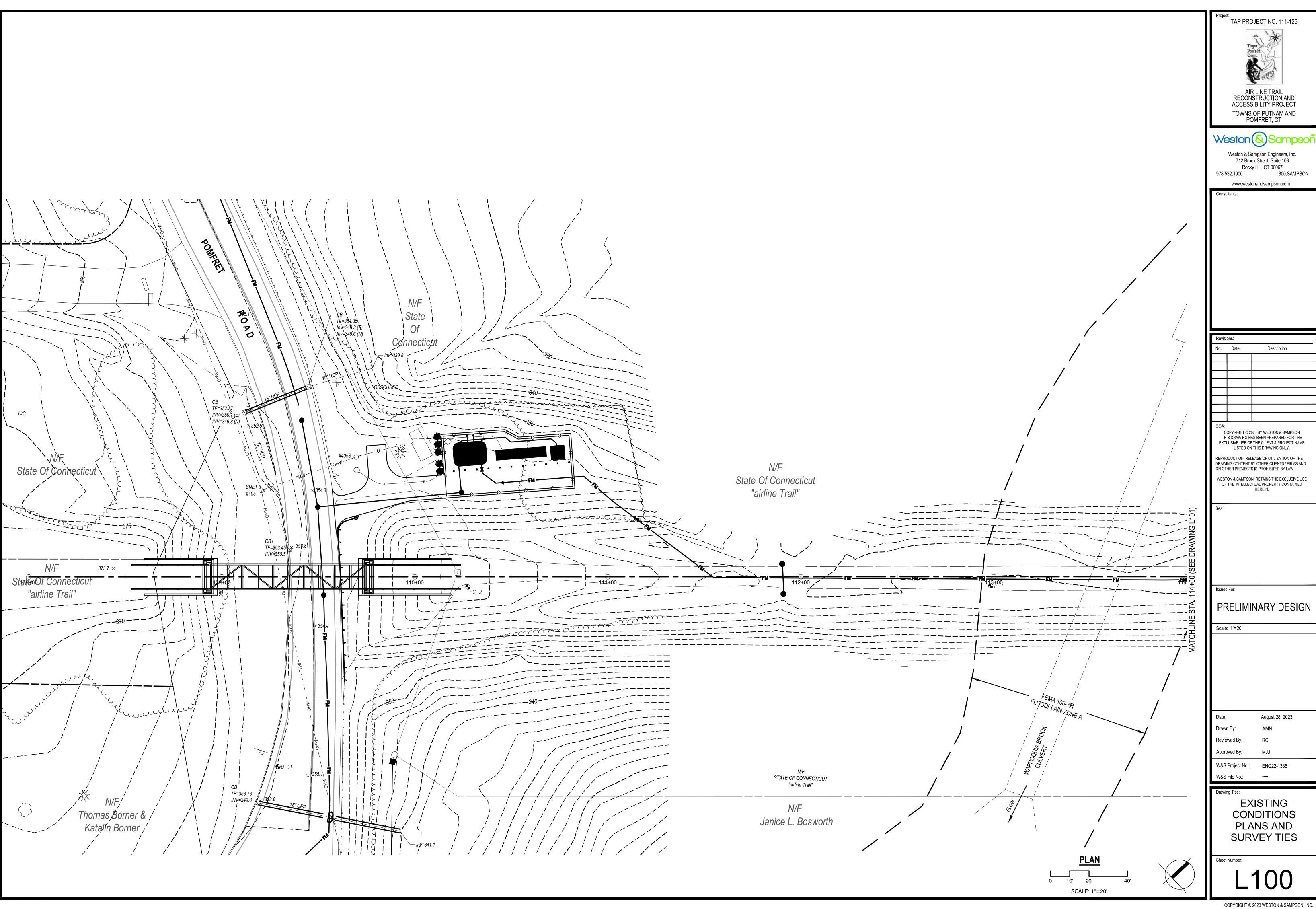
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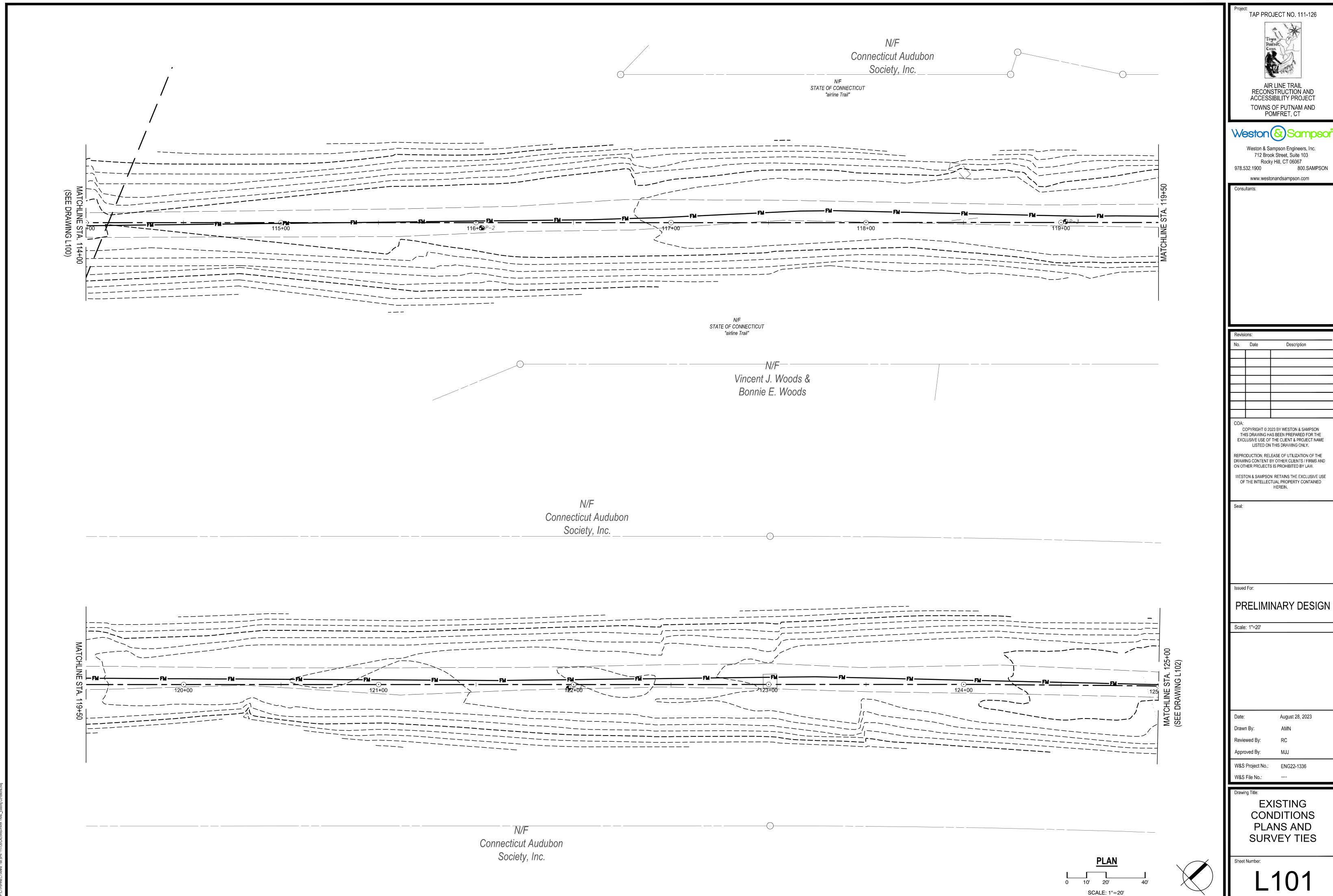
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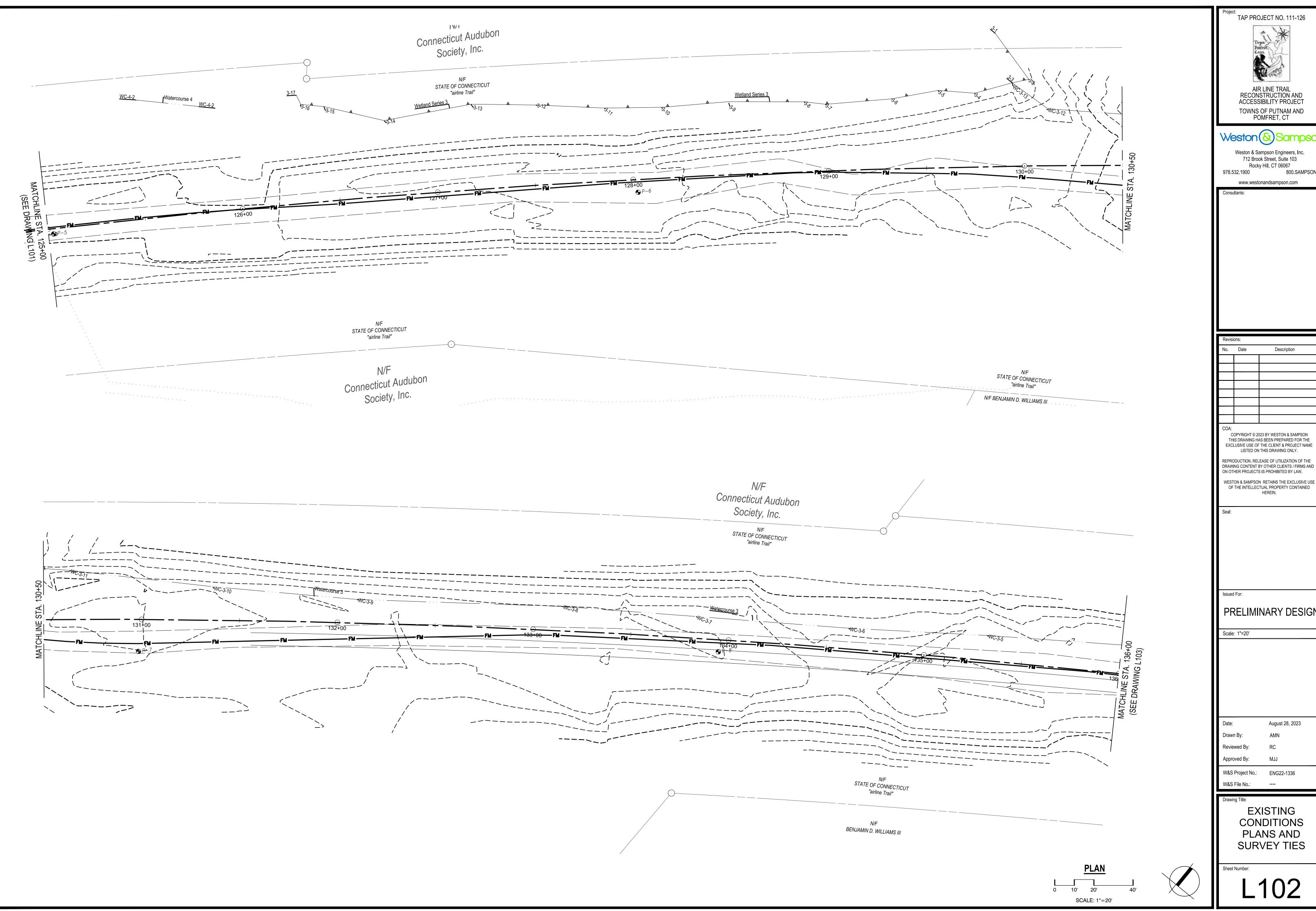
August 28, 2023 Drawn By: Reviewed By: Approved By:

W&S Project No.: ENG22-1336 W&S File No.:

LEGEND, **ABBREVIATIONS** AND NOTES







TAP PROJECT NO. 111-126 AIR LINE TRAIL RECONSTRUCTION AND ACCESSIBILITY PROJECT TOWNS OF PUTNAM AND POMFRET, CT

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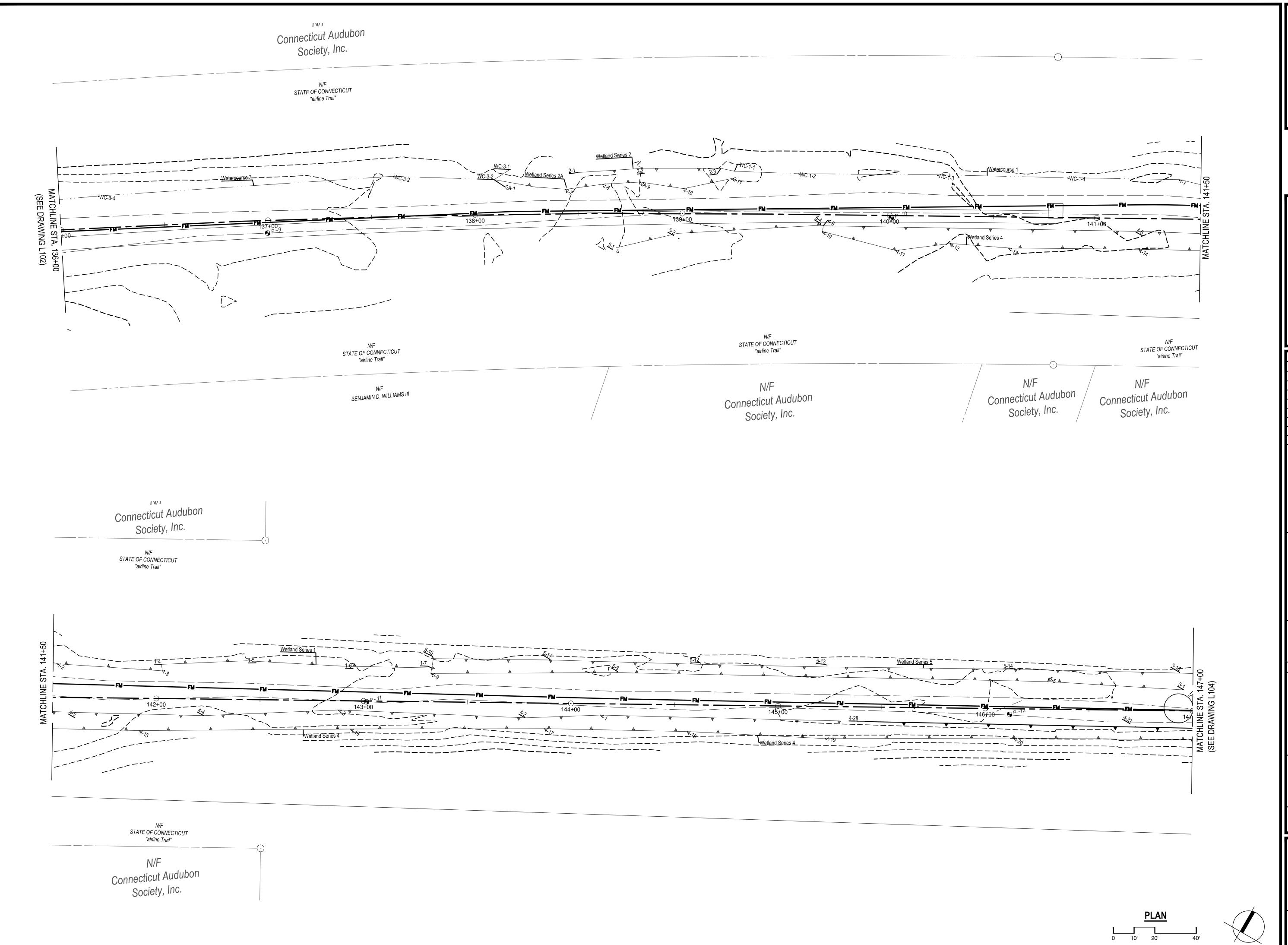
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TOWNS OF PUTNAM AND
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Reviewed By: RC

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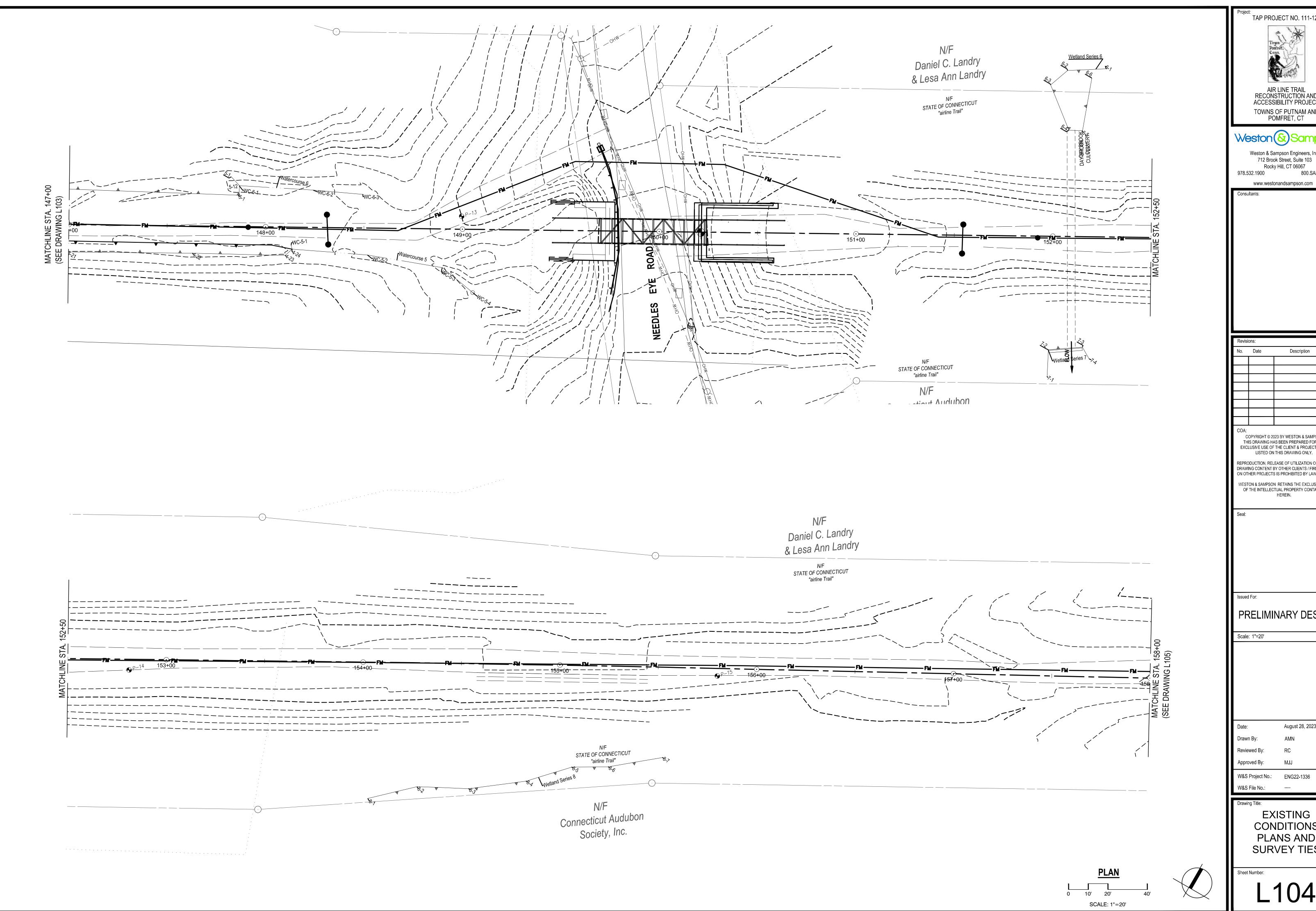
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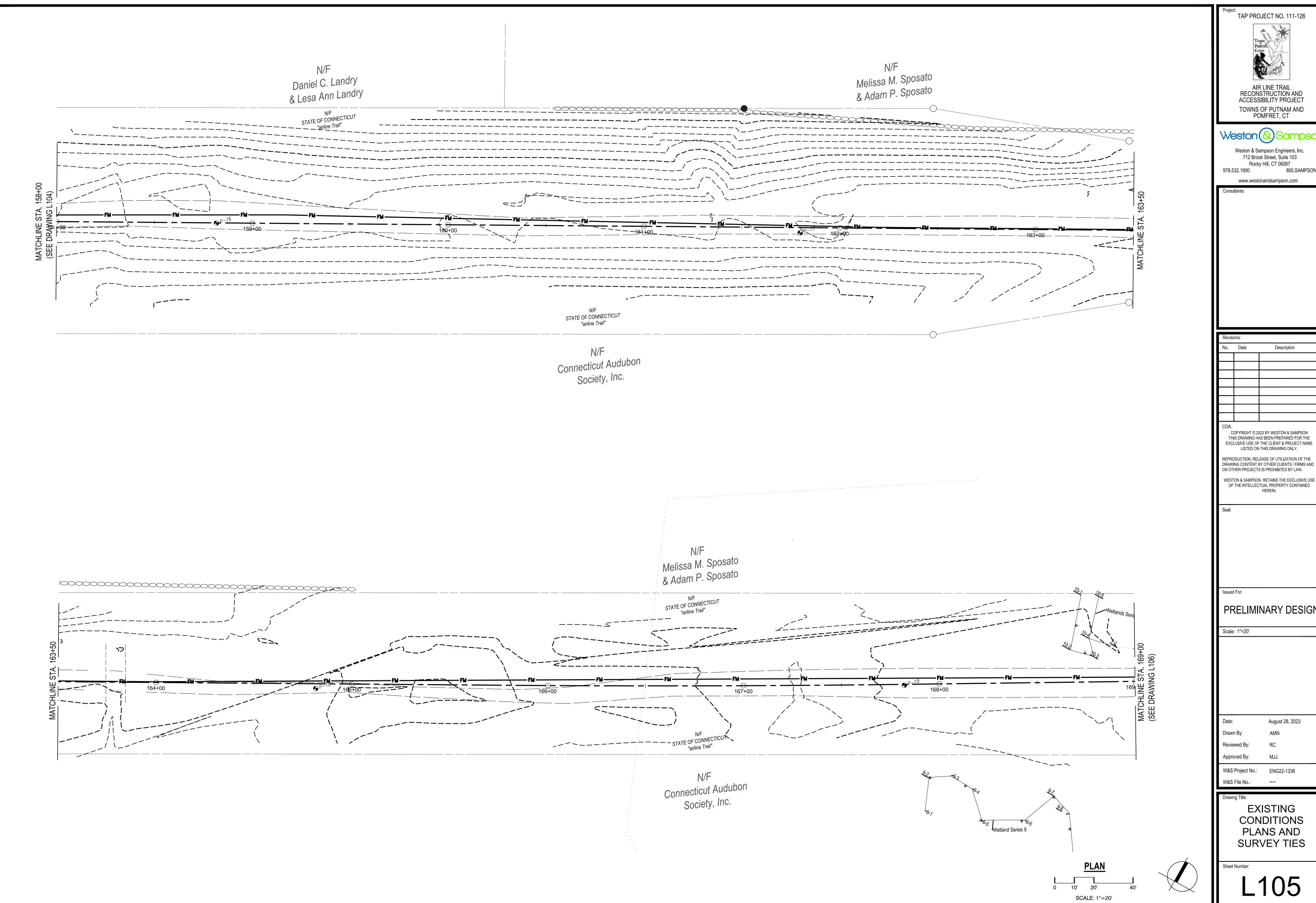
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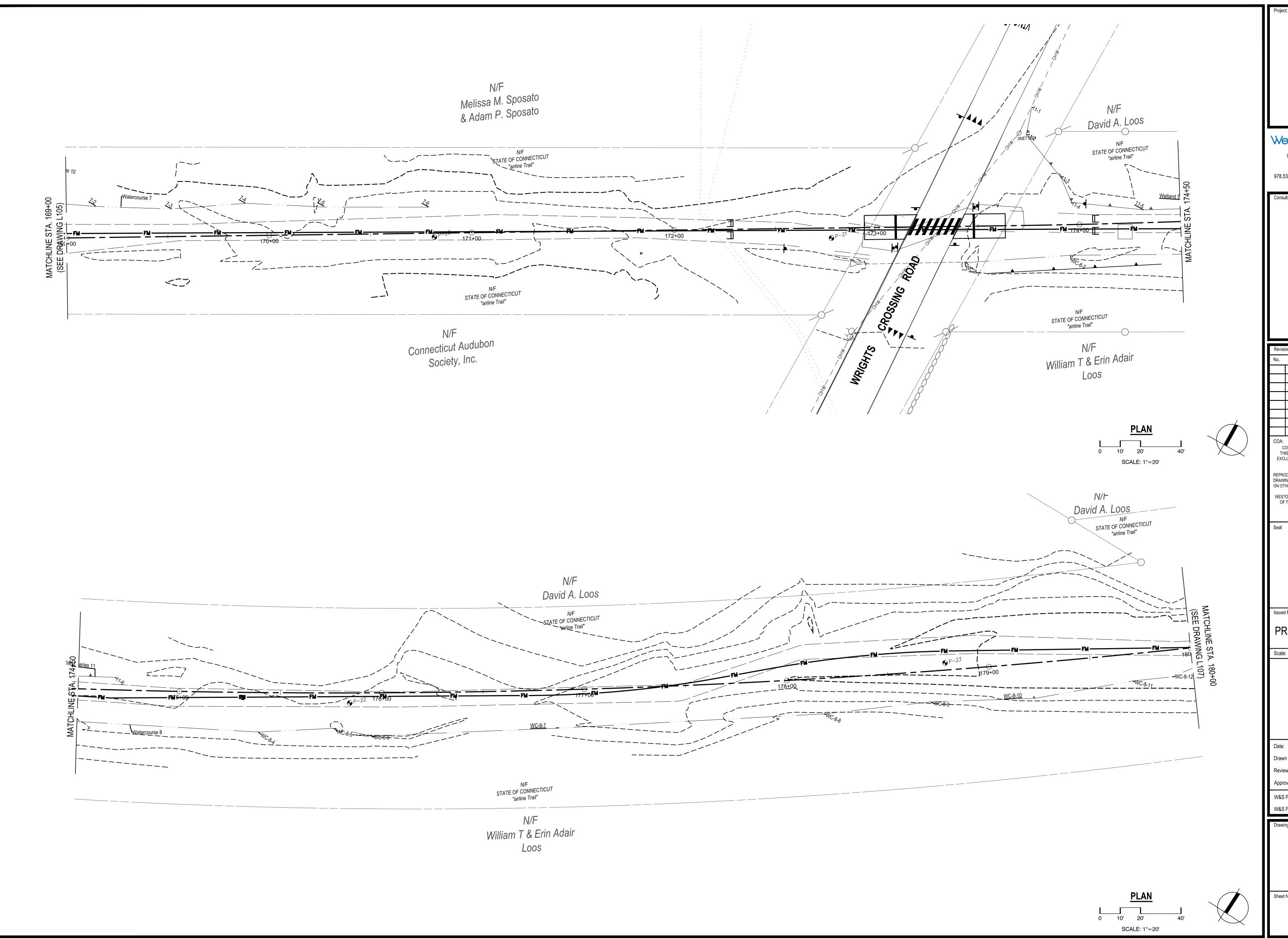
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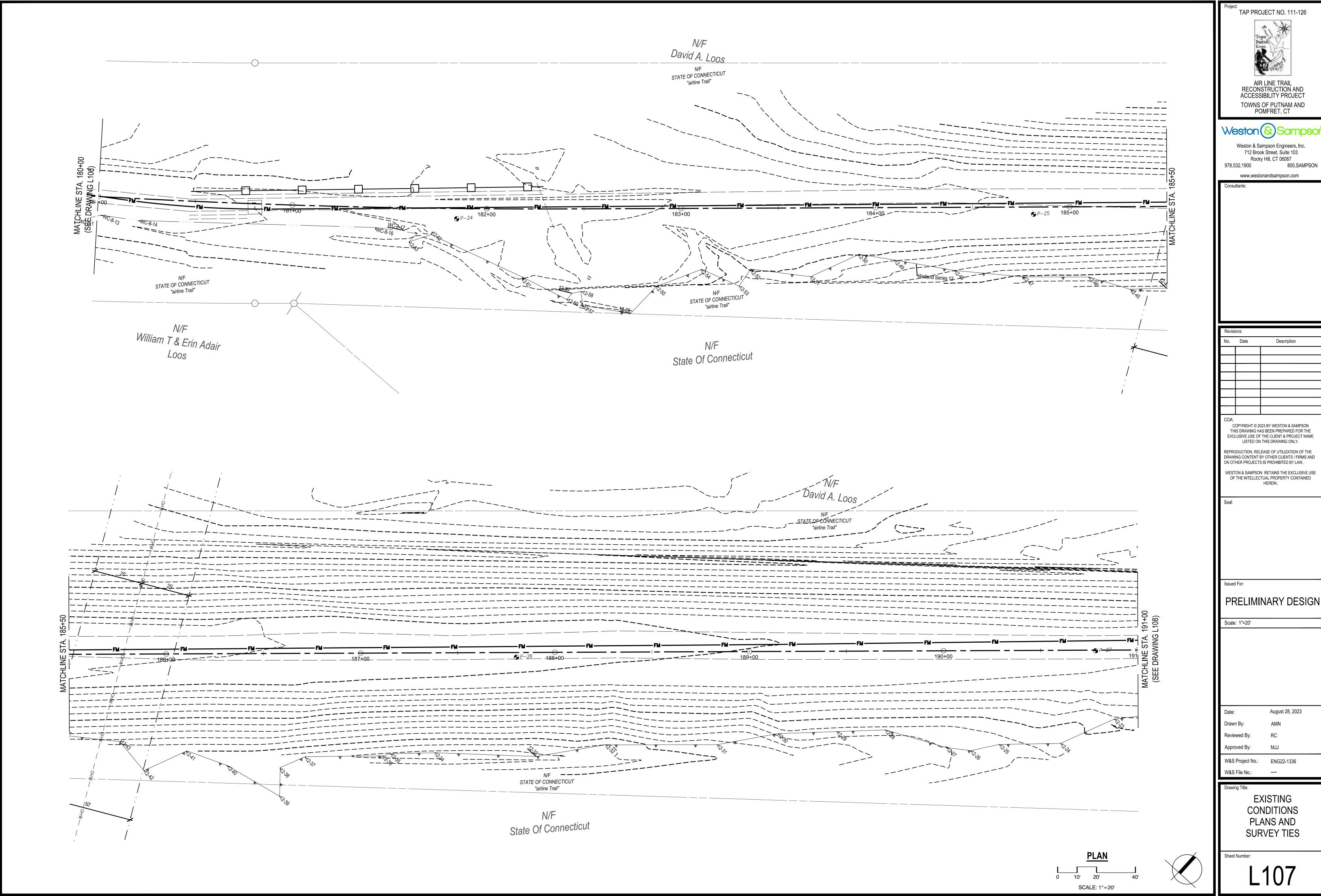
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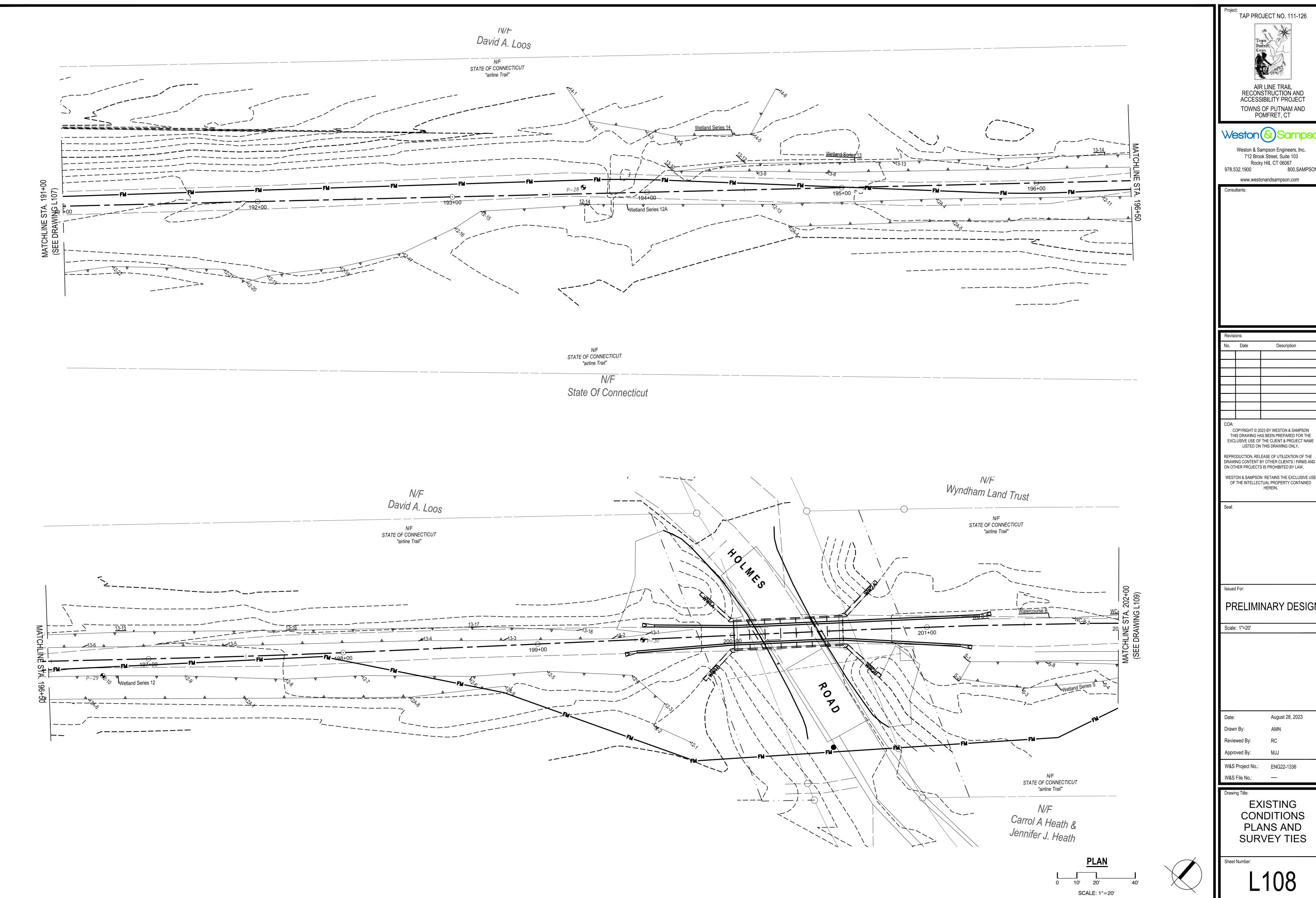
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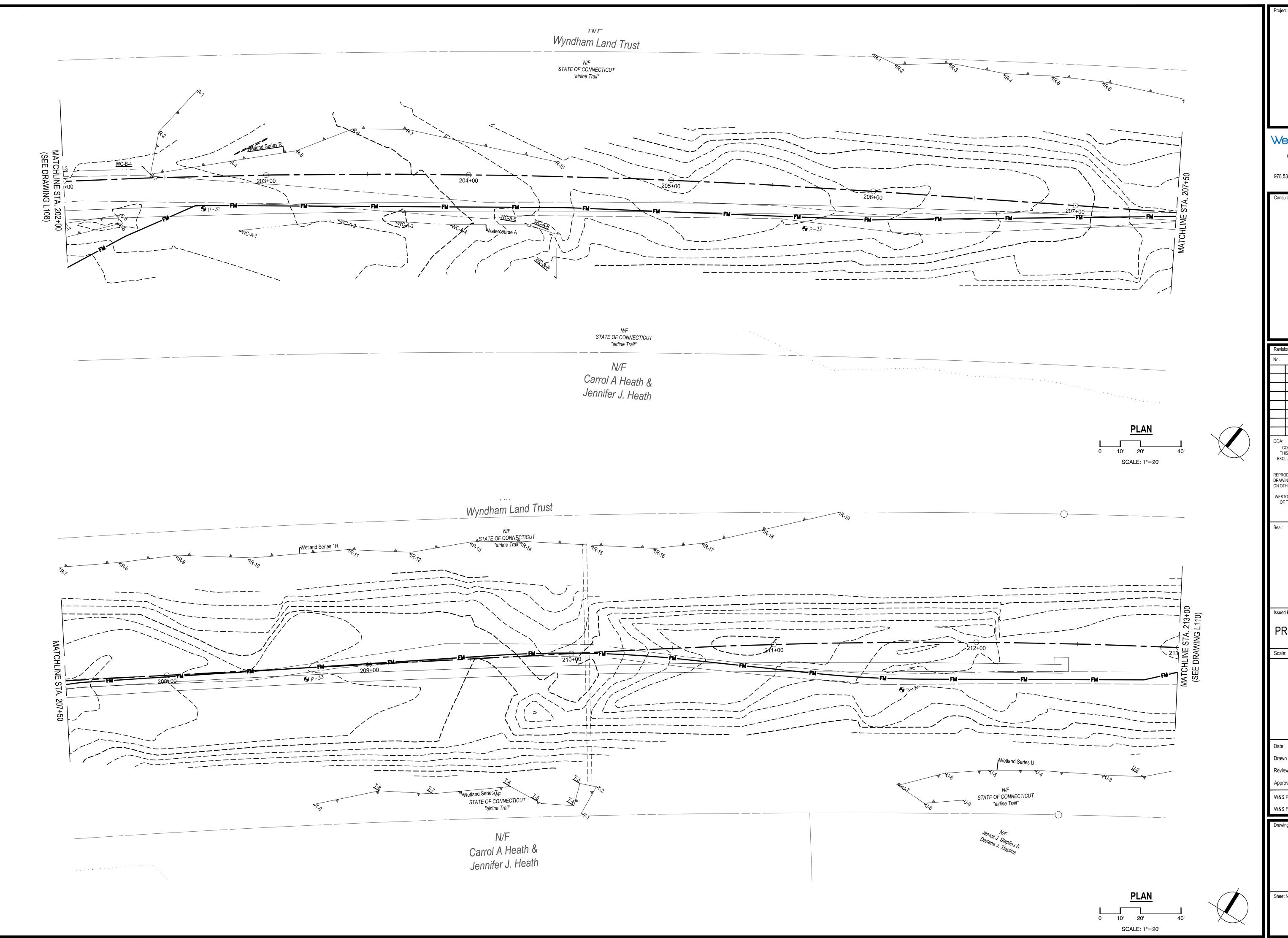
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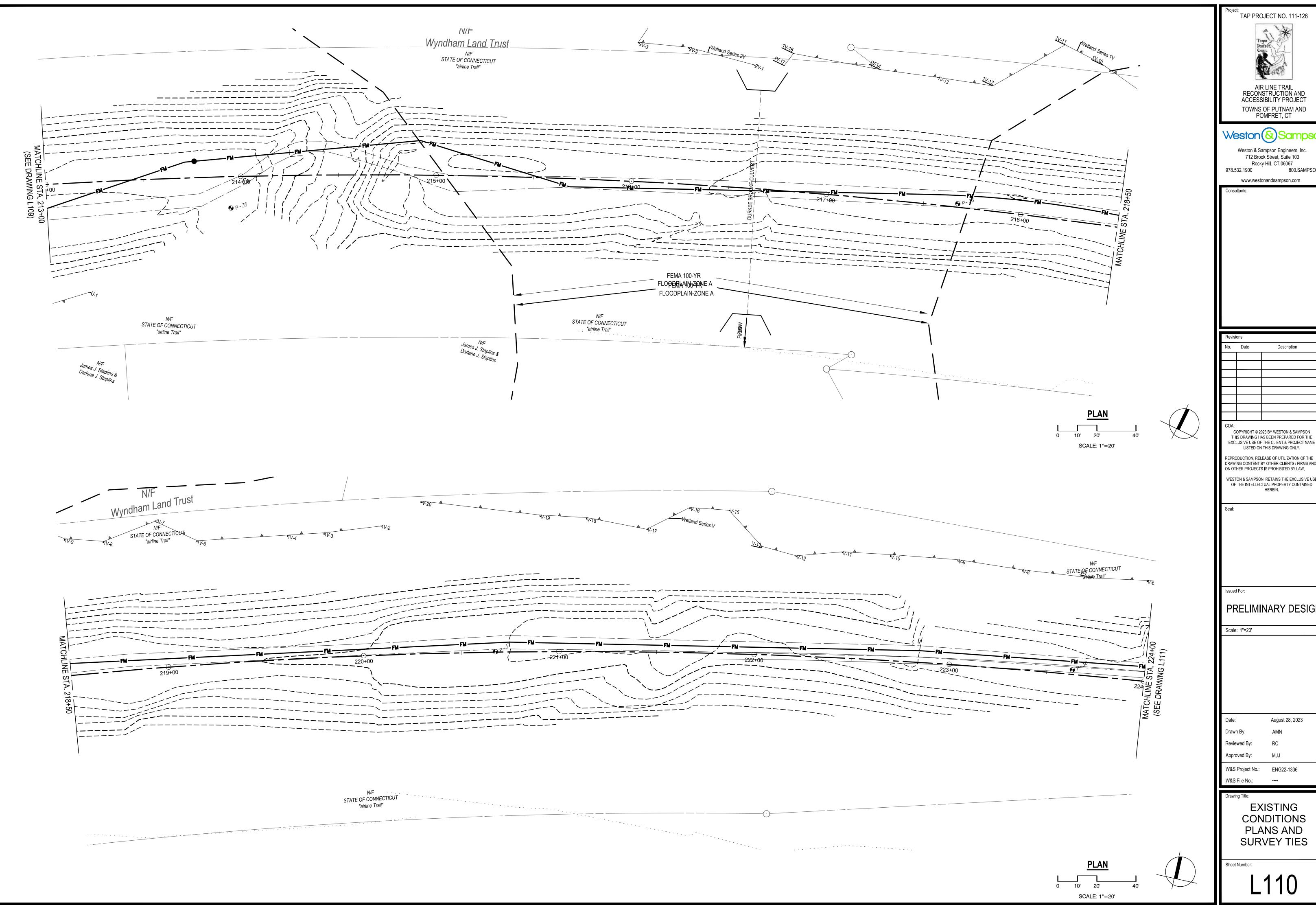
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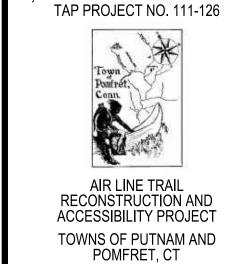
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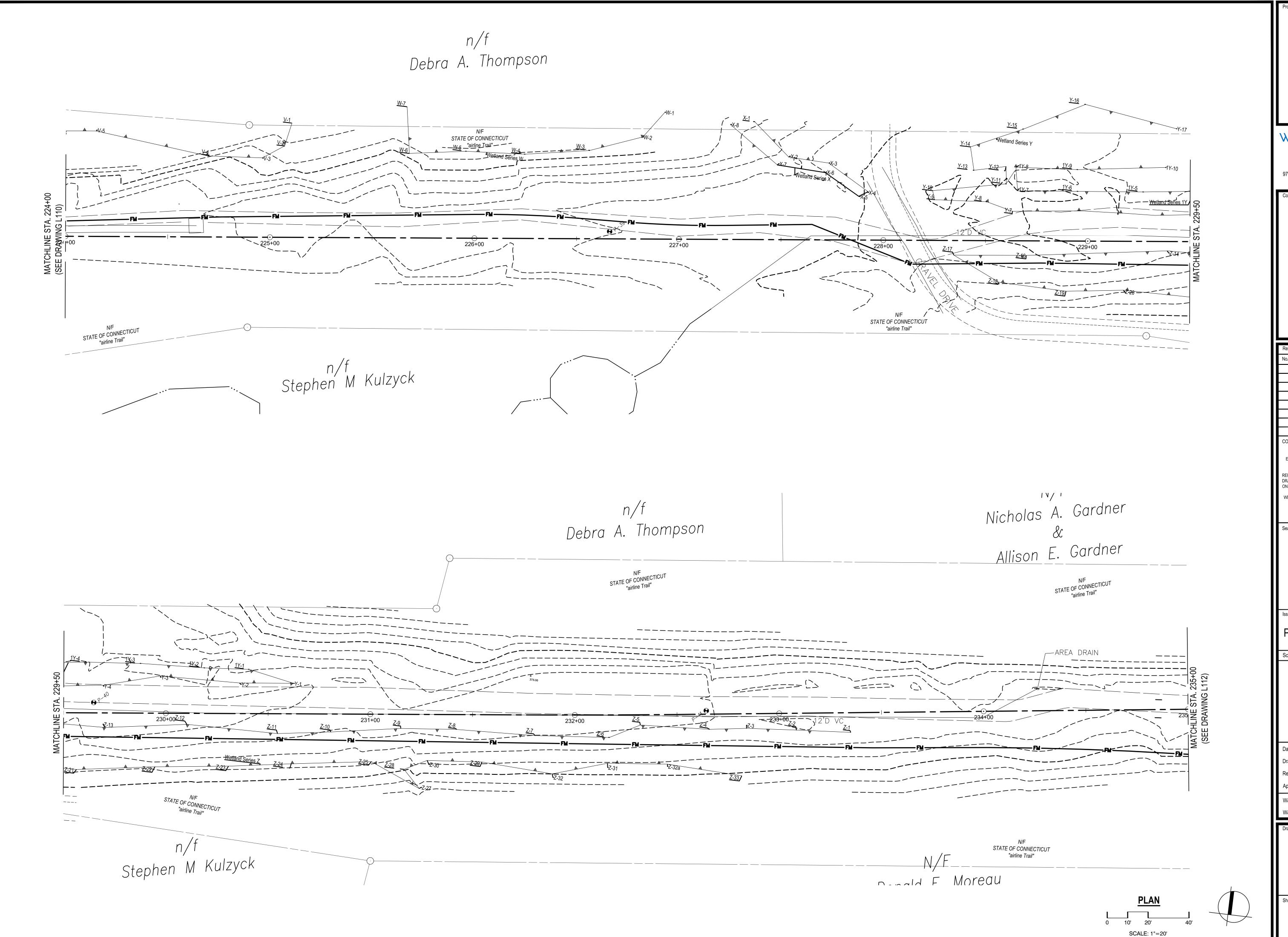
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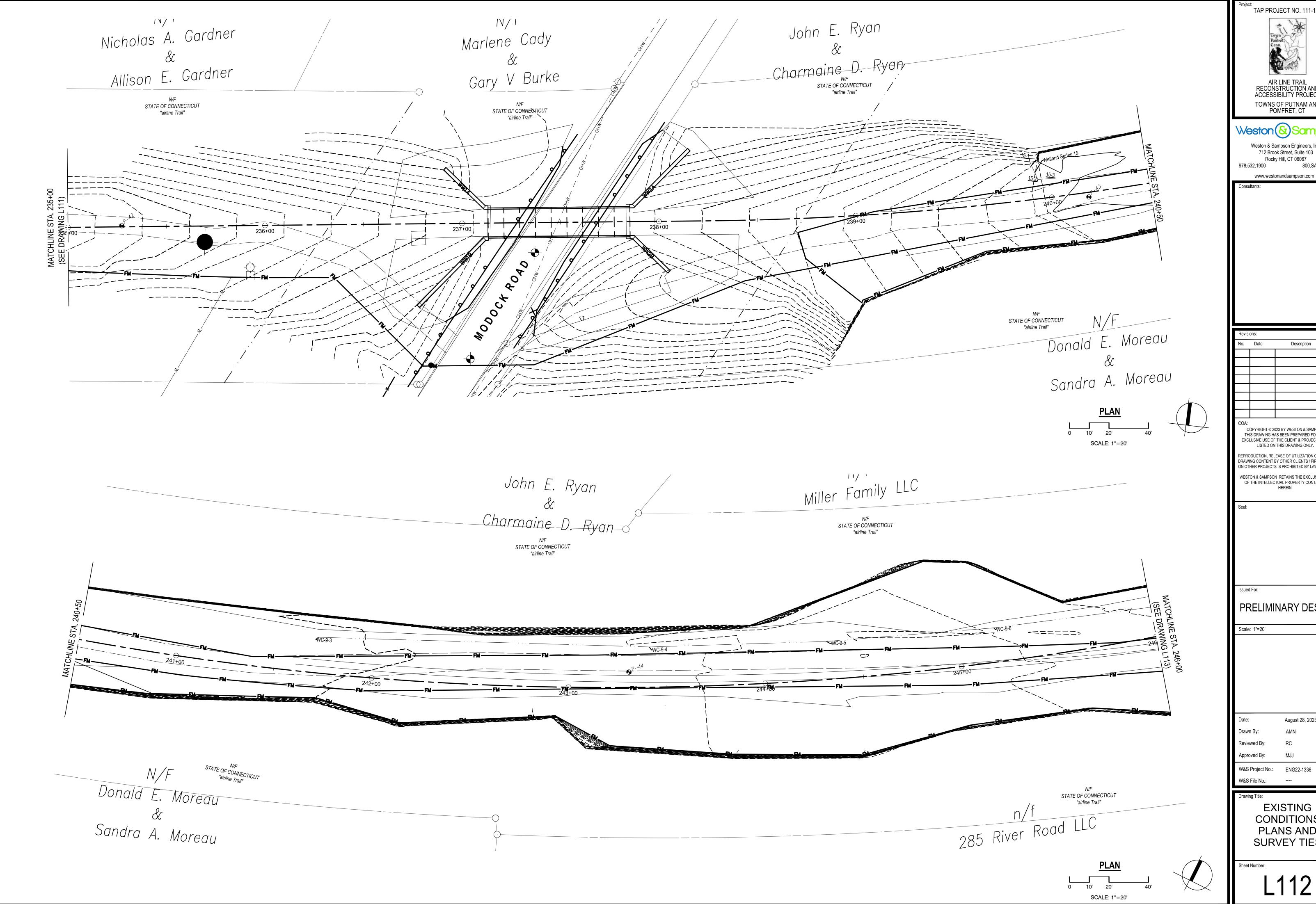
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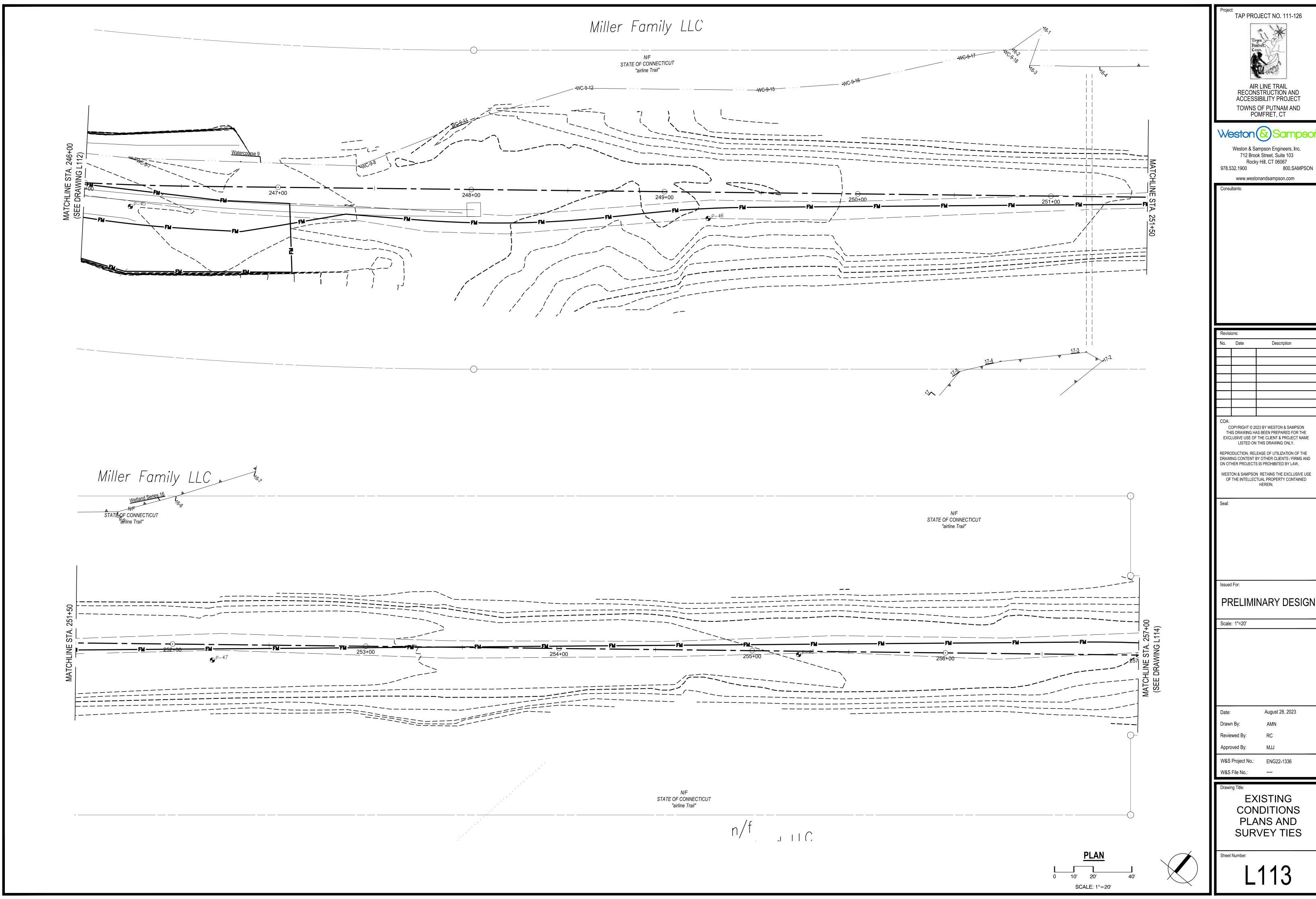
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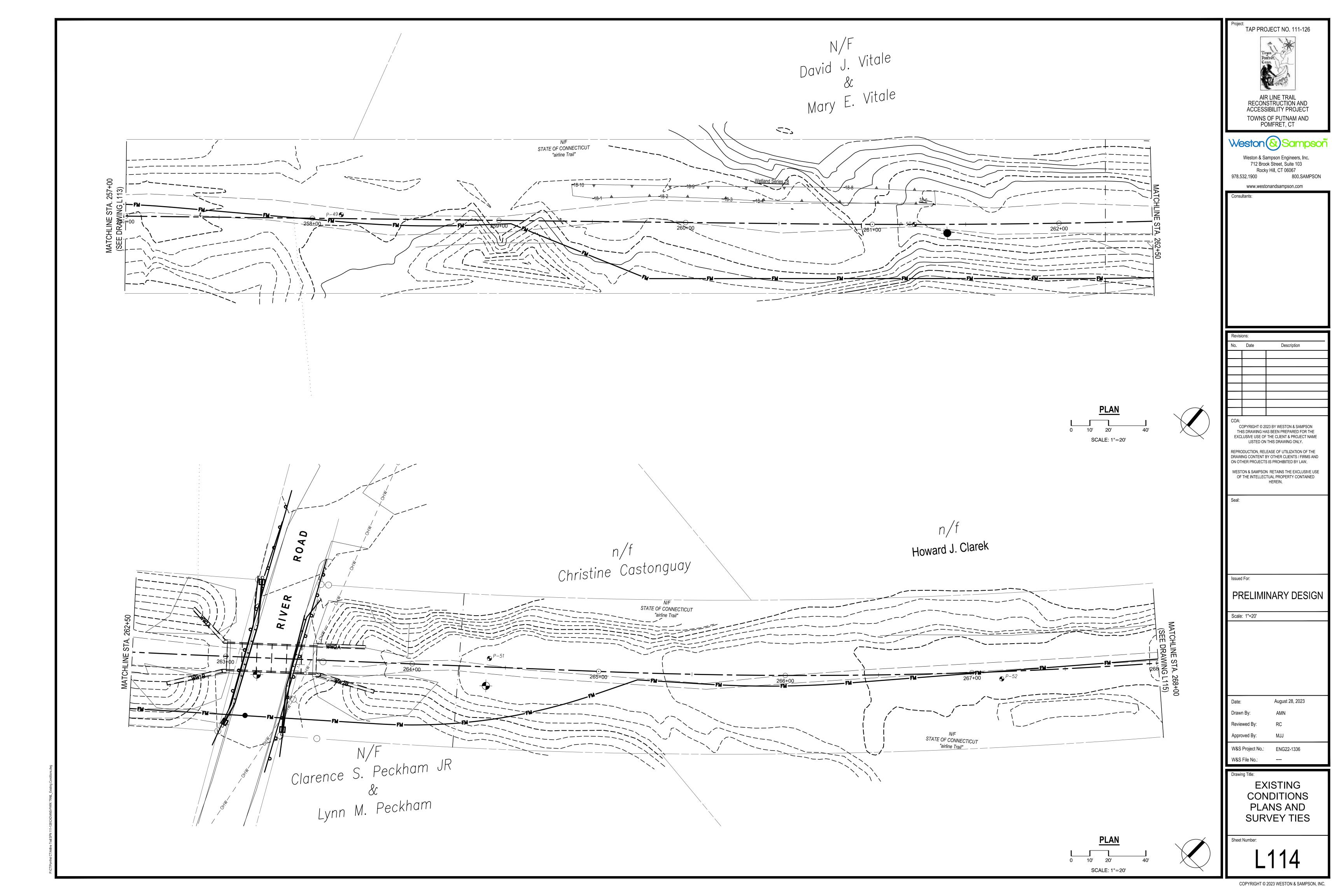
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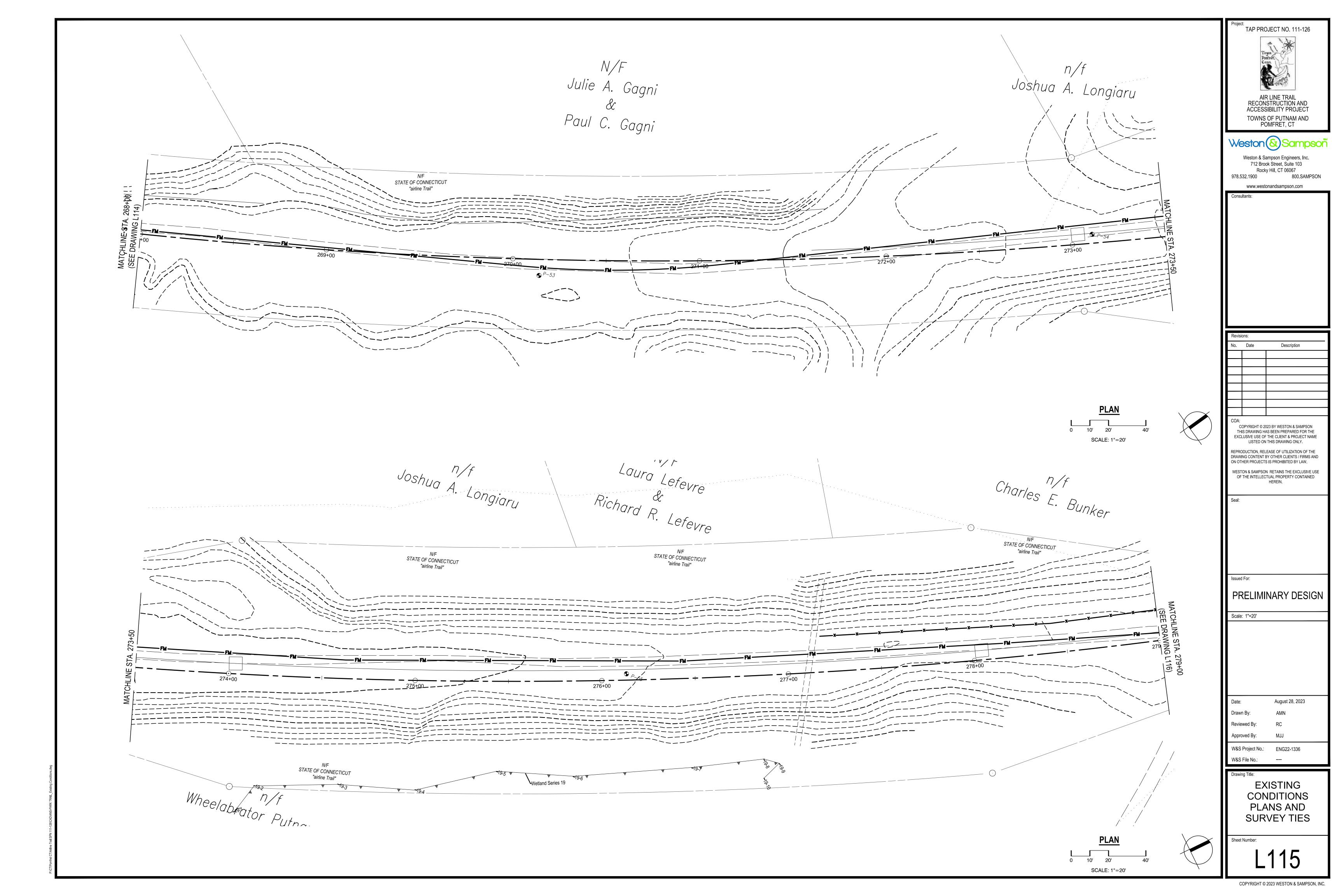
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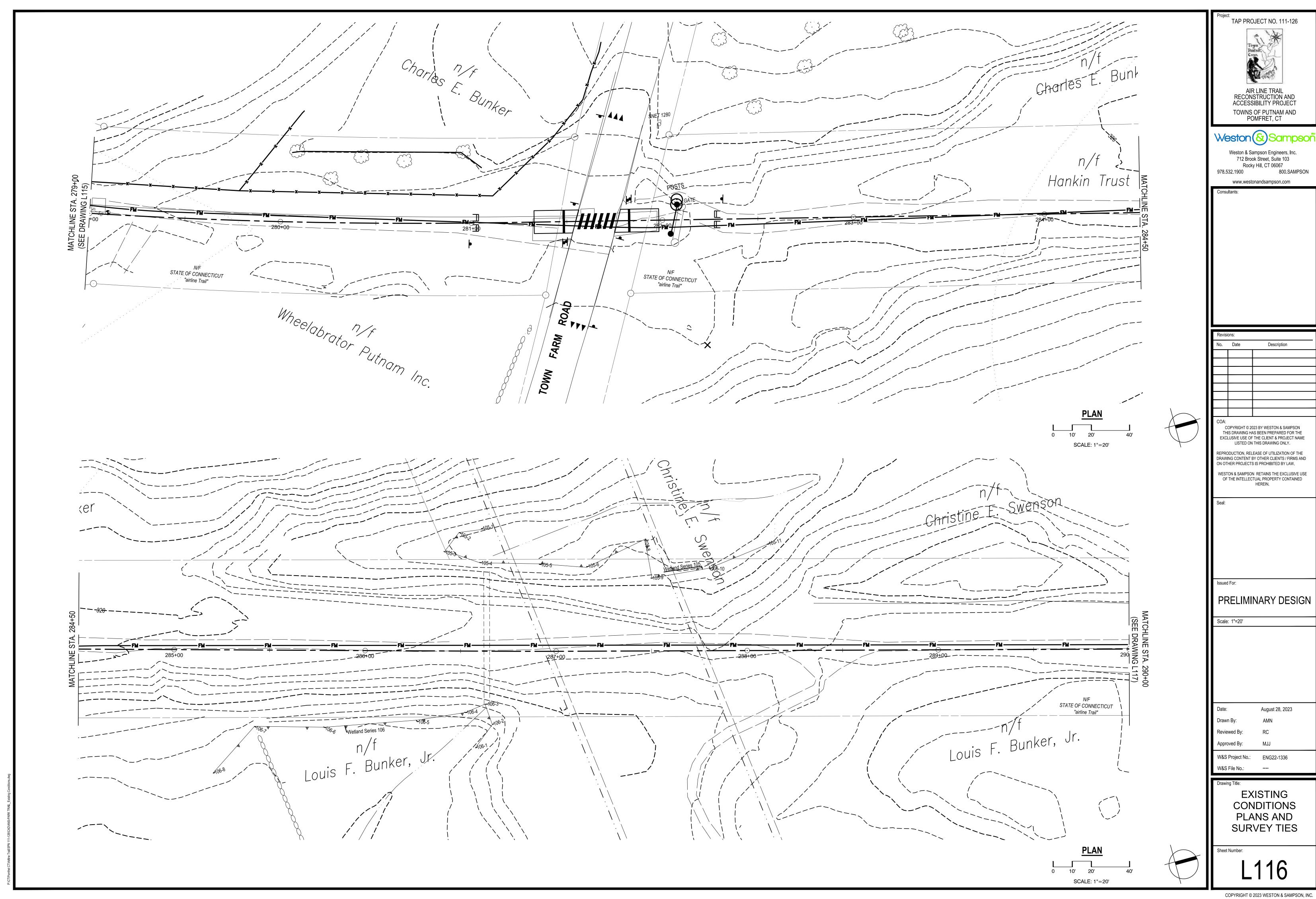
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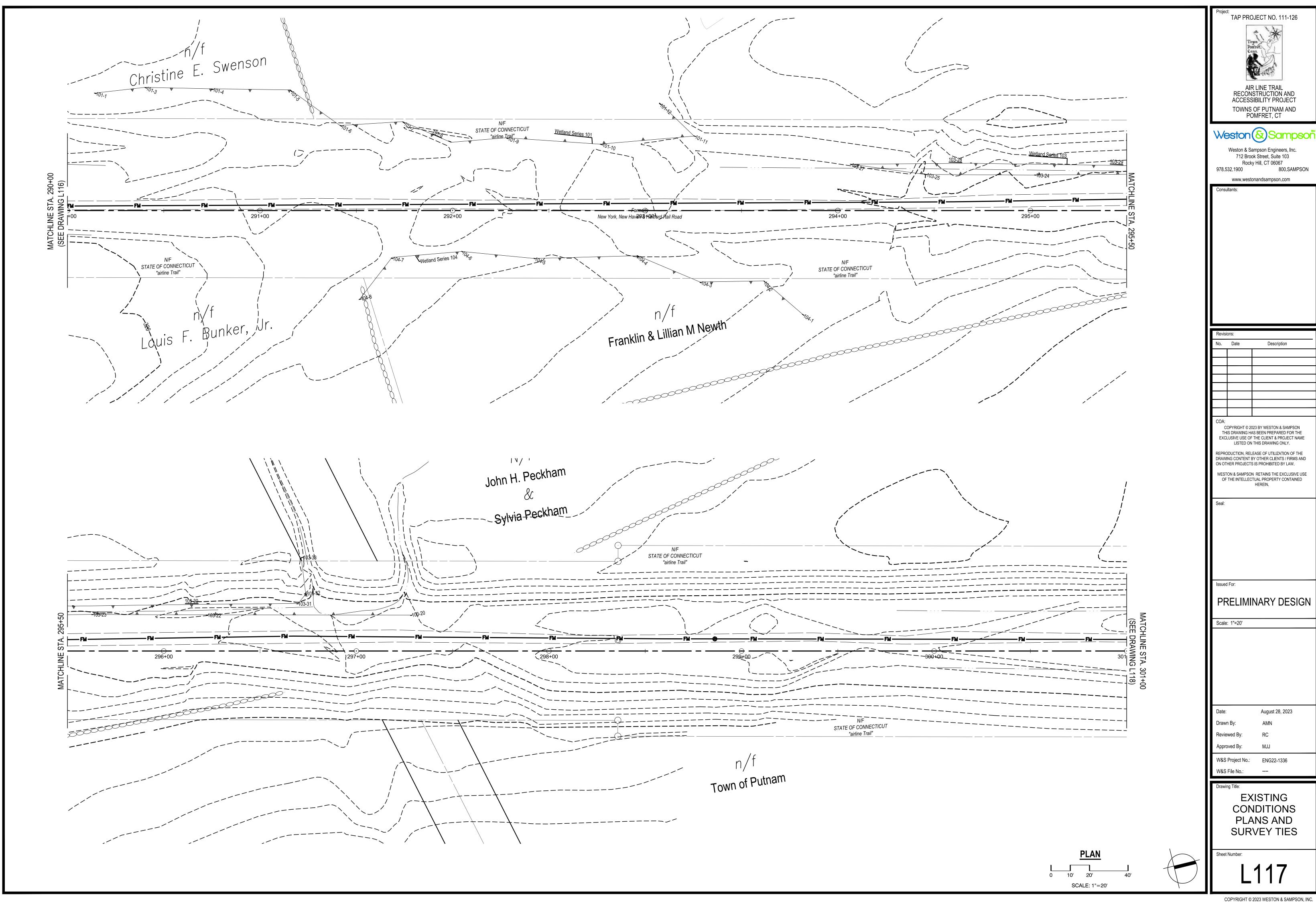


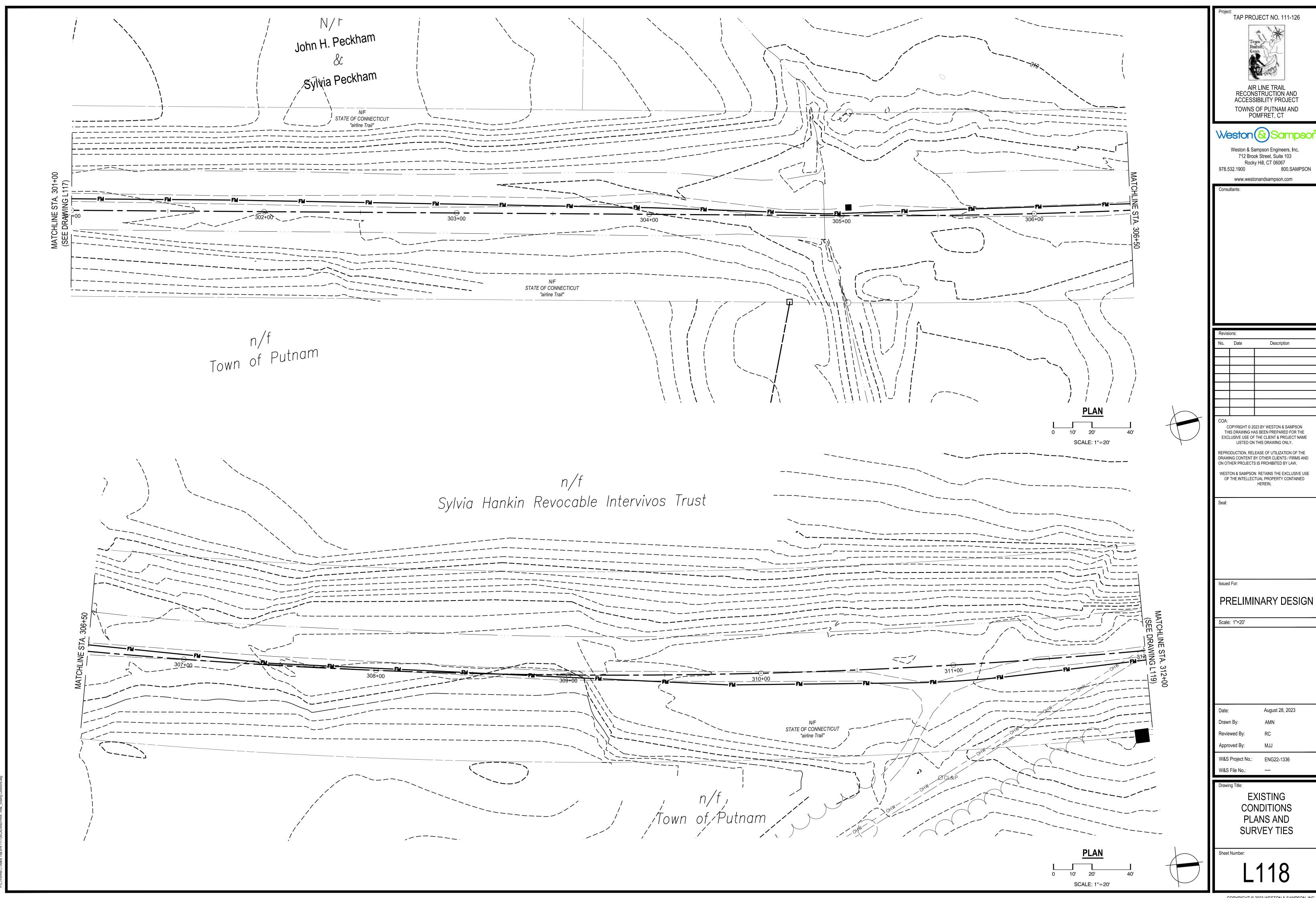
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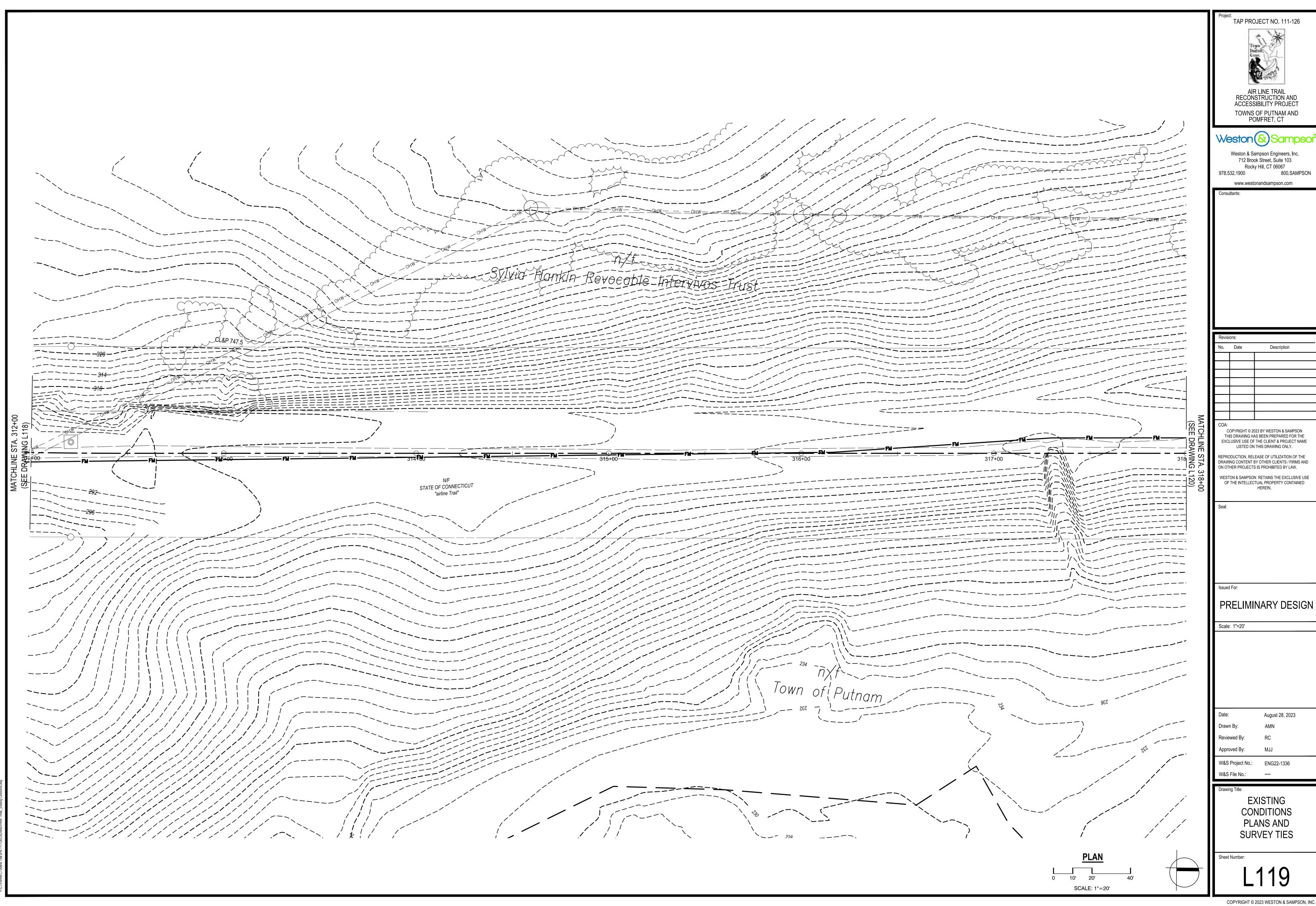


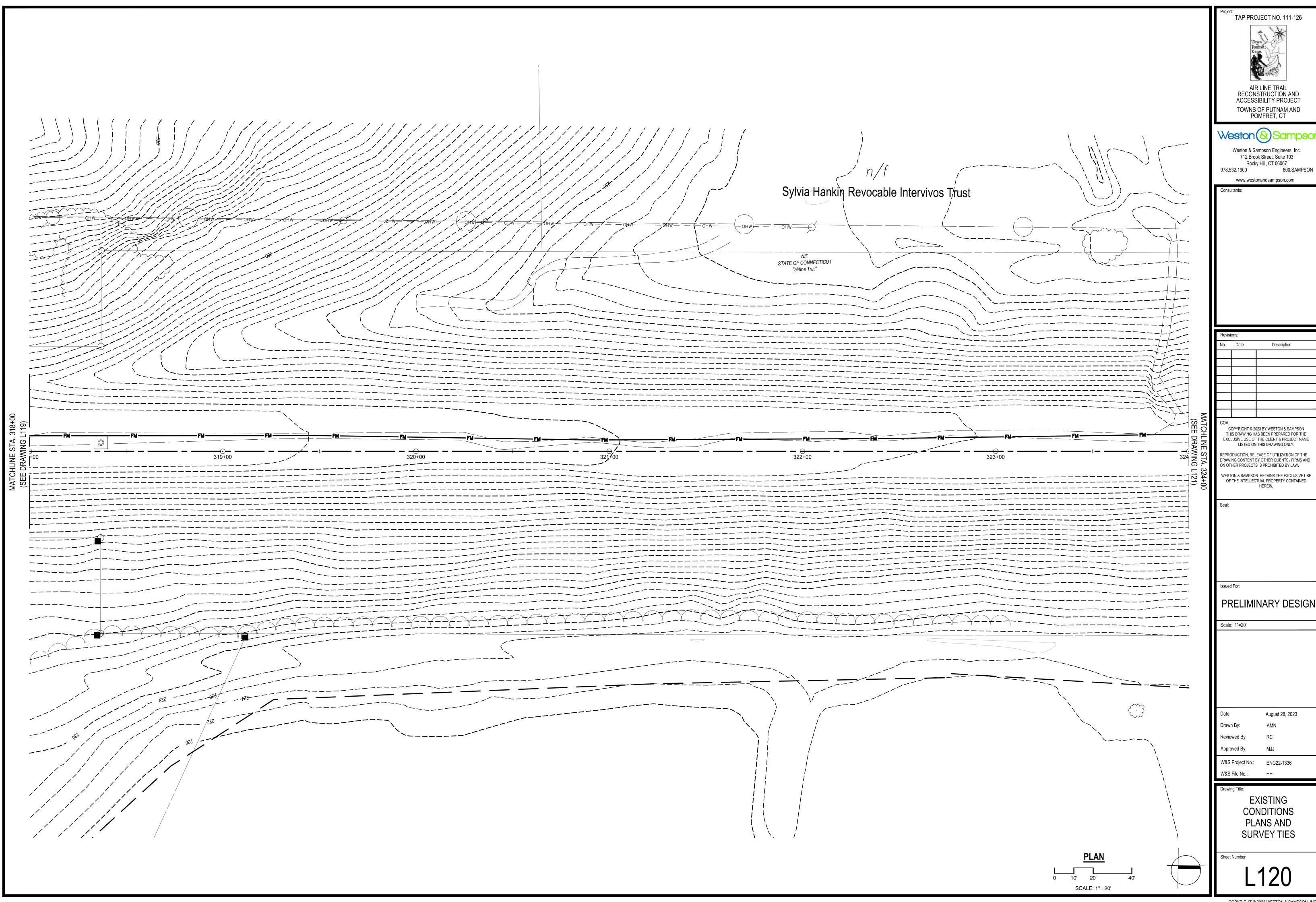


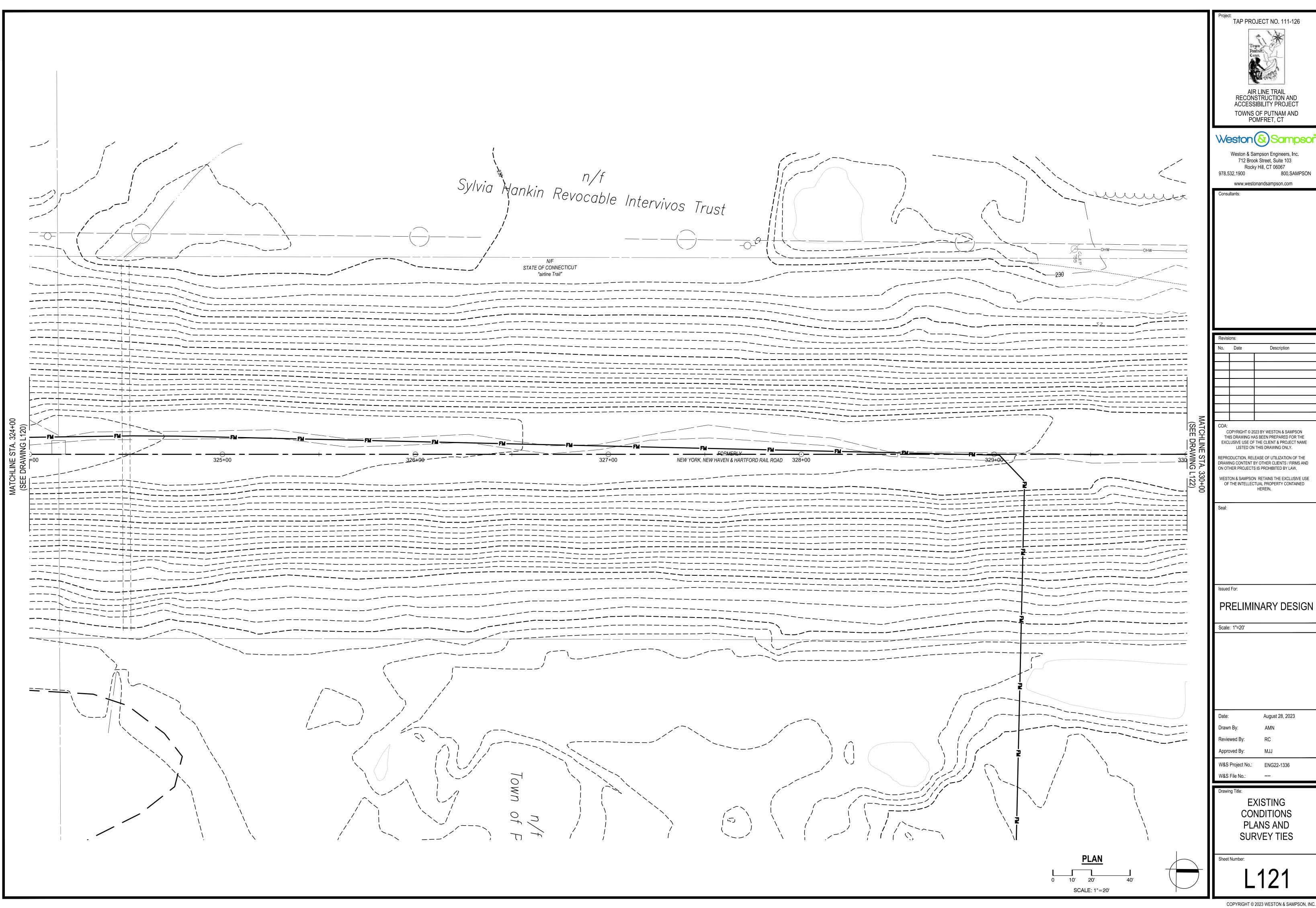


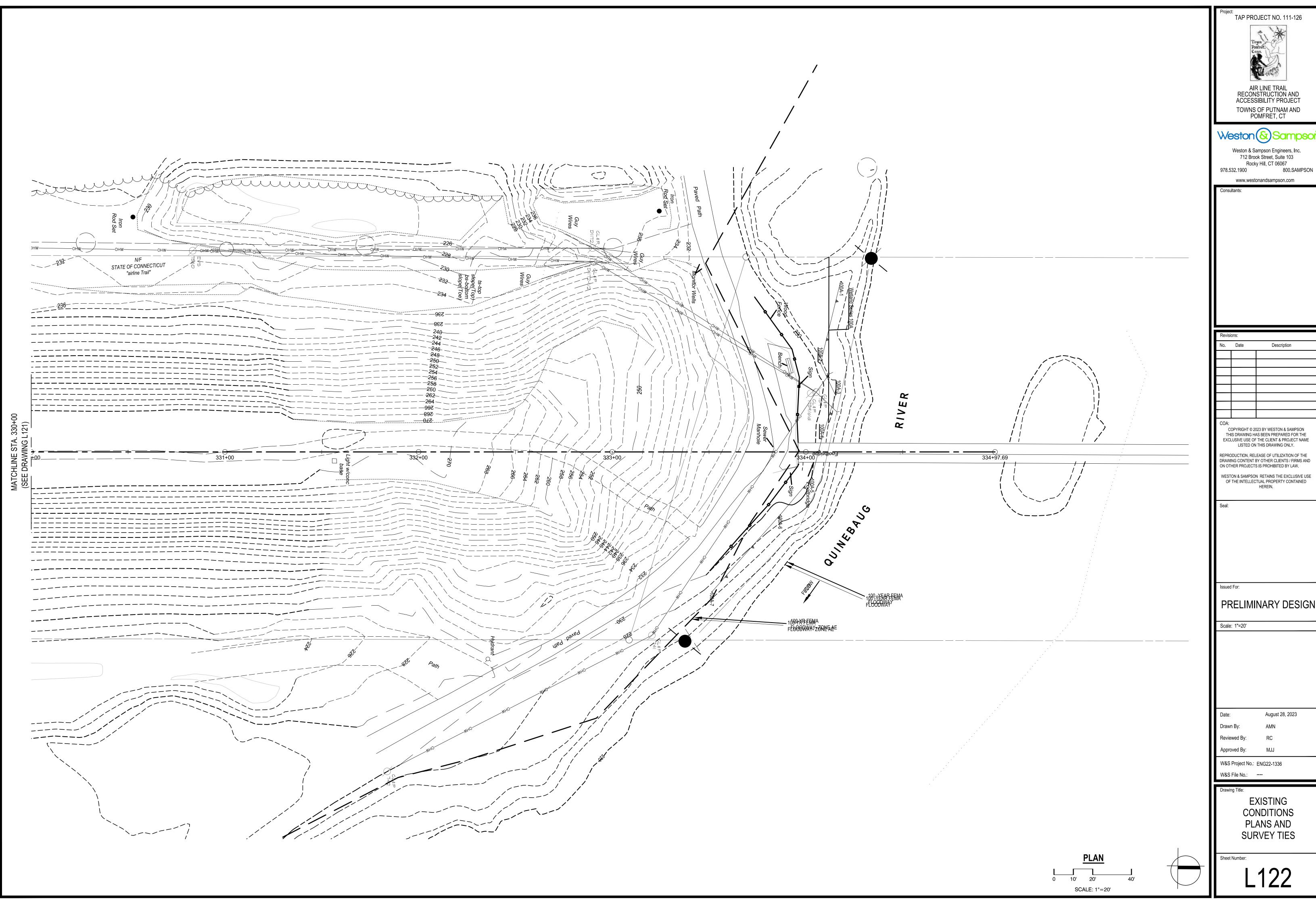


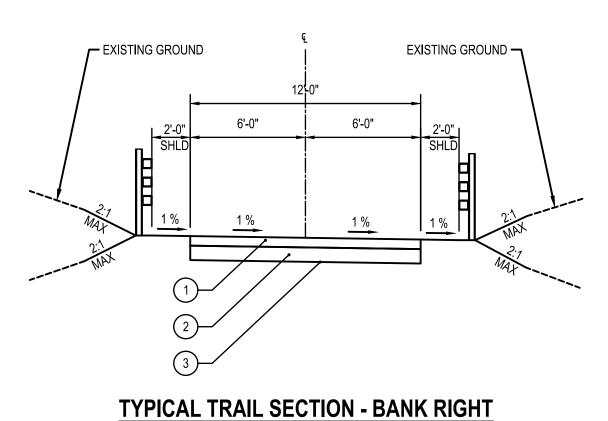




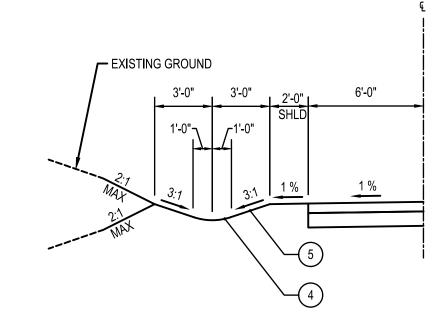






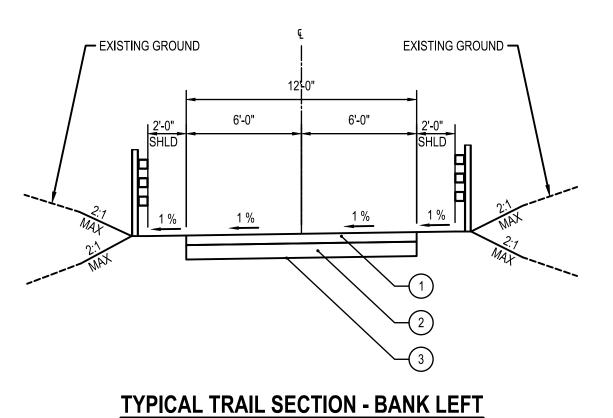


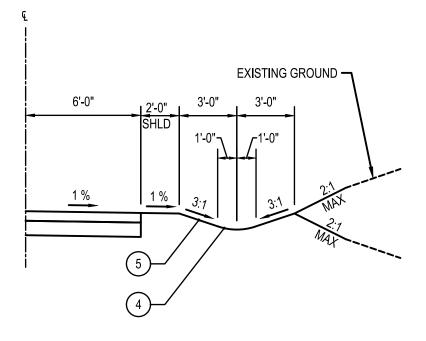
SCALE: 1"=5'-0"



### **TYPICAL DRAINAGE SWALE - LEFT**

SCALE: 1"=5'-0"





**TYPICAL DRAINAGE SWALE - RIGHT** 

<u>LEGEND</u>

- (1) 4" GRANULAR PAVEMENT
- 8" PROCESSED AGGREGATE BASE (SEE NOTE 3)
- 3 FORMATION OF SUBGRADE
- 4" TOPSOIL & TURF ESTABLISHMENT LAWN
- 5 EROSION CONTROL MATTING (SEE NOTE 4)

NOTE:

1. EROSION CONTROL MATTING TYPE D SHALL BE PLACED ON ALL SLOPES 3:1 OR STEEPER.

- 2. SEE MISCELLANEOUS DETAILS FOR UNDERDRAIN APPLICATION AND LOCATIONS.
- 3. IN AREAS OF POOR DRAINAGE OR UNSUITABLE SOIL CONDITIONS, 12" OF PROCESSED AGGREGATE BASE SHALL BE PROVIDED. FOR THE PURPOSES OF PRELIMINARY DESIGN, IT IS ASSUMED THIS WILL BE REQUIRED FOR 10% OF THE LENGTH OF THE IMPROVED TRAIL.
- 4. SEE TRAIL PLANS FOR EROSION CONTROL MATTING TYPE REQUIRED AT EACH DRAINAGE SWALE.

TAP PROJECT NO. 111-126 AIR LINE TRAIL RECONSTRUCTION AND

Weston & Sampson

ACCESSIBILITY PROJECT

TOWNS OF PUTNAM AND POMFRET, CT

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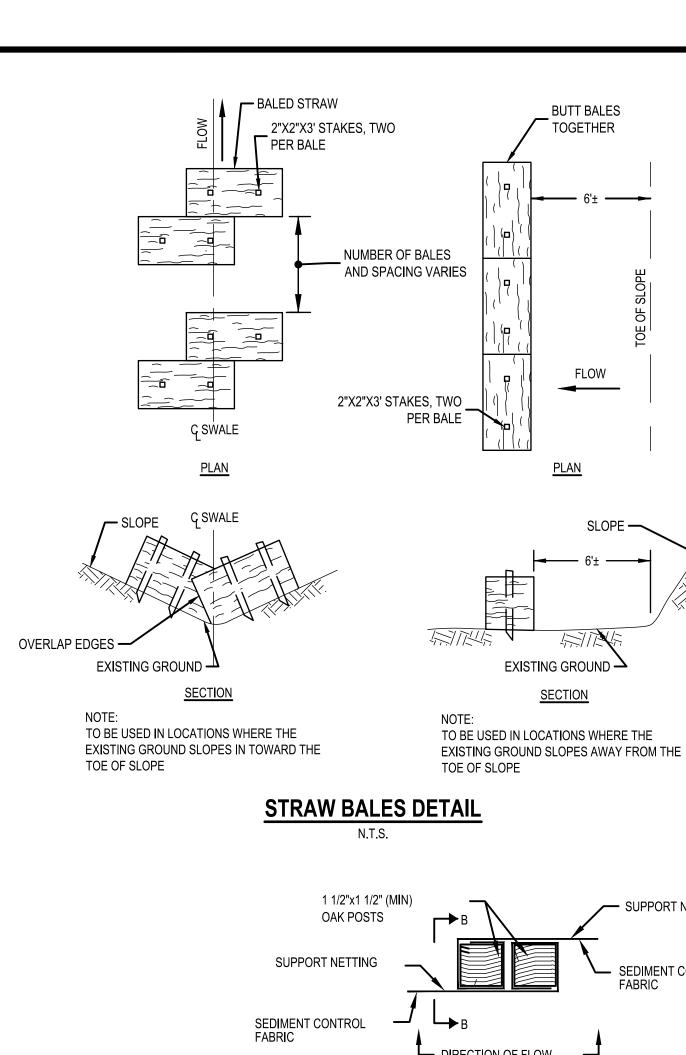
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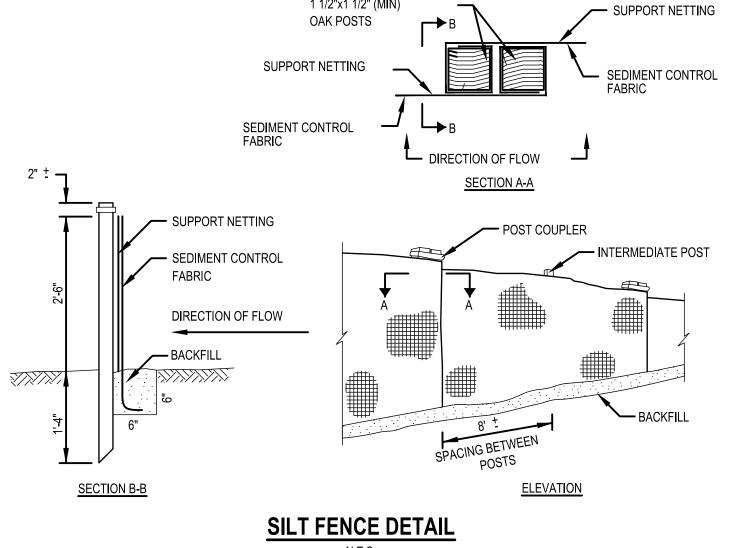
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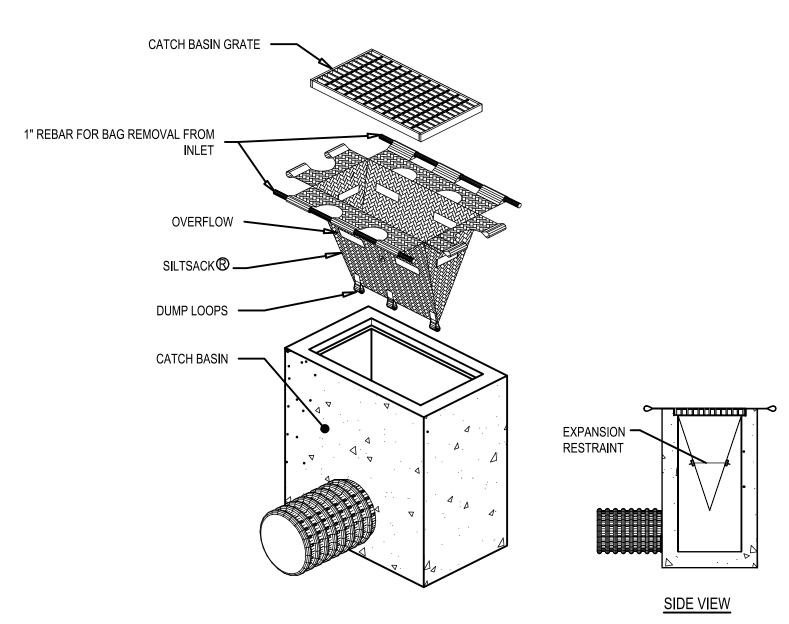
June 26, 2023 Drawn By: GB Reviewed By: Approved By:

W&S Project No.: ENG22-1336

TRAIL TYPICAL SECTION





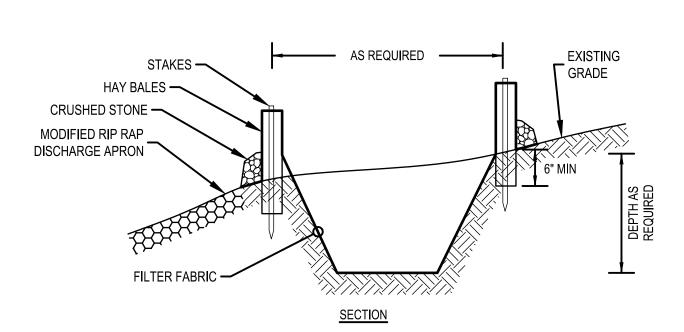


NOTES:

1. PROVIDE HI-FLOW SILT SACK TYPE A FOR TYPE "C-L" CATCH BASIN TOPS AND TYPE B WITH CURB DEFLECTOR FOR TYPE "C" CATCH BASIN TOPS OR OTHER STRUCTURES WITH CURB INLET.

**CATCH BASIN INLET PROTECTION** 

AS REQUIRED HAY BALES AND CRUSHED STONE PUMP DISCHARGE PIPE(S) <u>PLAN</u>

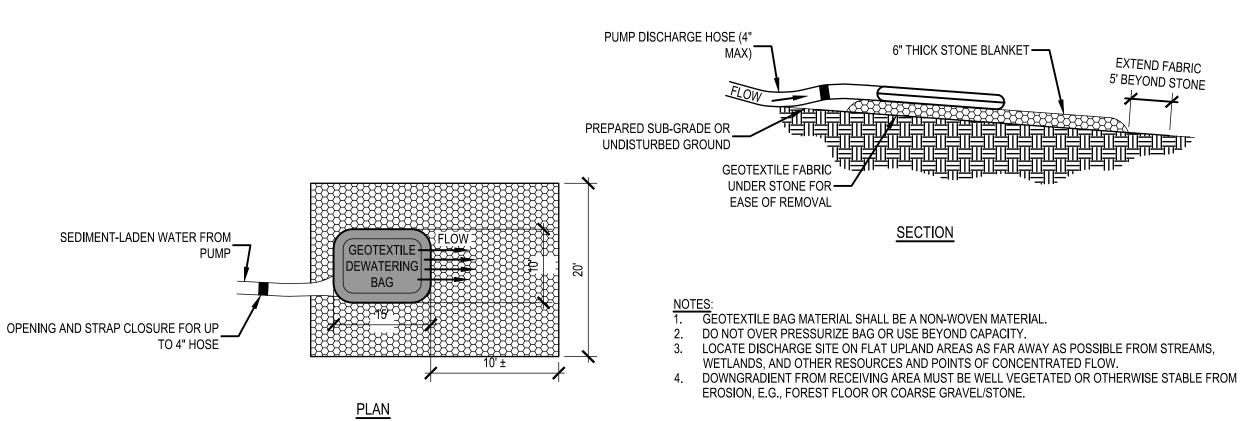


1. THE CONTRACTOR SHALL SIZE BASIN BASED ON THE SELECTED PUMP DISCHARGE FLOWS AND ENLARGE AS NECESSARY AT NO ADDITIONAL COST TO THE OWNER, TO ALLOW FOR PROPER FUNCTION OF THE BASIN.

2. ALTERNATE SEDIMENTATION BASINS MAY BE USED WITH PRIOR APPROVAL OF THE ENGINEER.

#### TYPICAL SEDIMENTATION BASIN

N.T.S.



# **GEOTEXTILE DEWATERING BAG**

SEDIMENTATION AND EROSION CONTROL MAINTENANCE PROCEDURES **AND NOTES** 

ALL SEDIMENTATION AND EROSION CONTROL DEVICES SHALL BE INSPECTED DURING CONSTRUCTION ON A WEEKLY BASIS, AND FOLLOWING ALL STORMS, BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN AND MAKE REPAIRS AND REMOVE SEDIMENT IF IT HAS ACCUMULATED TO A DEPTH OF 1/2 THE HEIGHT OF THE STRAW BALES OR SILT FENCE AS INSTALLED. THE CONTRACTOR HAS THE OPTION TO INSTALL A NEW ROW OF STRAW BALES OR SILT FENCE BEHIND THE SEDIMENTED CONTROLS. THE CONTRACTOR SHALL MAINTAIN AND MAKE REPAIRS AND REMOVE SEDIMENT AS REQUESTED BY THE RESIDENT ENGINEER. THIS WORK SHALL BE PERFORMED WITHIN 24 HOURS OF THE REQUEST AND THERE SHALL BE NO SEPARATE PAYMENT FOR THIS MAINTENANCE. WHERE CONSTRUCTION ACTIVITIES HAVE PERMANENTLY CEASED OR HAVE TEMPORARILY BEEN SUSPENDED FOR MORE THAN THIRTY DAYS, OR WHEN FINAL GRADES HAVE BEEN REACHED IN ANY PORTION OF THE SITE, STABILIZATION PRACTICES SHALL BE IMPLEMENTED WITHIN 7 DAYS.

THE CONTRACTOR SHALL CLEAN SEDIMENT AND DEBRIS FROM ALL DRAINAGE STRUCTURES, AND PIPES AT THE COMPLETION OF CONSTRUCTION, AND AS REQUESTED BY THE RESIDENT ENGINEER TO KEEP THE SYSTEM FUNCTIONING PROPERLY DURING CONSTRUCTION.

FOLLOWING COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL REPAIR ALL ERODED AREAS AND ENSURE A GOOD STAND OF TURF IS ESTABLISHED THROUGHOUT. THE CONTRACTOR SHALL REPAIR ALL ERODED OR DISPLACED RIPRAP, AND CLEAN SEDIMENT COVERED STONES.

THIS PLAN PROPOSES EROSION CONTROL MEASURES TO ADEQUATELY CONTROL ACCELERATED EROSION AND SEDIMENTATION AND REDUCE THE DANGER FROM STORM WATER RUNOFF AT THE SITE. THE RUNOFF SHALL BE CONTROLLED BY THE INTERCEPTION, DIVERSION, AND SAFE DISPOSAL OF THE PRECIPITATION. RUNOFF SHALL ALSO BE CONTROLLED BY STAGING CONSTRUCTION ACTIVITY AND PRESERVING NATURAL VEGETATION WHENEVER POSSIBLE.

EXISTING VEGETATION SHALL BE PROTECTED AND ONLY THAT CLEARING AND GRUBBING ABSOLUTELY NECESSARY FOR THE PROPOSED CONSTRUCTION SHALL BE PERFORMED. ALL DISTURBED AREAS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND CONTOUR, UNLESS OTHERWISE INDICATED ON THE PLANS. THE CONTRACTOR SHALL TAKE SPECIAL CARE WITH HIS CONSTRUCTION METHODS AND SHALL COMPLY WITH THE FOLLOWING GUIDELINES.

REFERENCE IS MADE TO THE "CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL" (2002), AS AMENDED. THE GUIDELINES ARE OBTAINABLE FROM THE D.E.E.P. STORE, 79 ELM STREET, HARTFORD, CONNECTICUT 06106-5127, (860) 424-4088. HTTP://WWW.CT.GOV/DEEP. AND SHOULD BE USED AS A REFERENCE IN CONSTRUCTING THE EROSION AND SEDIMENT CONTROLS INDICATED ON THESE PLANS.

ALL AREAS SHALL BE PROTECTED FROM SEDIMENTATION DURING AND AFTER CONSTRUCTION, PARTICULARLY THE STORAGE OF EXCAVATED OR STOCKPILED MATERIAL. THE CONTRACTOR SHALL CAREFULLY STRIP ALL TOPSOIL, LOAM, OR ORGANIC MATTER PRIOR TO TRENCHING OR OTHER OPERATIONS AND SHALL STORE THEM SEPARATELY FROM ALL OTHER MATERIALS DURING EXCAVATION. EACH STOCKPILE MUST BE ADEQUATELY SURROUNDED WITH SEDIMENT CONTROL MATERIAL (I.E. STRAW BALES AND/OR FILTER FABRIC FENCE).

EXCAVATION THAT MUST BE DEWATERED WILL BE PUMPED INTO AN ACTIVE DRAINAGE SYSTEM IN ACCORDANCE WITH ANY APPLICABLE PERMITTING.

BOTH THE INLET AND OUTLET OF THE PUMPS SHOULD BE FILTERED AND PROTECTED FROM SURGE ACTION. DEBRIS AND OTHER WASTE RESULTING FROM EQUIPMENT MAINTENANCE AND CONSTRUCTION WILL NOT BE DISCARDED ON SITE.

STABILIZING OF SLOPES SHALL BE DONE IMMEDIATELY AFTER CONSTRUCTION OF SLOPES. SLOPES STEEPER THAN 2:1 SHALL BE PROTECTED WITH EROSION MATS. THESE MATS ARE MANUFACTURED COMBINATIONS OF MULCH AND NETTING AND SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. ALL OTHER AREAS SHALL BE MULCHED WITH STRAW AT THE RATE OF 1.5 TO 2 TONS PER ACRE. STRAW MULCH MUST BE ANCHORED IMMEDIATELY AFTER SPREADING TO PREVENT WINDBLOWING. THE METHODS RECOMMENDED BY THE "CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL" SHALL BE USED FOR THE ANCHORING OF MULCH OR NETTING.

SEDIMENTATION CONTROL SYSTEM - THE SEDIMENTATION CONTROL SYSTEM SHALL CONSIST OF FILTER FABRIC BARRIER FENCE. THE SEDIMENTATION CONTROL SYSTEM SHALL BE INSTALLED IMMEDIATELY AFTER A CUT SLOPE HAS BEEN GRADED BEFORE A FILL SLOPE HAS BEEN CREATED AND AS INDICATED ON THE PLANS. THE SYSTEM IS DESIGNED TO INTERCEPT SILT AND SEDIMENT BEFORE IT REACHES WETLAND SILT AREAS, OR WATERCOURSES. DEPOSITS OF SEDIMENT AND SILT ARE TO BE PERIODICALLY REMOVED FROM THE UPSTREAM SIDE OF THE FENCE. THIS MATERIAL IS TO BE SPREAD AND STABILIZED IN AREAS NOT SUBJECT TO EROSION, OR IN AREAS WHICH ARE NOT TO BE PAVED OR BUILT ON. THE SEDIMENTATION CONTROL SYSTEM IS TO BE REPLACED AS NECESSARY TO PROVIDE PROPER FILTERING ACTION. THE SYSTEM IS TO REMAIN IN PLACE AND BE MAINTAINED TO INSURE EFFICIENT SILTATION CONTROL UNTIL ALL AREAS ABOVE THE FENCE ARE STABILIZED AND VEGETATION HAS BEEN ESTABLISHED.

CONSTRUCTION ENTRANCE - A STONE STABILIZED PAD SHALL BE LOCATED AT POINTS OF VEHICULAR INGRESS AND EGRESS FROM THE CONSTRUCTION SITE TO REDUCE TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THE AREA OF THE ENTRANCE SHOULD BE CLEARED OF ALL VEGETATION, ROOTS AND OTHER OBJECTIONABLE MATERIAL. FILTER FABRIC SHALL BE PLACED ON SUBGRADE PRIOR TO PLACEMENT OF STONE. STONE SHALL BE PLACED TO THE DIMENSIONS SHOWN ON THE PLAN. PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDITIONAL LENGTH, AS CONDITIONS DEMAND, MAY BE REQUIRED TO INSURE THAT THE ENTRANCE FUNCTIONS AS INTENDED. PUBLIC ROADWAYS SHALL BE CLEANED OF DIRT AND DEBRIS AS NECESSARY, OR AS DIRECTED BY THE RESIDENT ENGINEER.

RIPRAP - RIPRAP, IF REQUIRED OR SPECIFIED, IS TO BE INSTALLED AS ENERGY DISSIPATION STRUCTURES. THE RIPRAP IS TO BE INSTALLED BEFORE OUTLET STRUCTURES ARE ACTIVATED, AND ALL ADJACENT AREAS ARE TO BE IMMEDIATELY SEEDED IF IN SEASON, OTHERWISE THE SOIL IS TO BE STABILIZED BY OTHER METHODS.

IN ALL AREAS, REMOVAL OF TREES, BUSHES AND OTHER VEGETATION, AND DISTURBANCE OF THE SOIL, IS TO BE KEPT TO AN ABSOLUTE MINIMUM WHILE ALLOWING PROPER DEVELOPMENT OF THE SITE.

DURING CONSTRUCTION, AS SMALL AN AREA OF SOIL AS POSSIBLE SHOULD BE EXPOSED FOR AS SHORT A TIME AS POSSIBLE. AFTER CONSTRUCTION, GRADE, RESPREAD TOPSOIL, AND STABILIZE SOIL BY SEEDING AND MULCHING SO AS TO

GRASSED SWALES - ALL GRASSED SWALES SHALL BE STABILIZED WITH SOIL EROSION CONTROL MATS IMMEDIATELY AFTER GRADING. THE SWALES SHALL BE IMMEDIATELY SODDED OR SEEDED DEPENDING ON THE SEASON.

TAP PROJECT NO. 111-126

AIR LINE TRAIL RECONSTRUCTION AND ACCESSIBILITY PROJECT TOWNS OF PUTNAM AND POMFRET, CT

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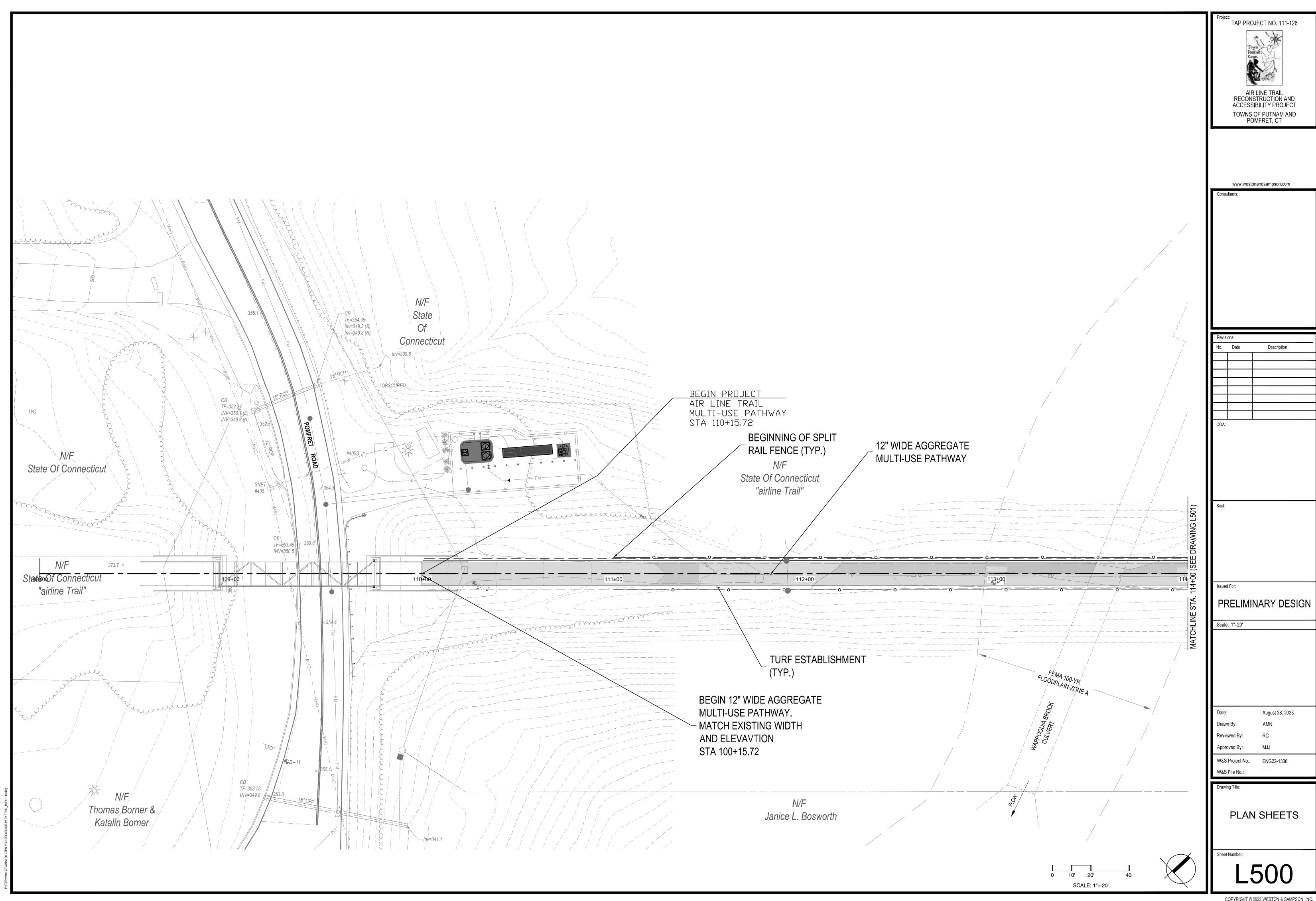
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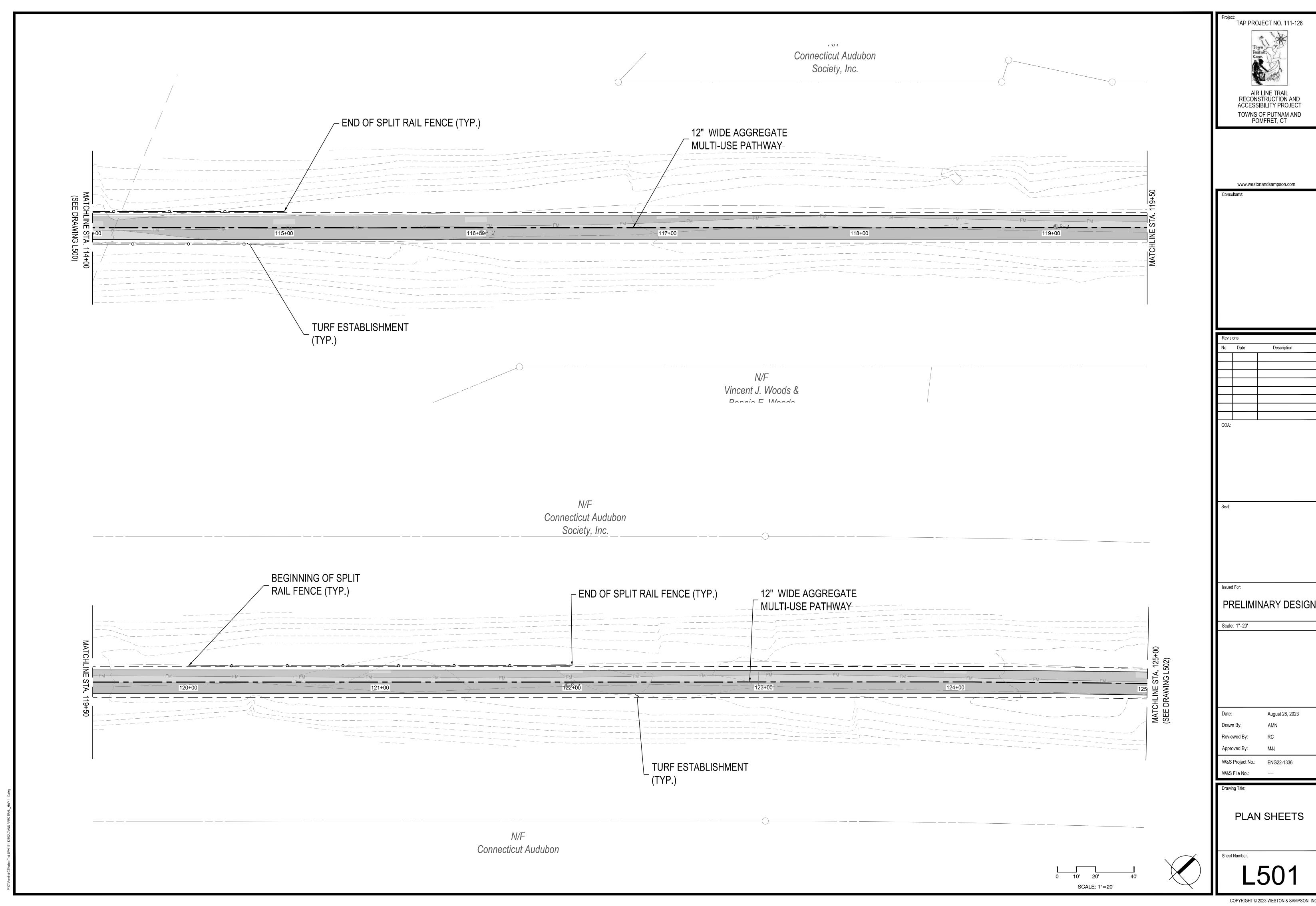
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Drawn By:	NSD	
Reviewed By:	RC	
Approved By:	LAS	

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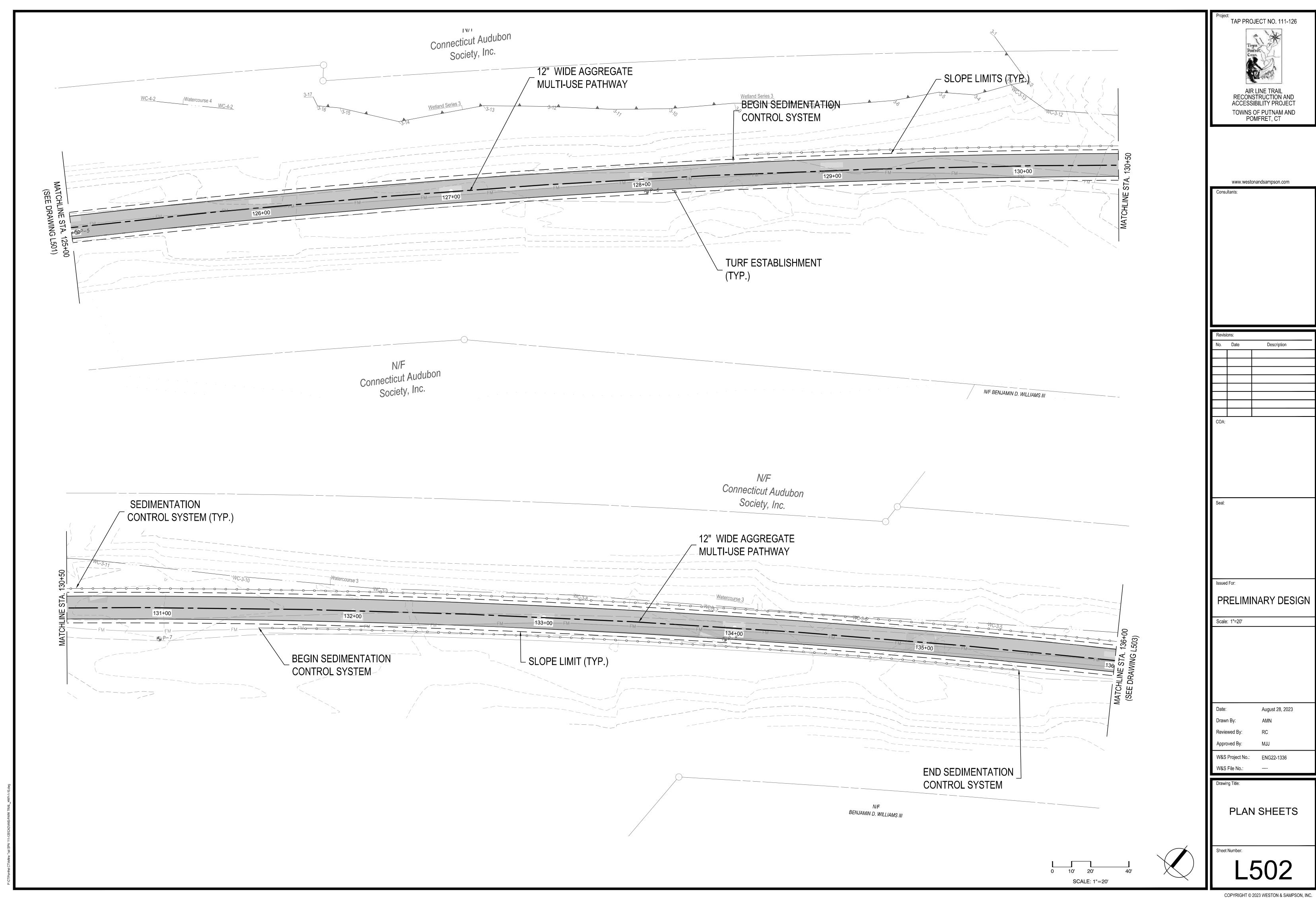
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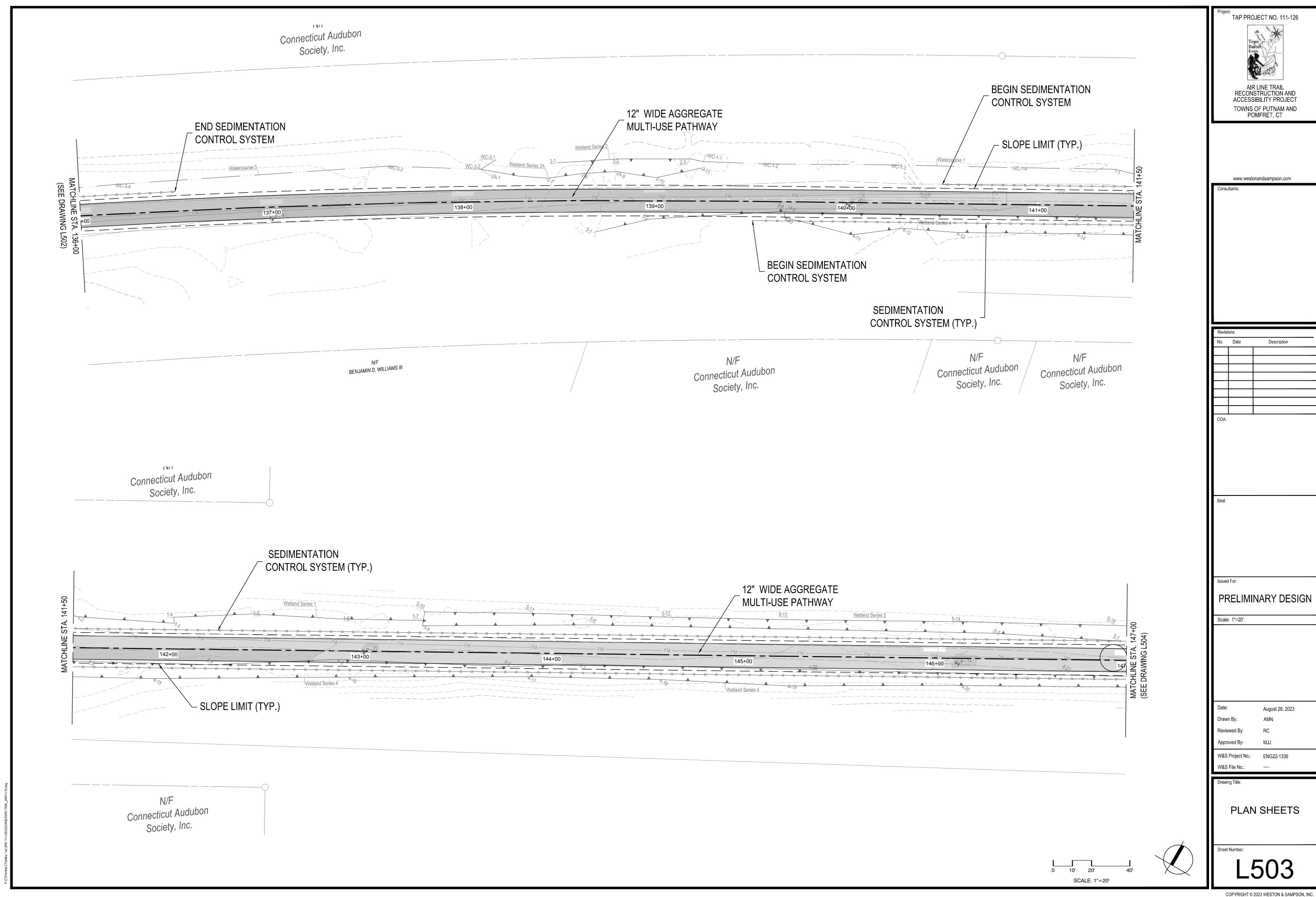
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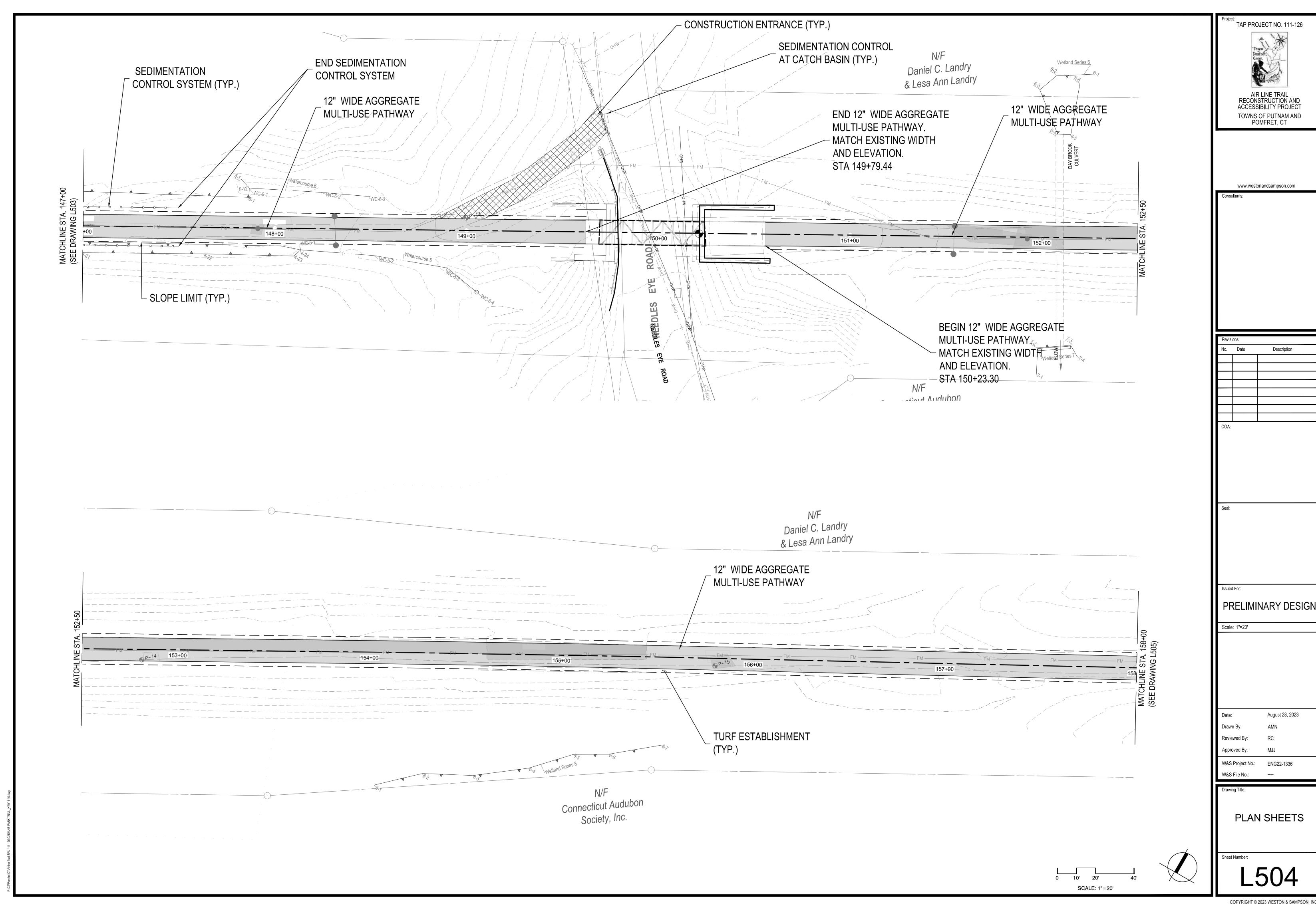




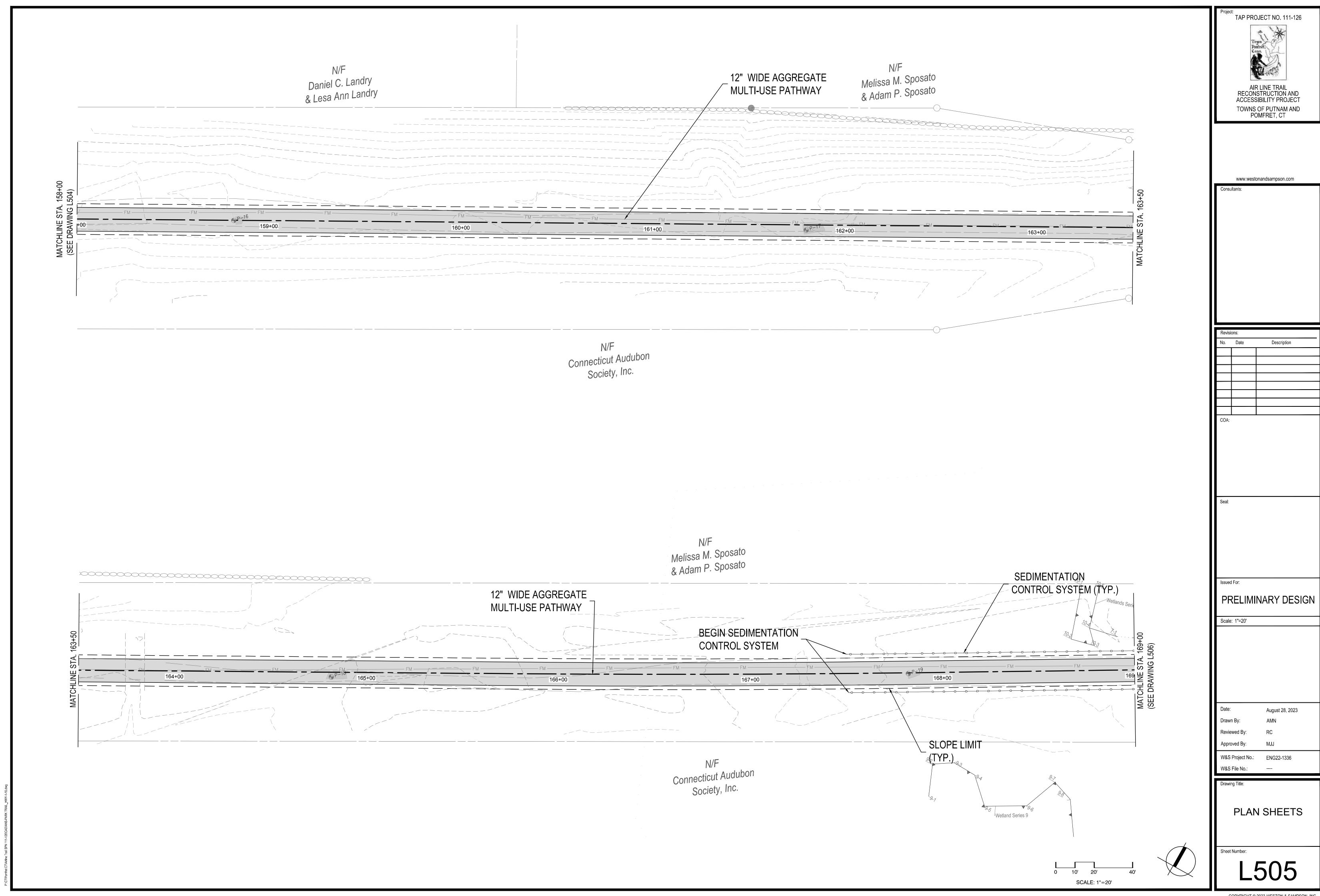
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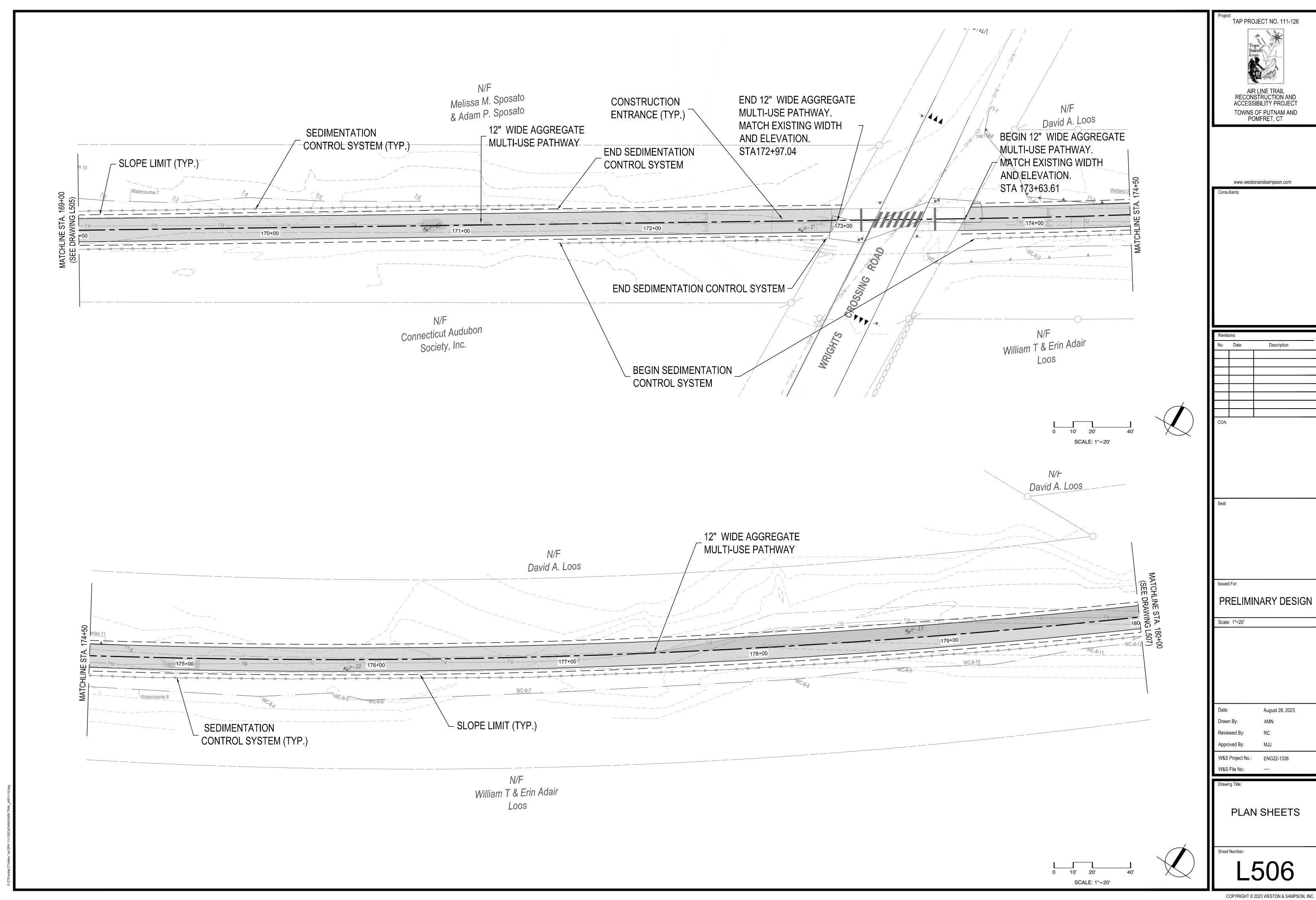


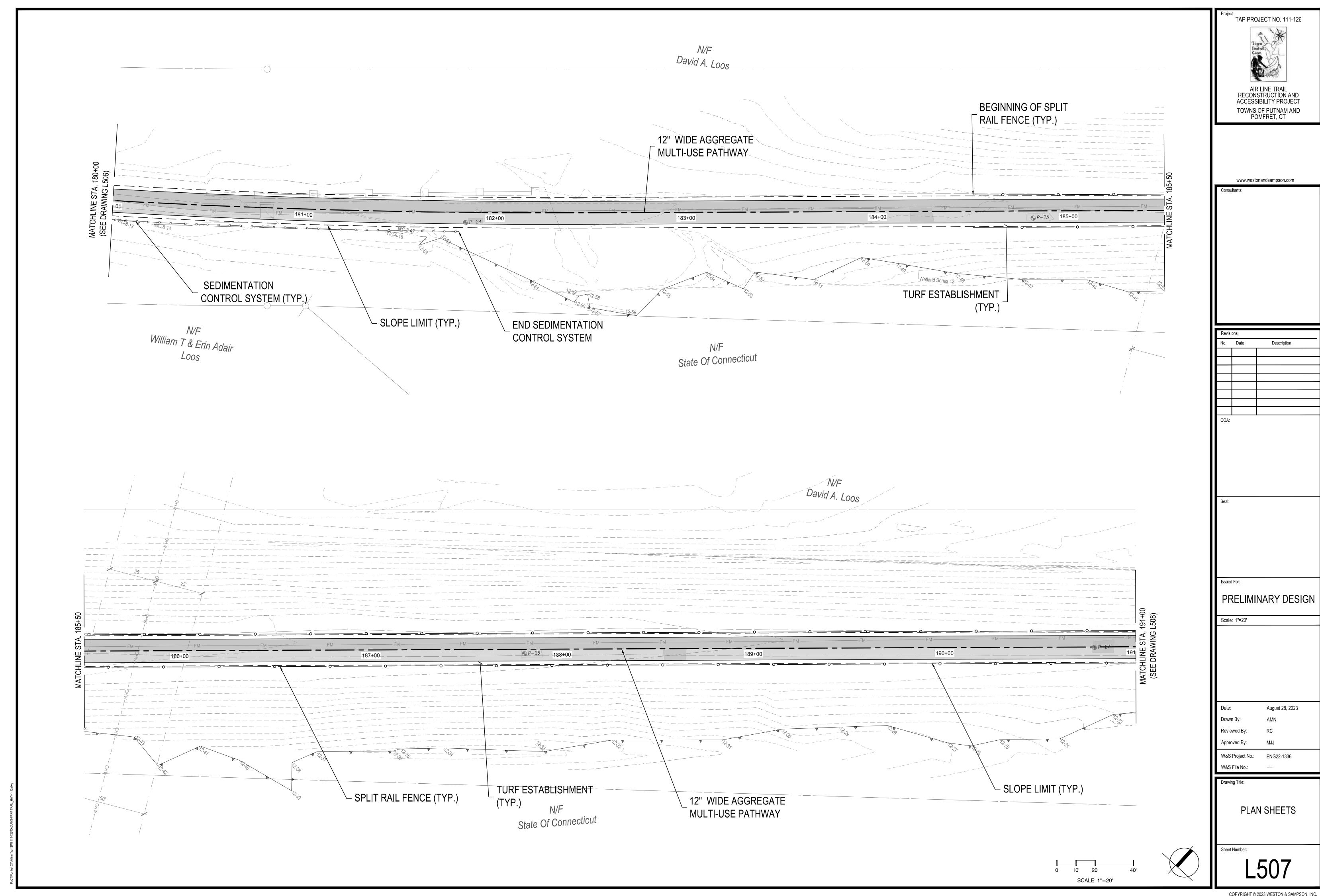


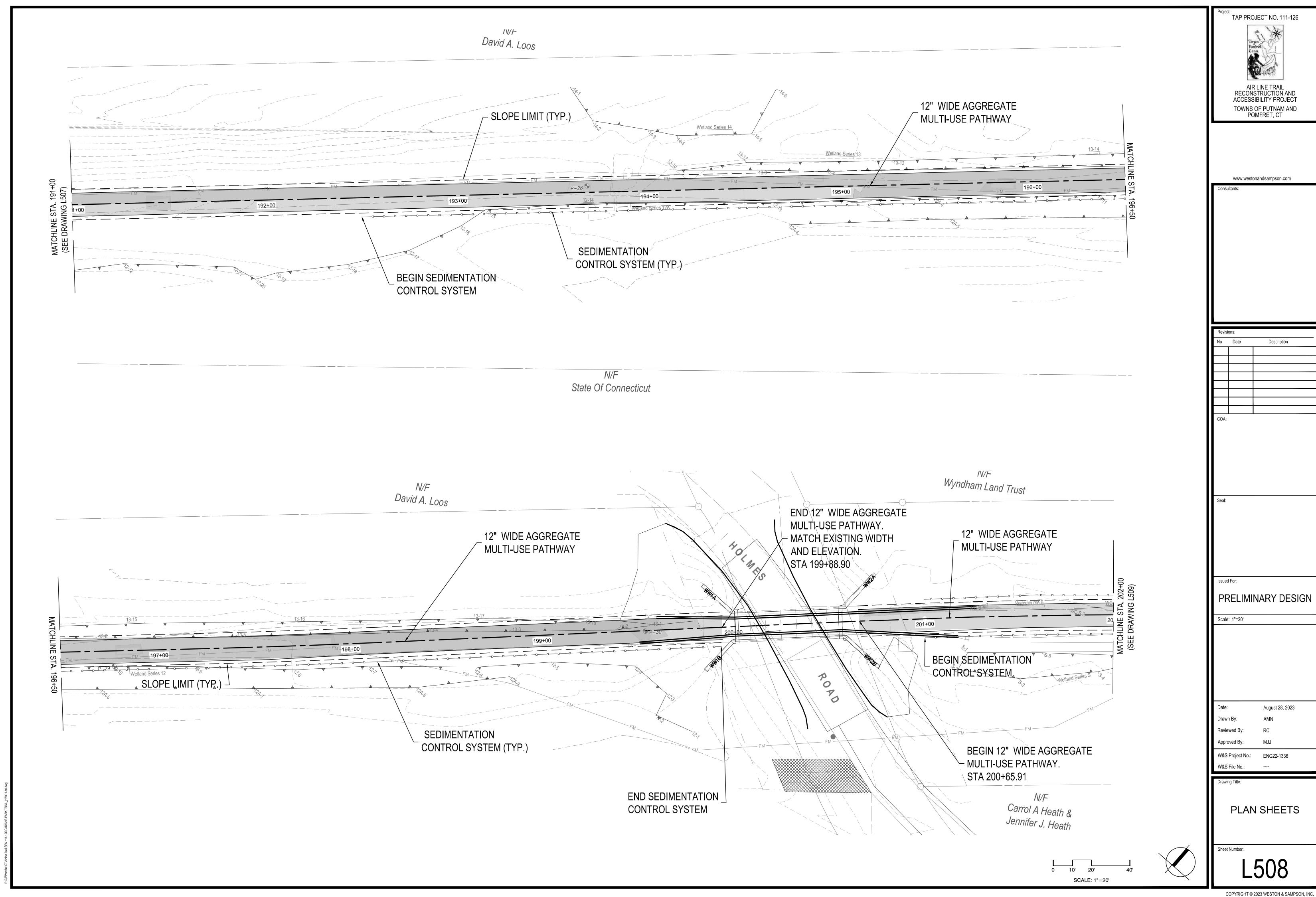


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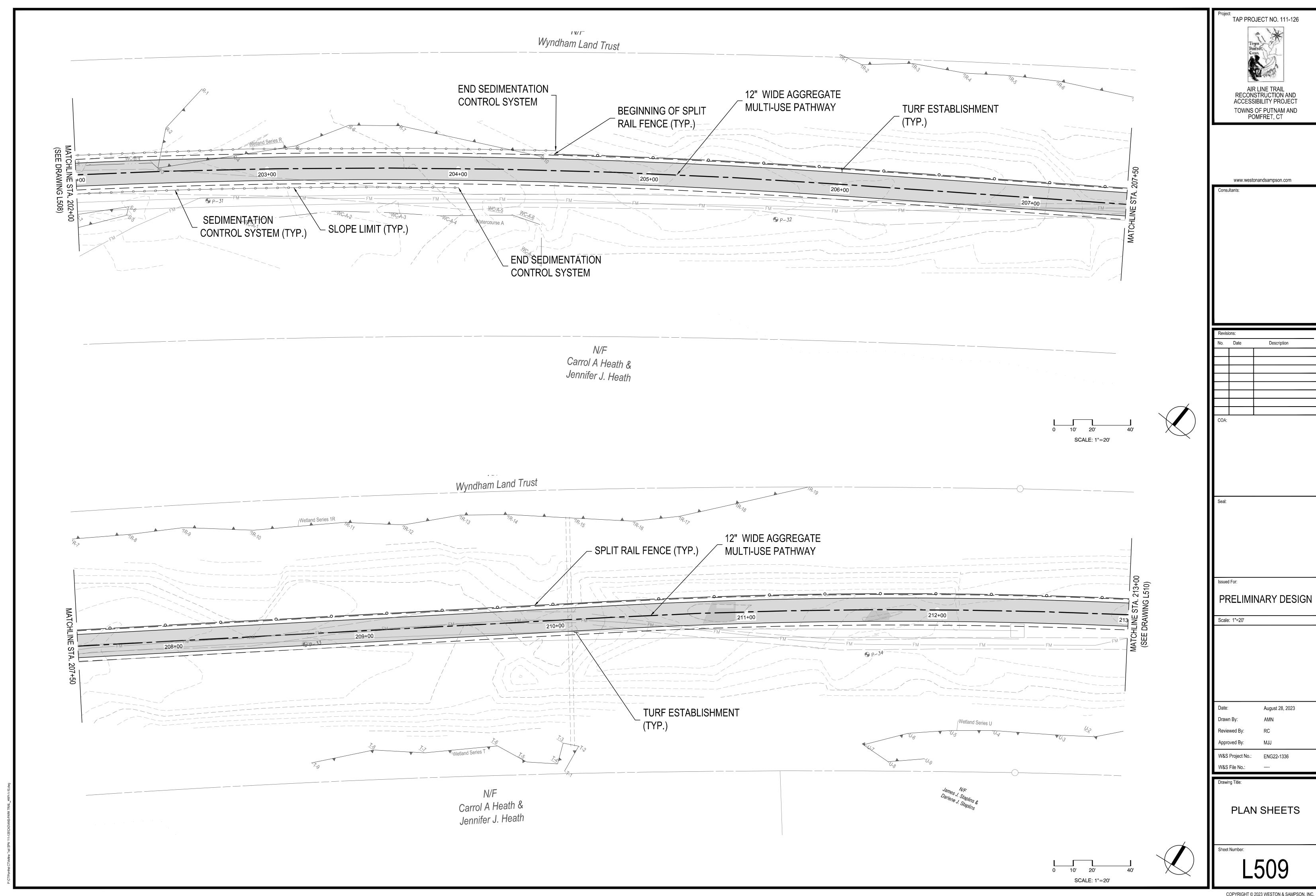


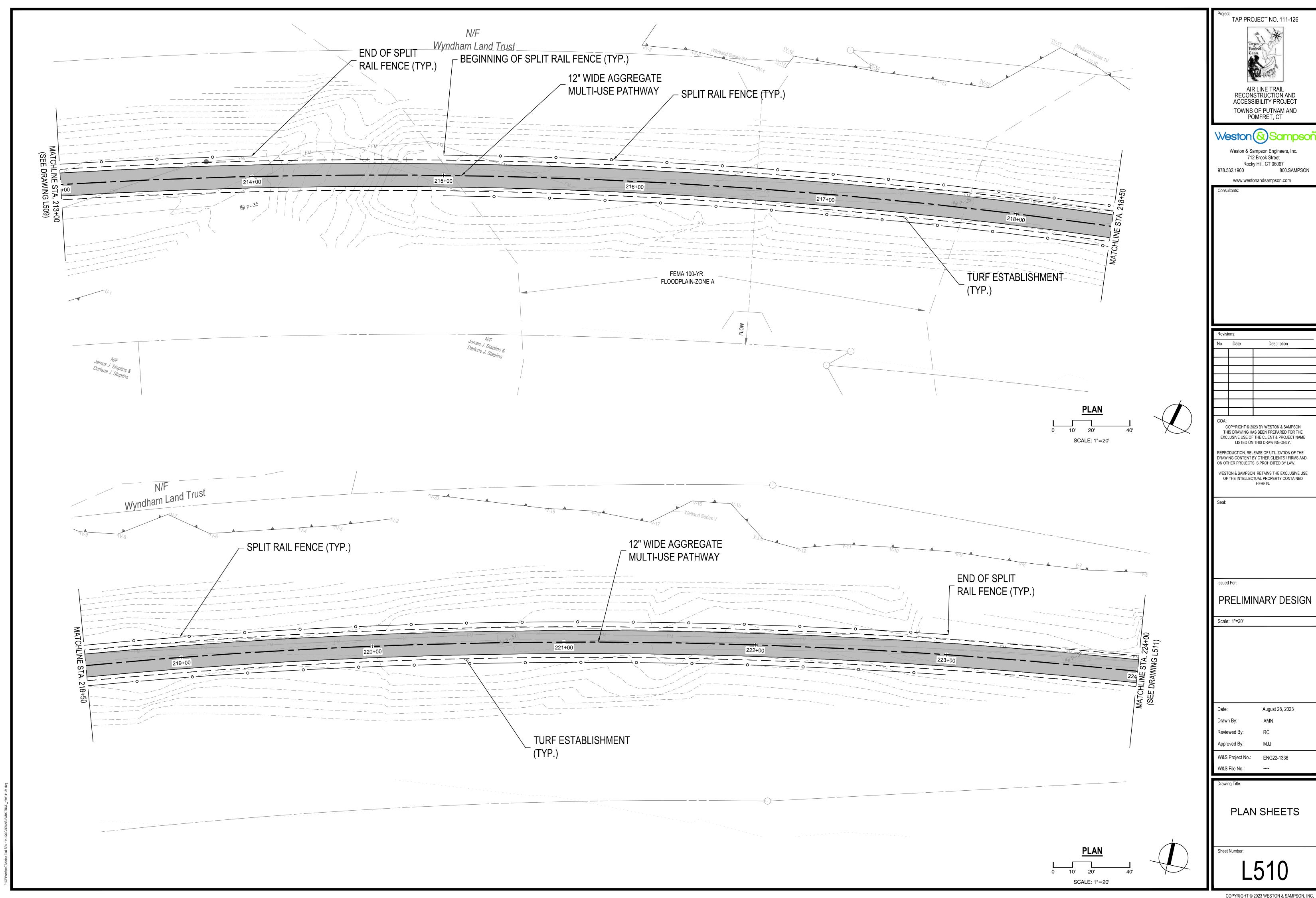


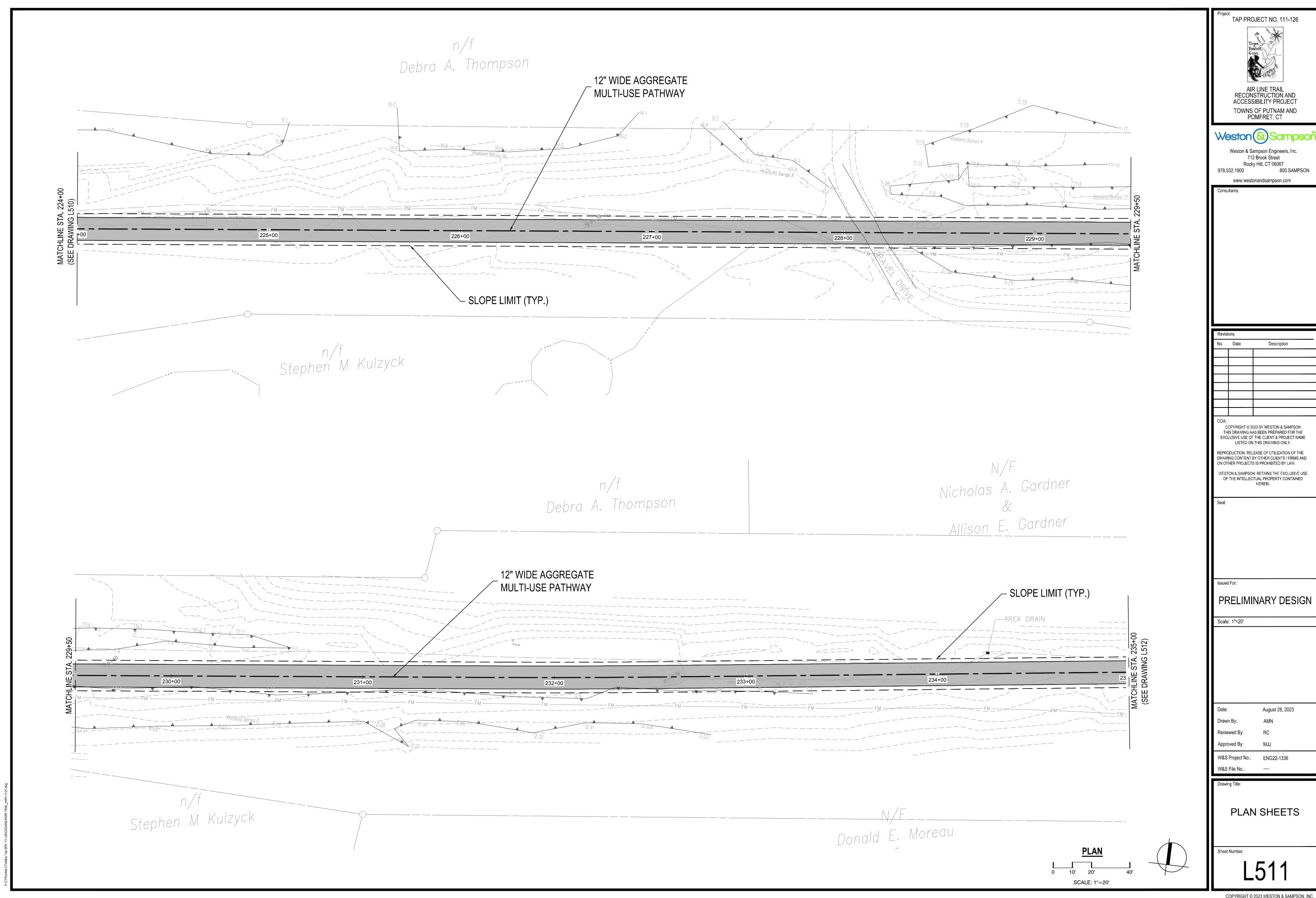


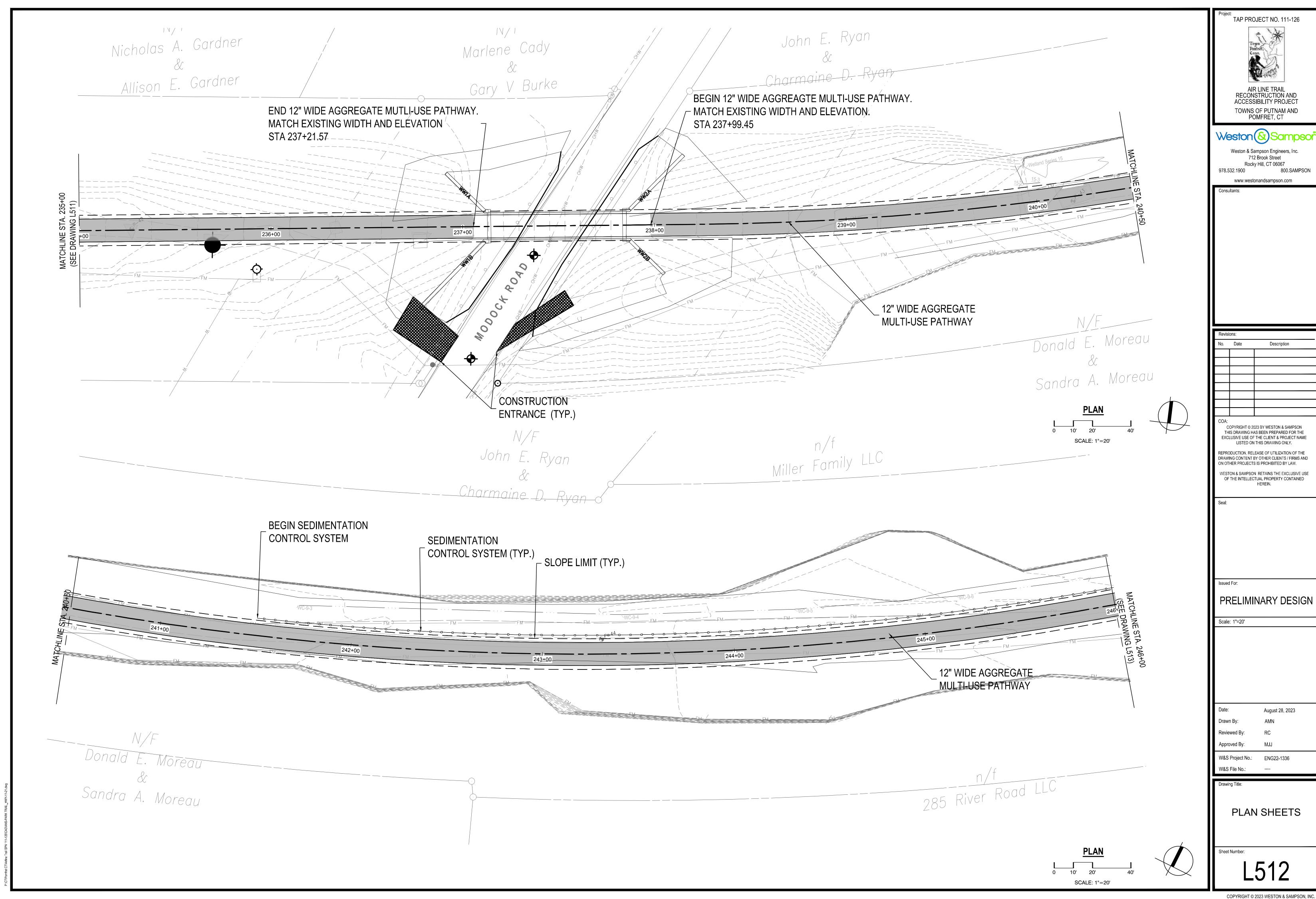


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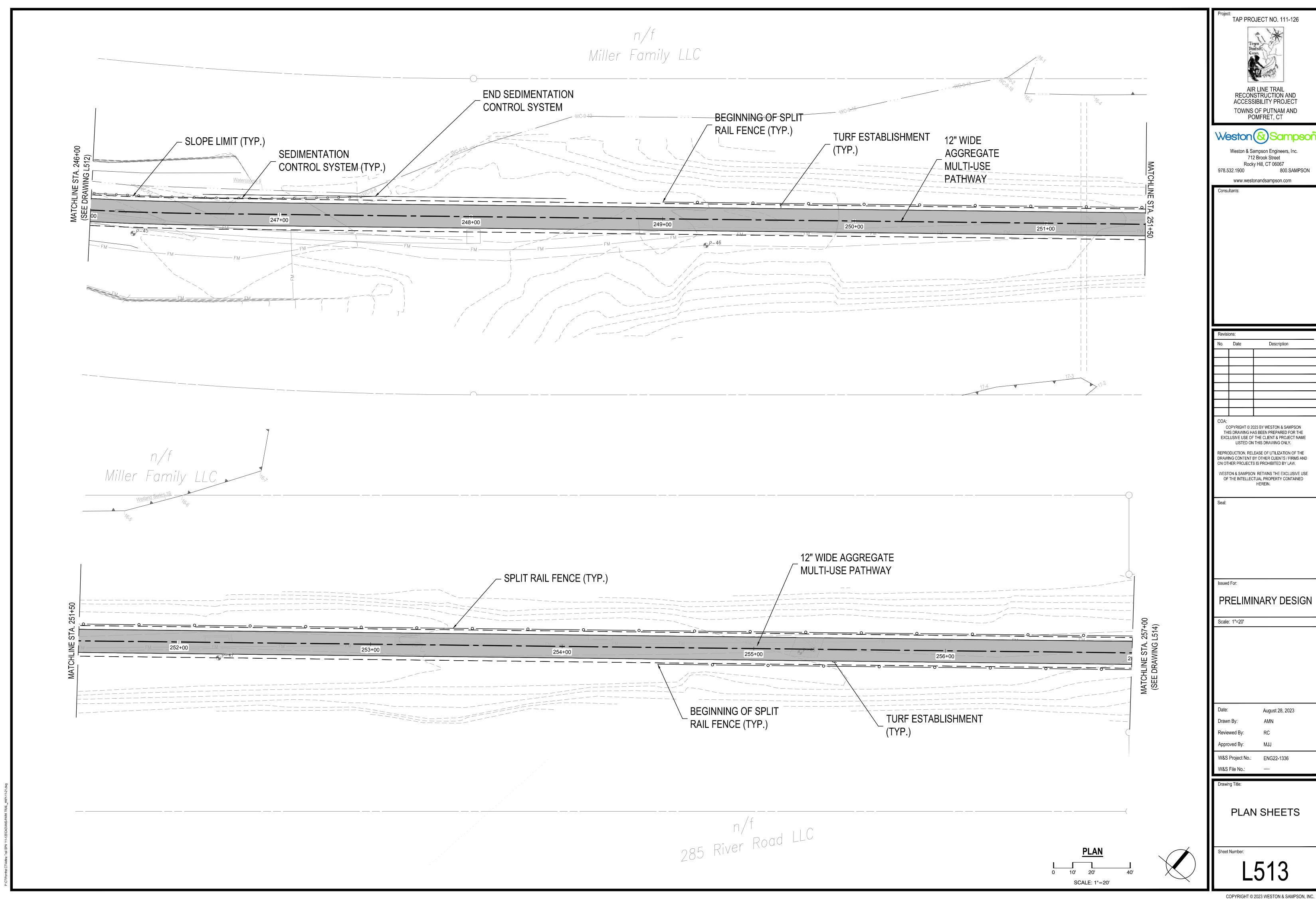


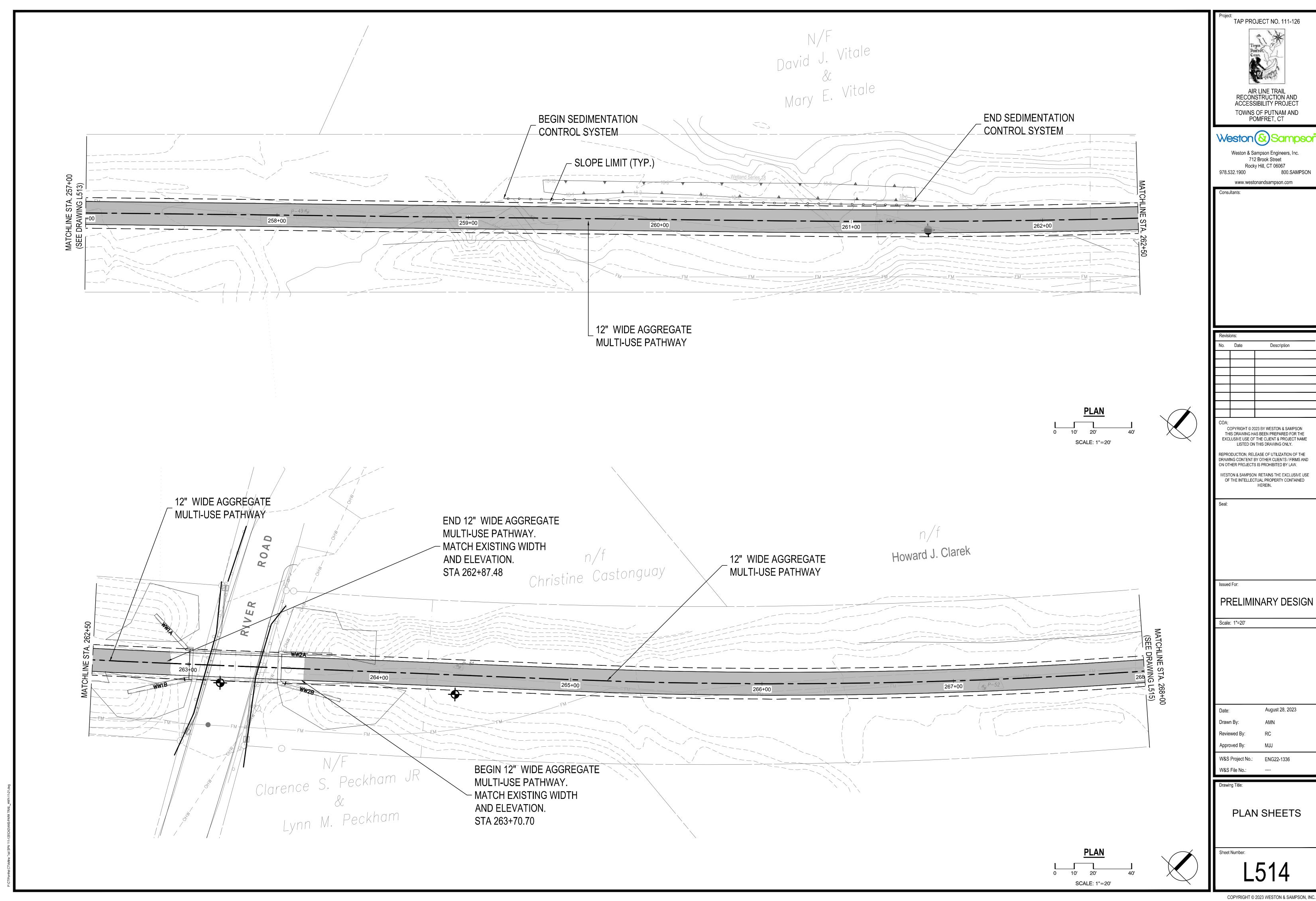


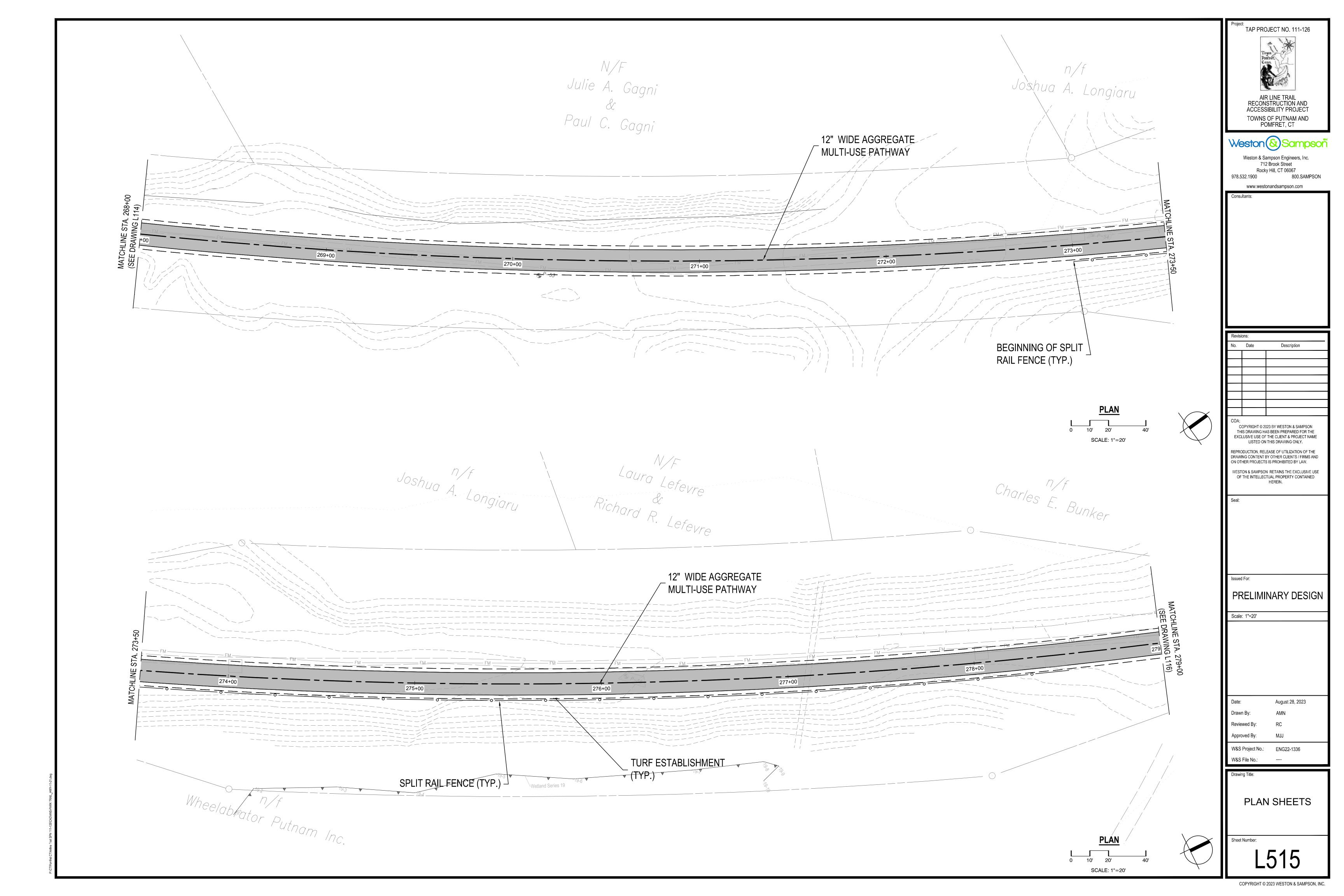


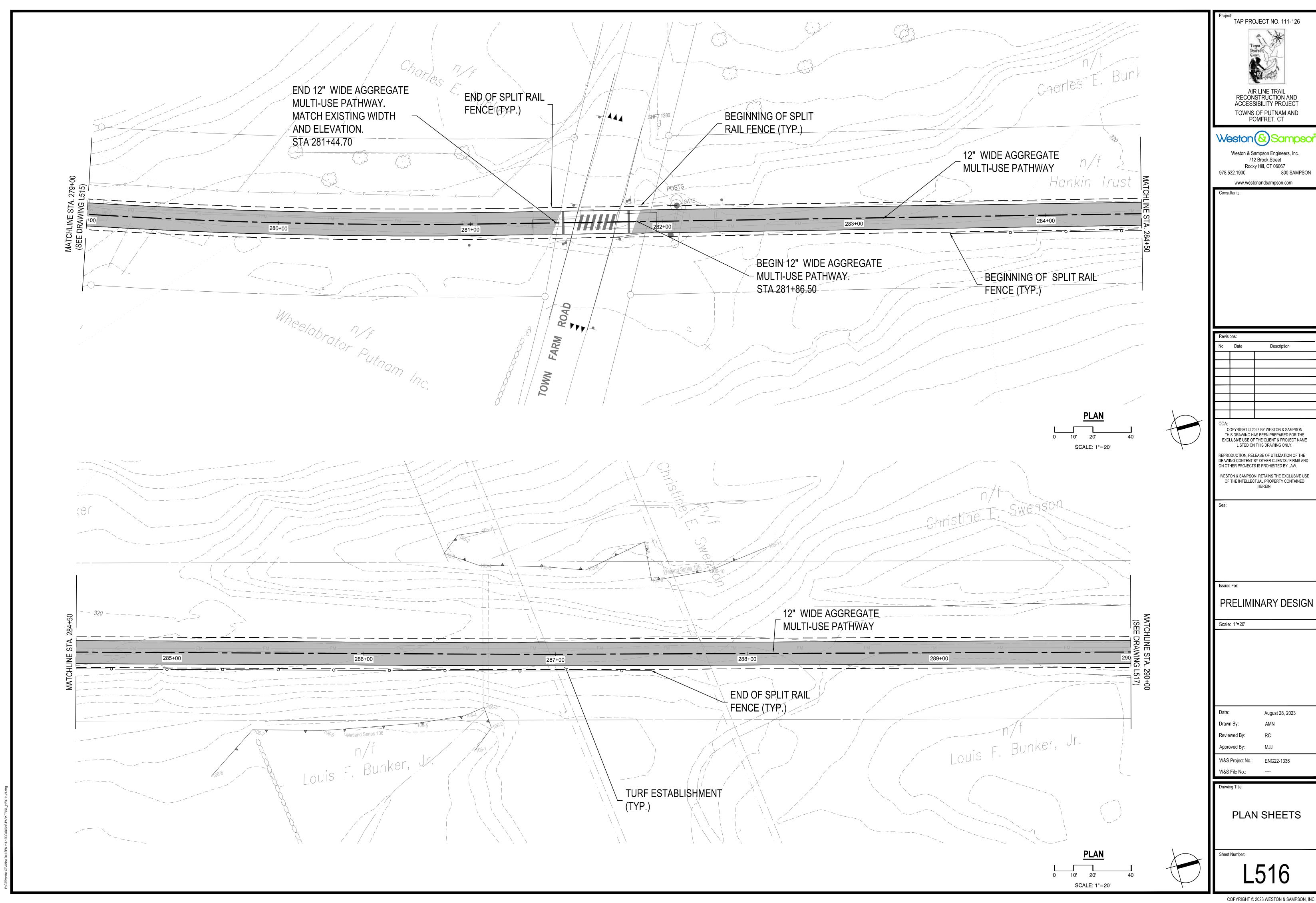


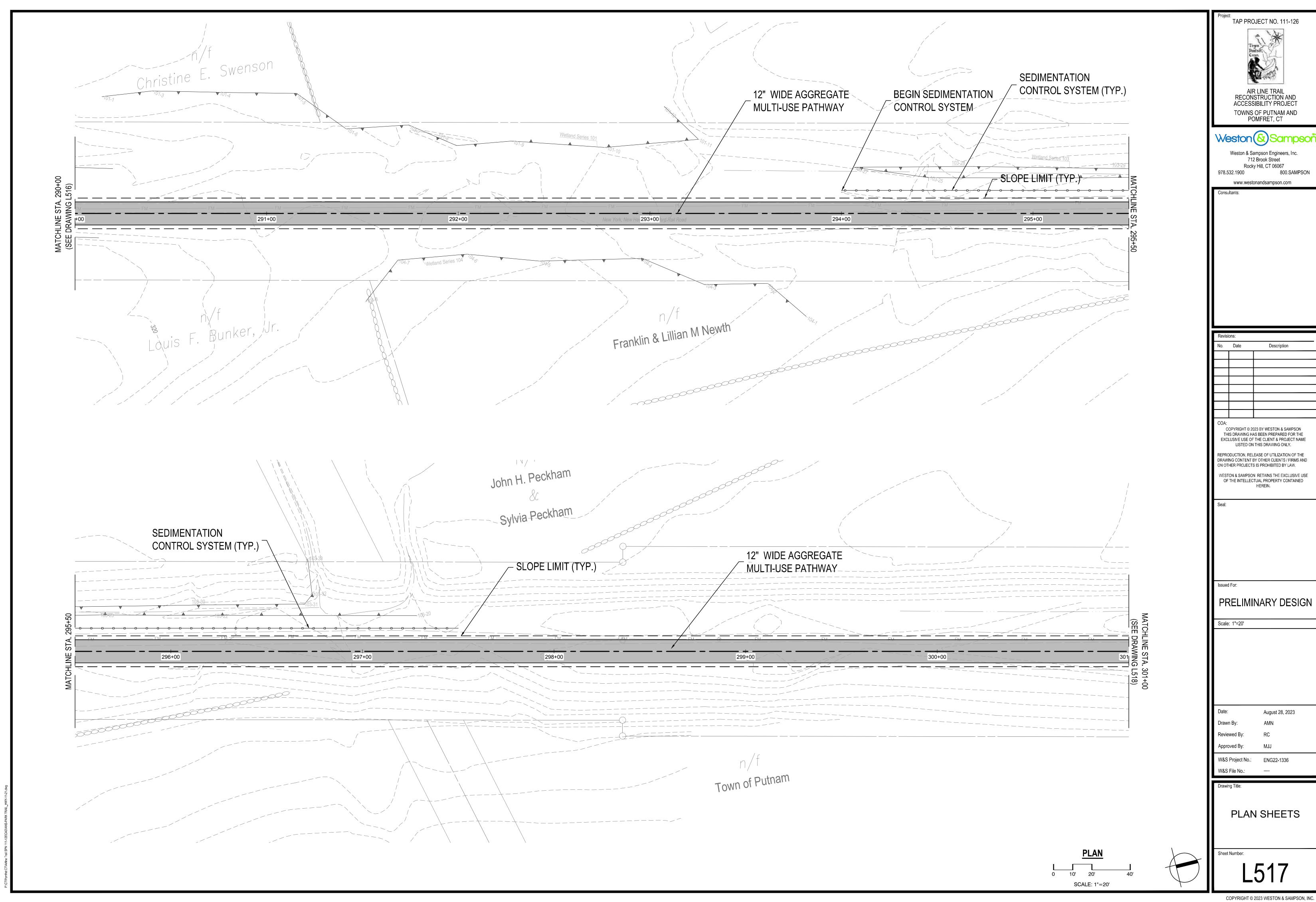
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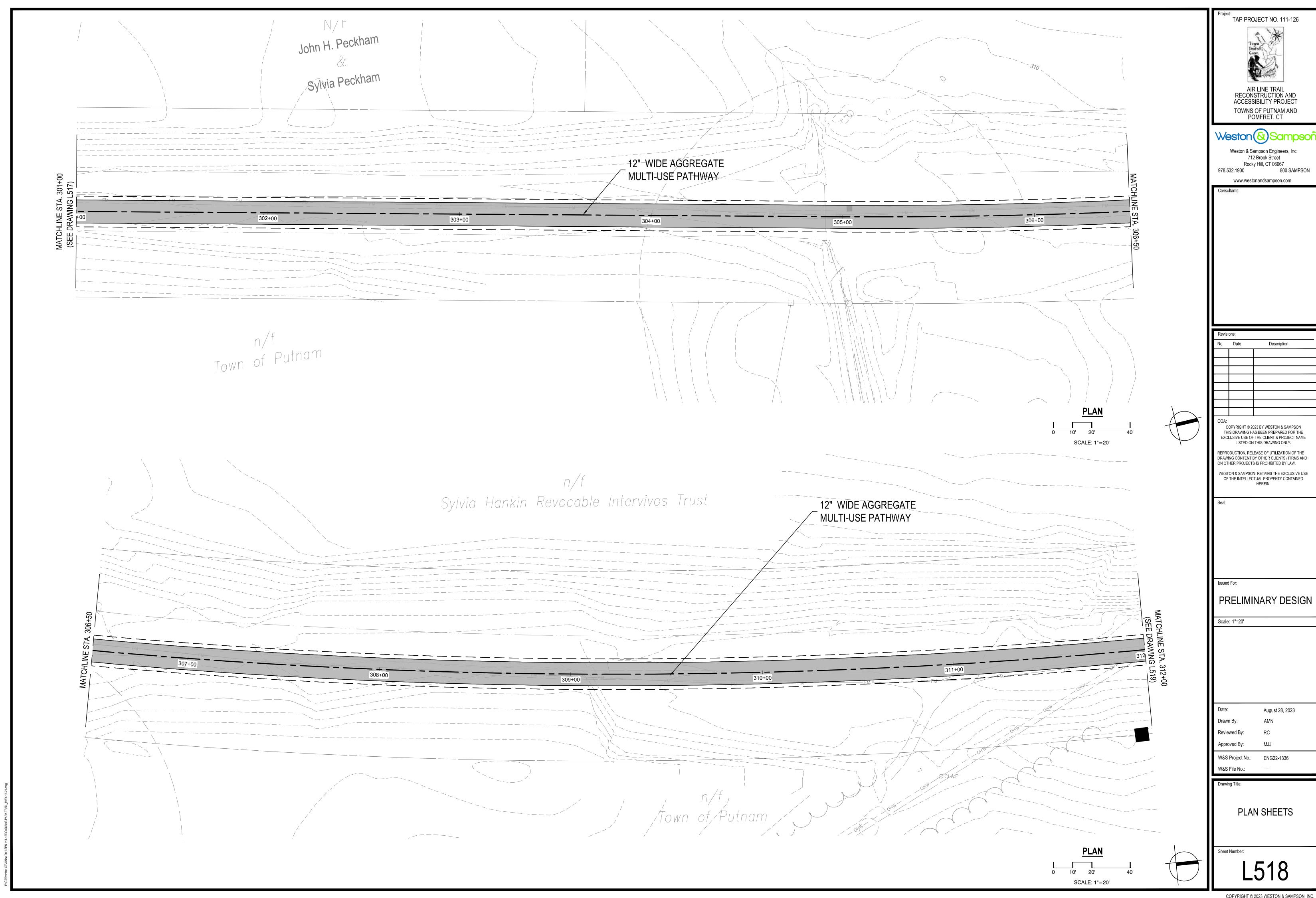


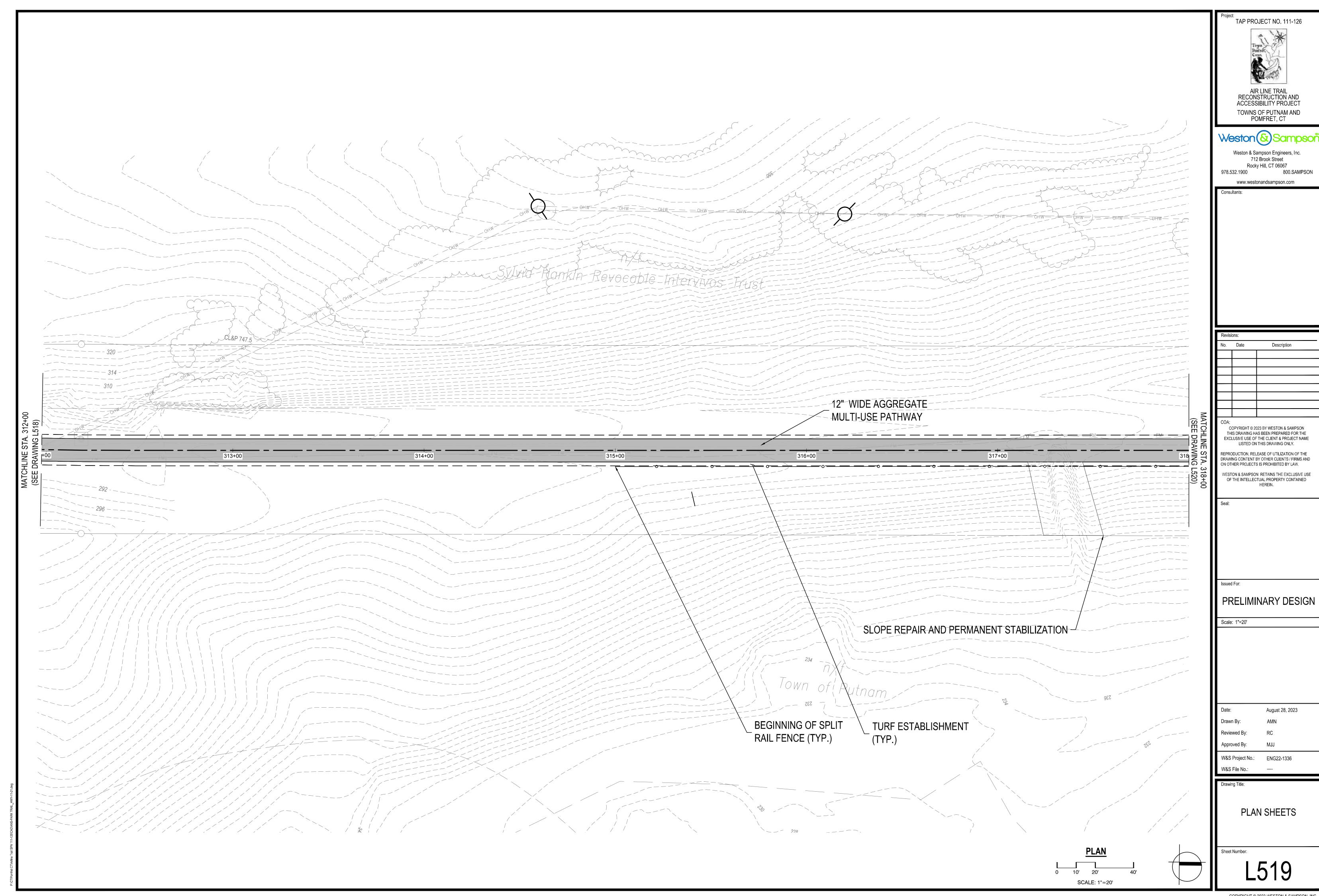


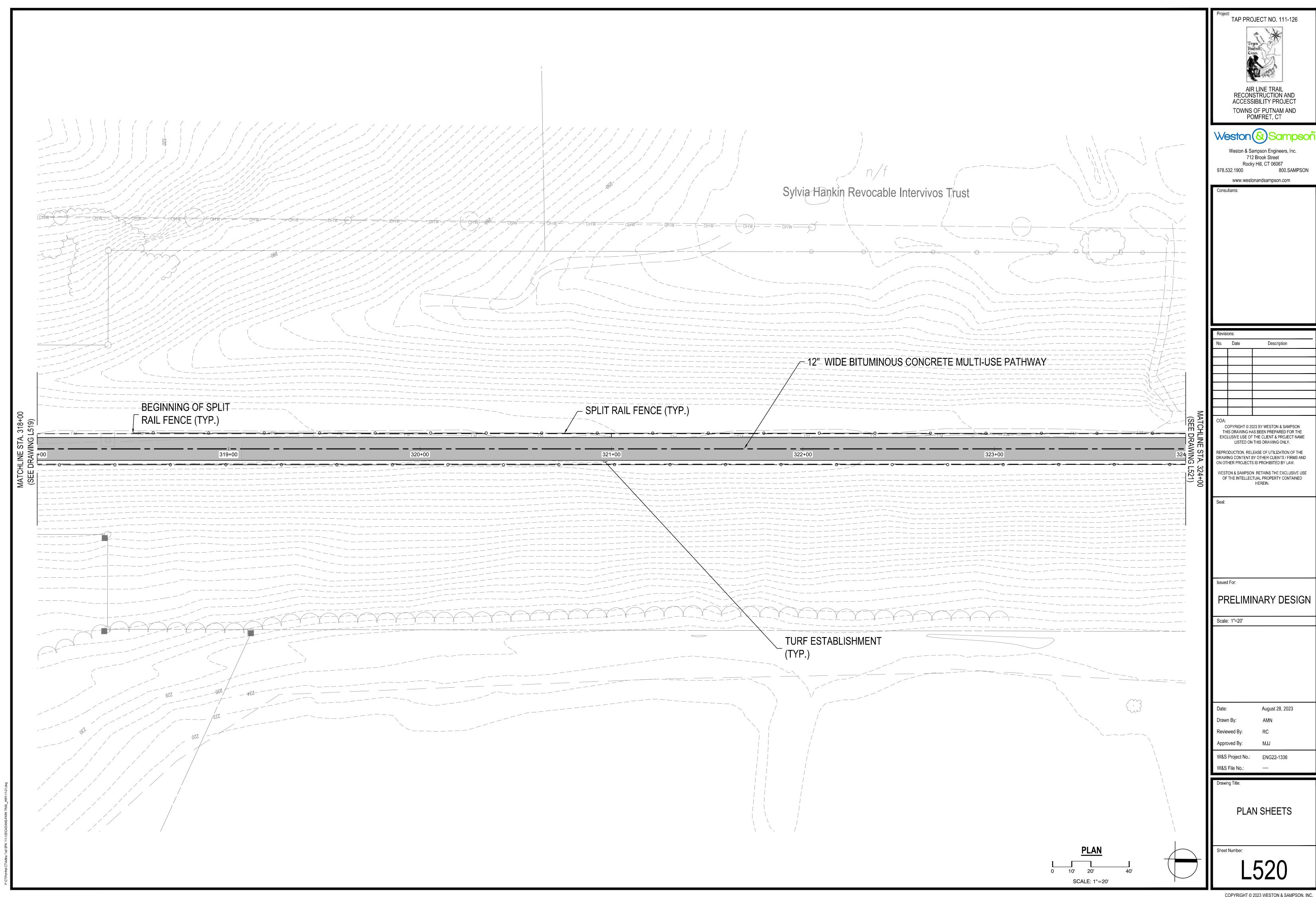


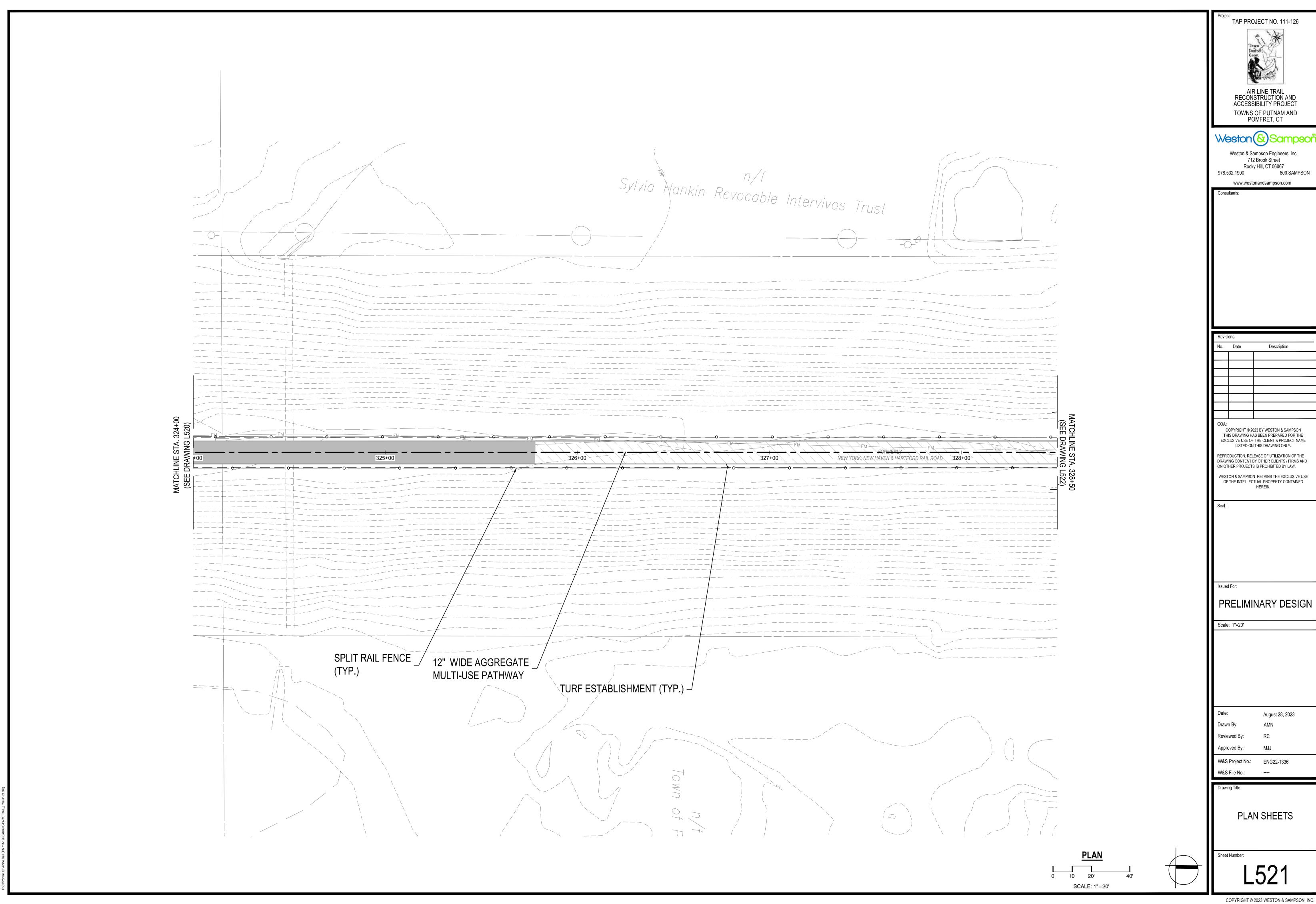




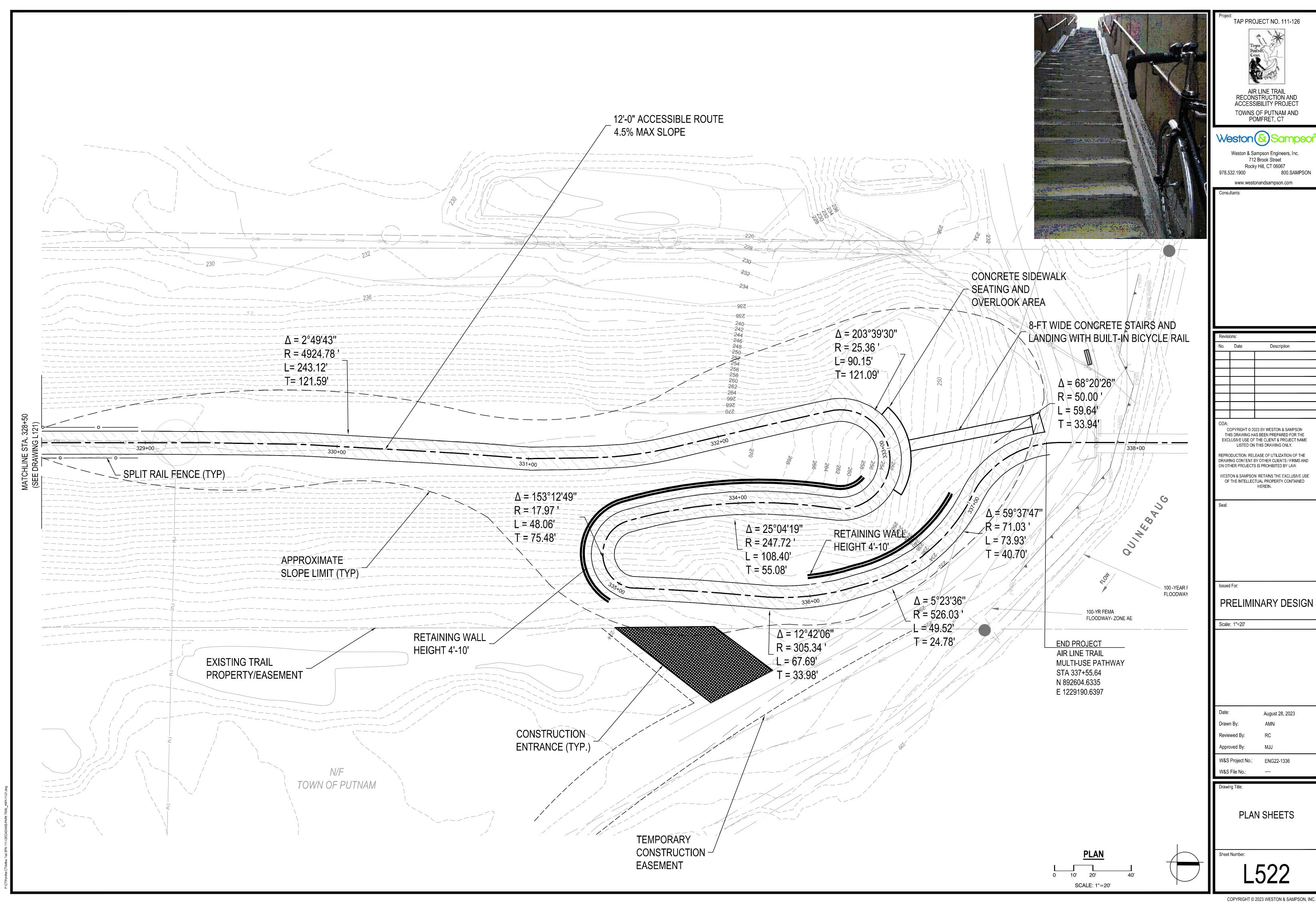


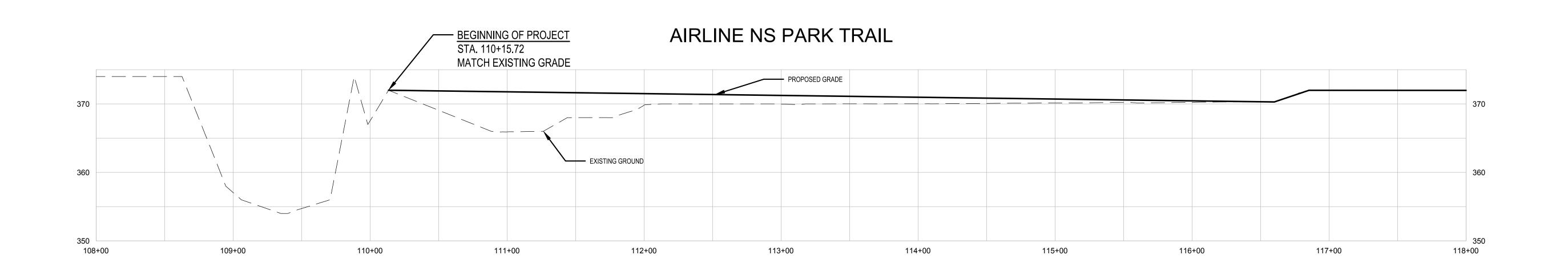


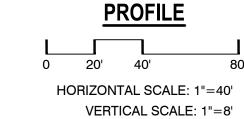


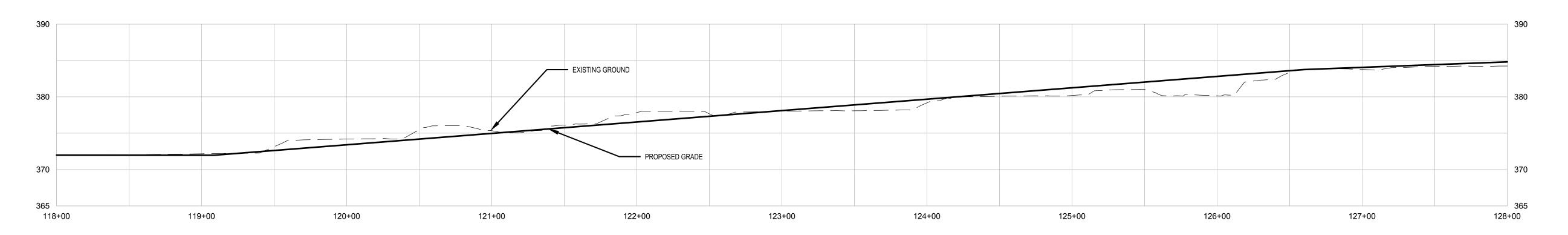


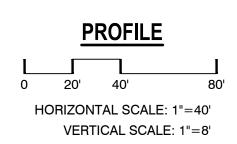
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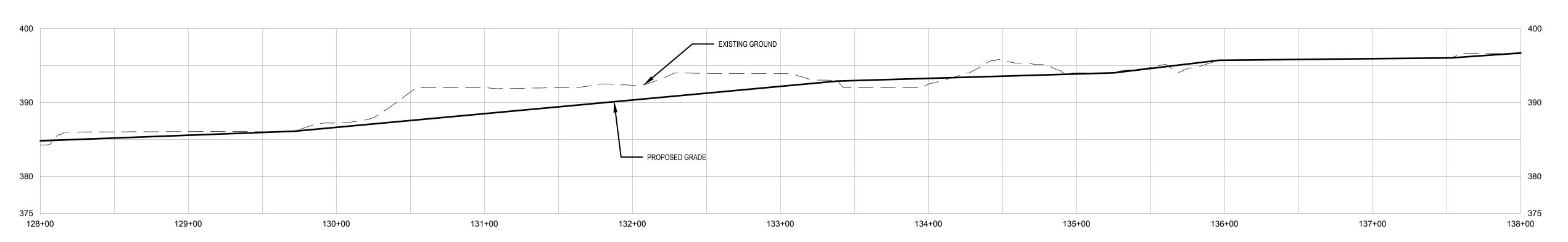
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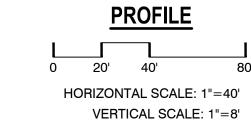
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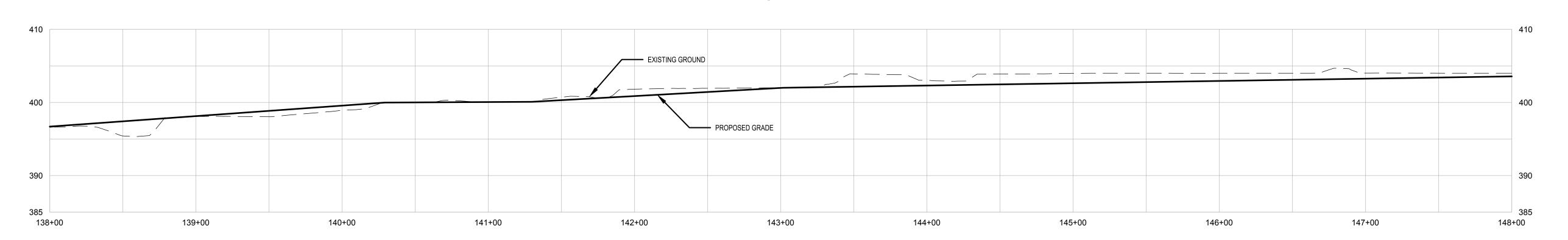
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#### AIRLINE NS PARK TRAIL



PROFILE

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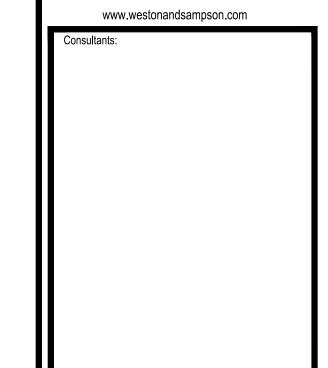
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VERTICAL SCALE: 1"=8'



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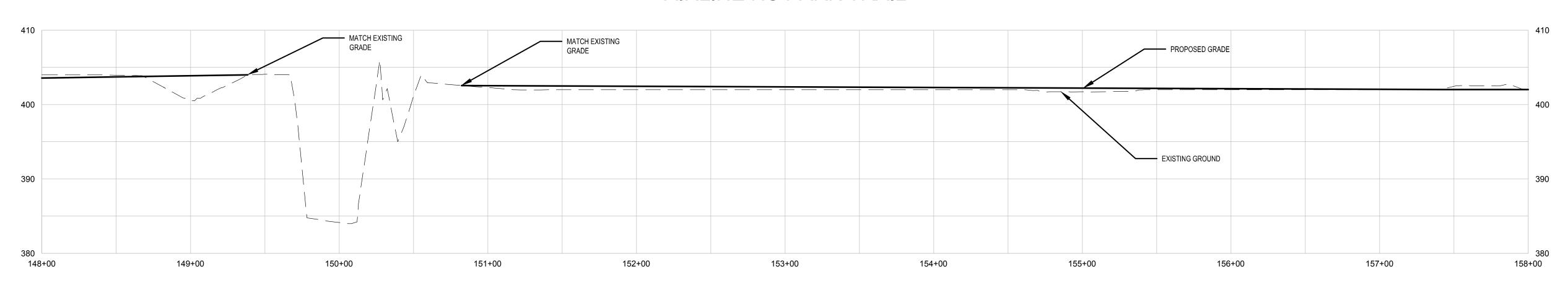
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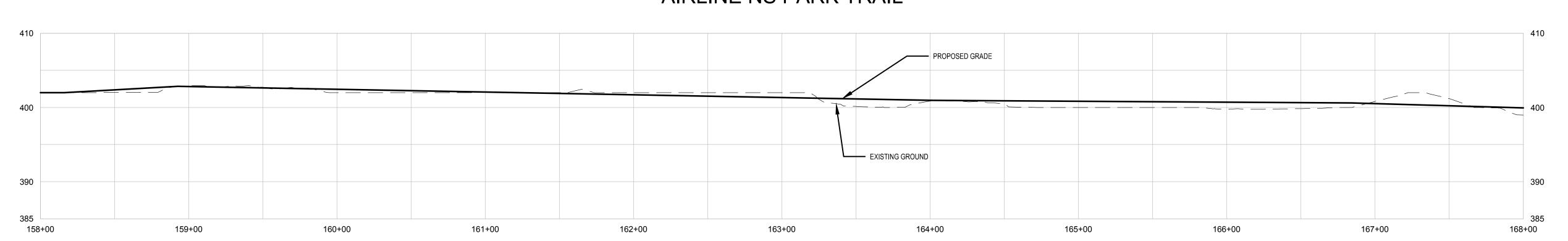
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HORIZONTAL SCALE: 1"=40' VERTICAL SCALE: 1"=8'

#### AIRLINE NS PARK TRAIL



**PROFILE** HORIZONTAL SCALE: 1"=40' VERTICAL SCALE: 1"=8'

TAP PROJECT NO. 111-126 TOWNS OF PUTNAM AND POMFRET, CT

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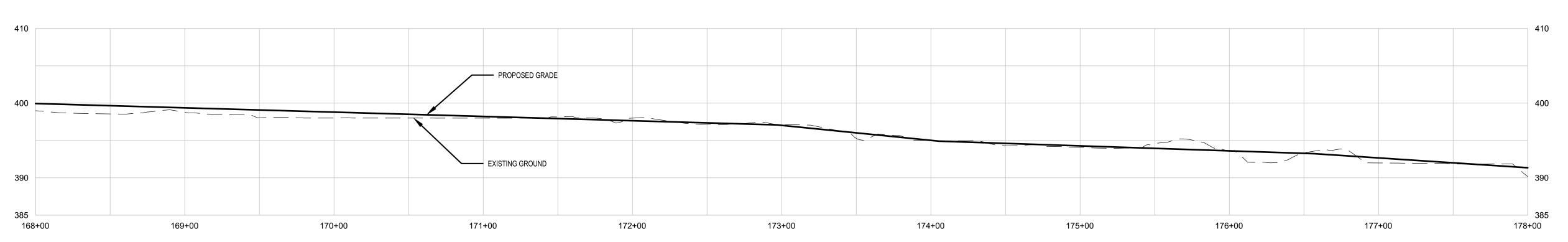
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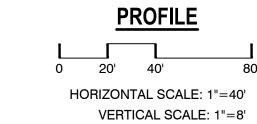
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August 28, 2023 Drawn By:

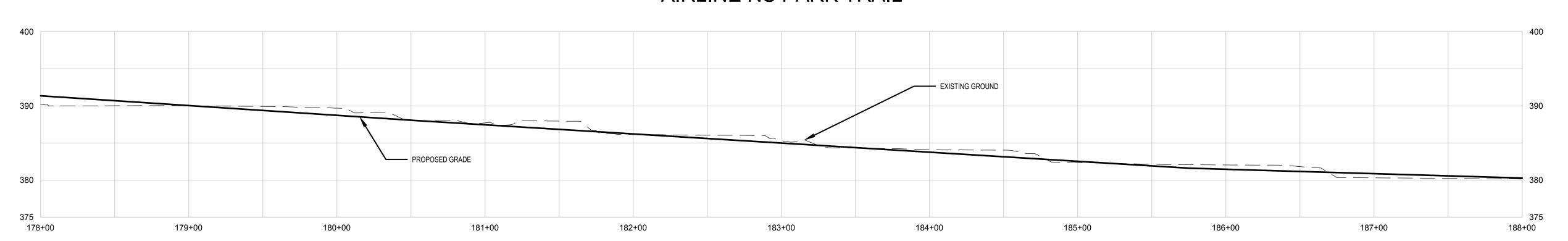
W&S Project No.: ENG22-1336

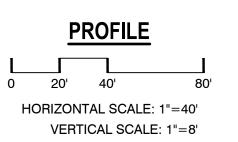
**EXISTING PROFILE** STA. 148+00 TO 168+00





## AIRLINE NS PARK TRAIL





TAP PROJECT NO. 111-126

Town
Pointret

AIR LINE TRAIL

TOWNS OF PUTNAM AND
POMFRET, CT

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Date: August 28, 2023

Drawn By: AMN

Reviewed By: RC

Approved By: MJJ

W&S Project No.: ENG22-1336
W&S File No.: ----

Drawing Title:

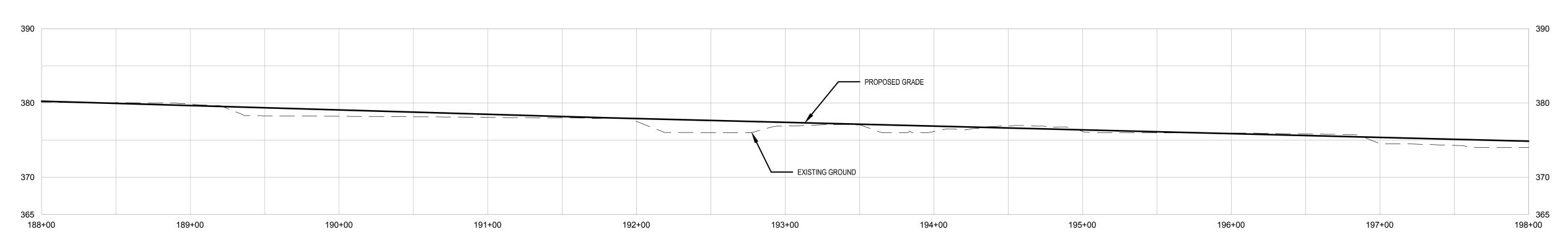
EXISTING PROFILE STA. 168+00 TO 188+00

Sheet Number:

L604

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Wwse03.localWSEIProjects/CTIPomfret CTAtrine Trail SPN 111-128/CADITest Profit



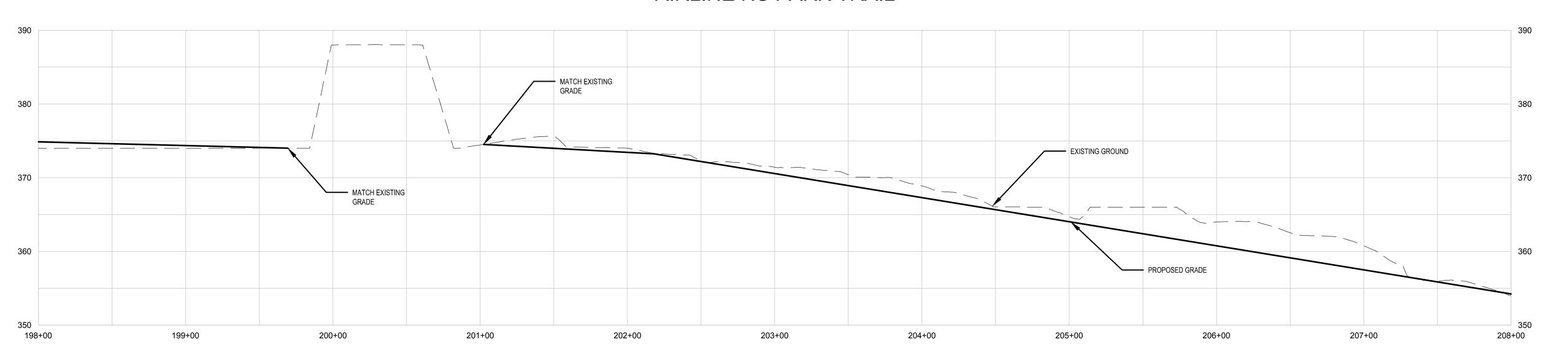
PROFILE

0 20' 40' 80

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

### AIRLINE NS PARK TRAIL



PROFILE

20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

Project:

TAP PROJECT NO. 111-126

Town
Pointet

AIR LINE TRAIL

TOWNS OF PUTNAM AND
POMFRET, CT

Weston & Sampson

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PRELIMINARY DESIGN

Scale: AS NOTED

Date: August 28, 2023

Drawn By: AMN

Reviewed By: RC

Approved By: MJJ

W&S Project No.: ENG22-1336
W&S File No.: ----

Drawing Ti

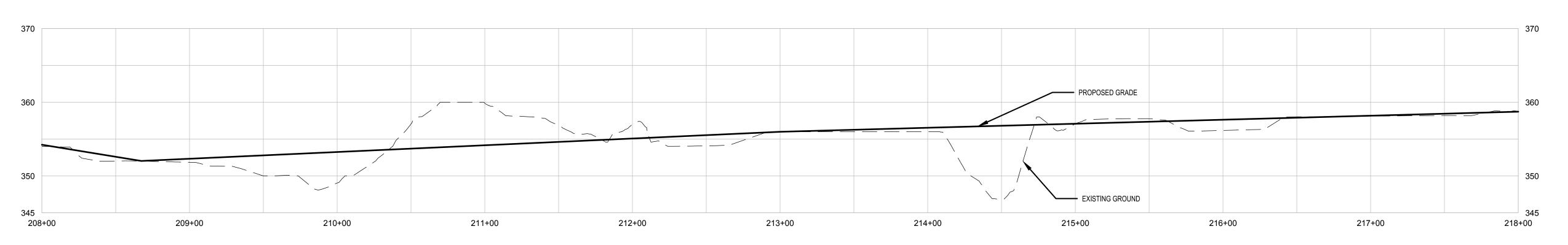
EXISTING PROFILE STA. 188+00 TO 208+00

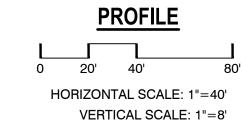
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L605

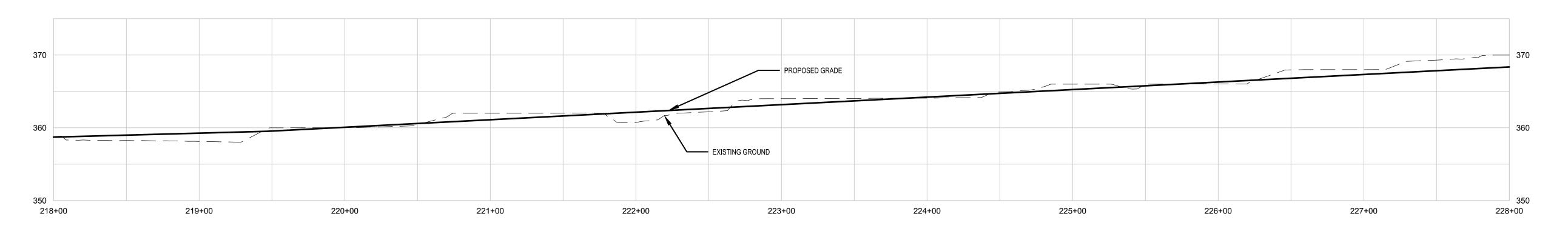
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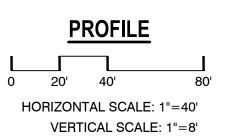
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### AIRLINE NS PARK TRAIL



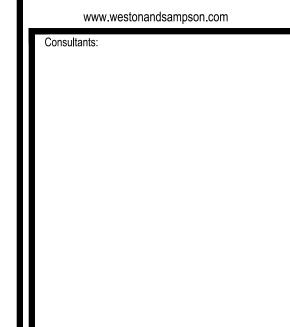


TAP PROJECT NO. 111-126

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Scale: AS NOTED

Date: August 28, 2023

Drawn By: AMN

Reviewed By: RC

Approved By: MJJ

W&S Project No.: ENG22-1336
W&S File No.: ----

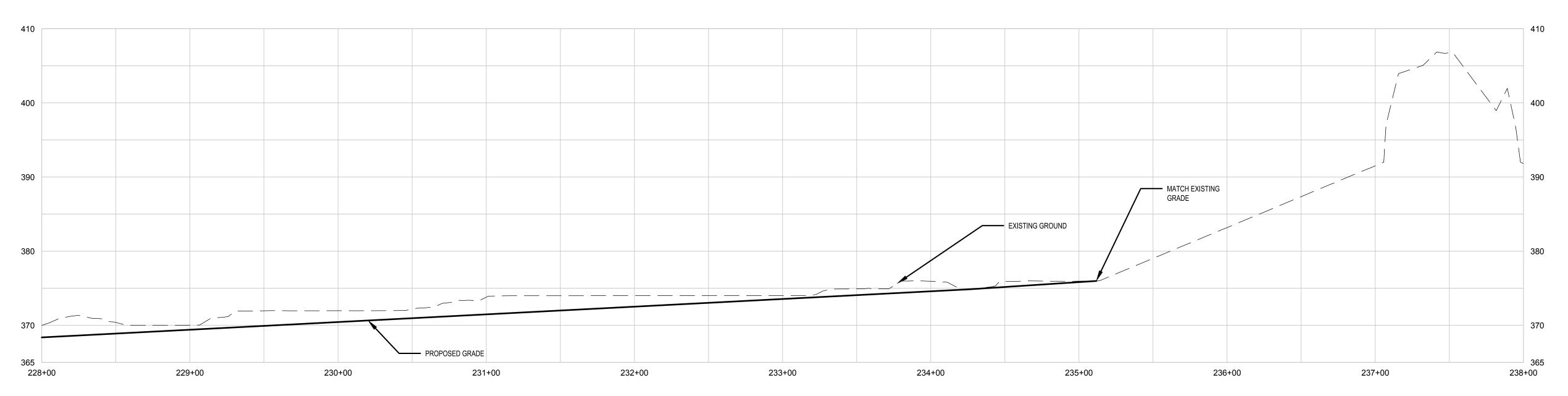
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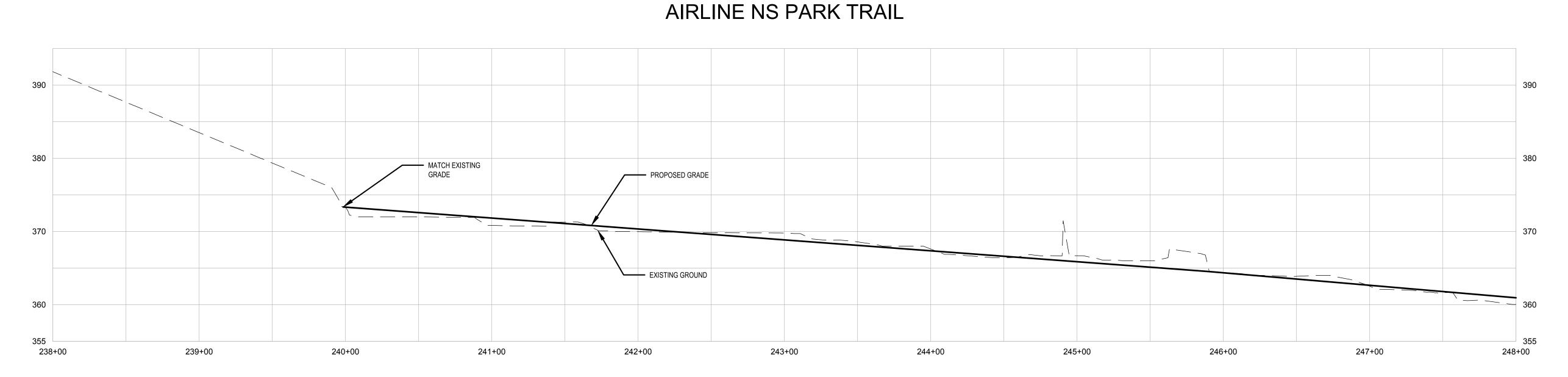
EXISTING PROFILE STA. 208+00 TO 228+00

Sheet Number:

L606

\\\Wse03.local\WSE\\Projects\CT\\Pomfret CT\Airline Trail SPN 111-126\\(CAD\)Test Pr





PROFILE

0 20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

HORIZONTAL SCALE: 1"=40' VERTICAL SCALE: 1"=8' Project:
TAP PROJECT NO. 111-126

Town
Pointret

AIR LINE TRAIL

TOWNS OF PUTNAM AND
POMFRET, CT

Weston & Sampson

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Drawn By: AMN

Reviewed By: RC

Approved By: MJJ

W&S Project No.: FNG22-1336

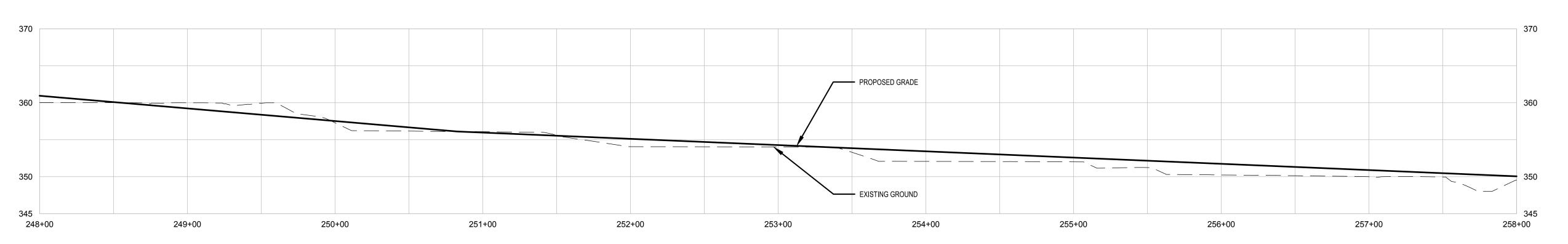
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W&S File No.: ----

Drawing Title:

EXISTING PROFILE STA. 128+00 TO 148+00

neet Number:

L607



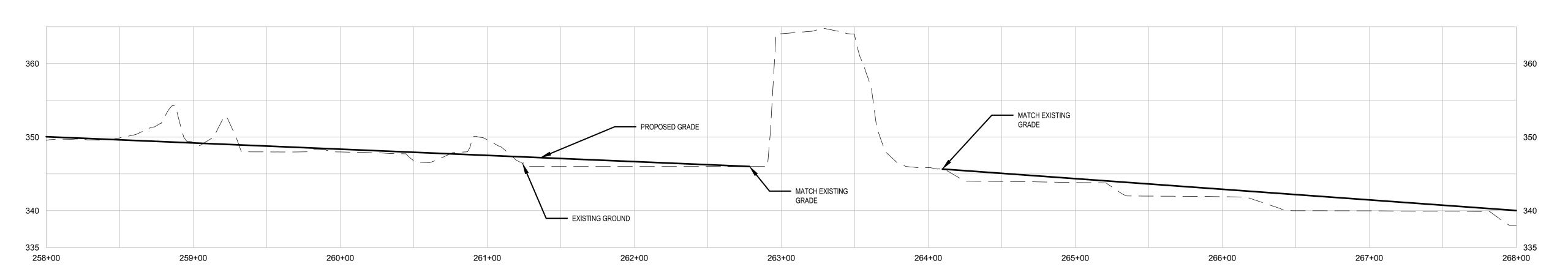
PROFILE

0 20' 40' 80

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

### AIRLINE NS PARK TRAIL

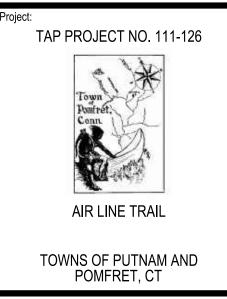


PROFILE

20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'



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Reviewed By: RC

Approved By: MJJ

W&S Project No.: ENG22-1336

W&S File No.:

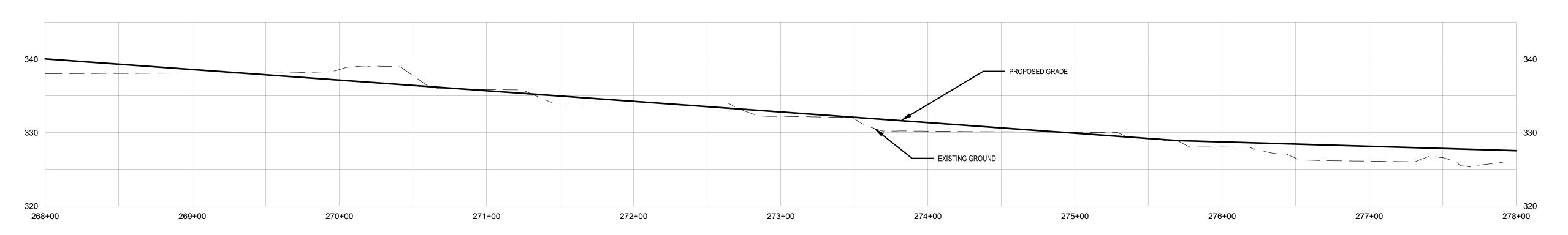
EXISTING PROFILE STA. 248+00 TO 268+00

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L608

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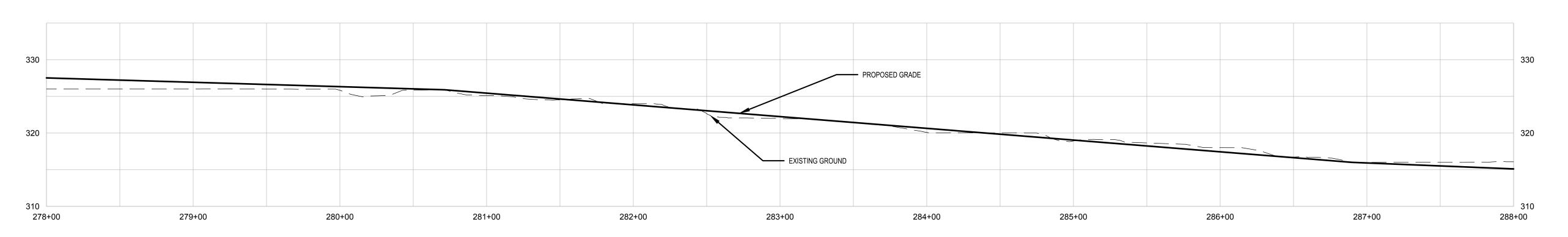
PROFILE

0 20' 40' 80

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

#### AIRLINE NS PARK TRAIL



PROFILE

20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

TAP PROJECT NO. 111-126

Town
Pointret
Conn.

AIR LINE TRAIL

TOWNS OF PUTNAM AND
POMFRET, CT

Weston & Sampson

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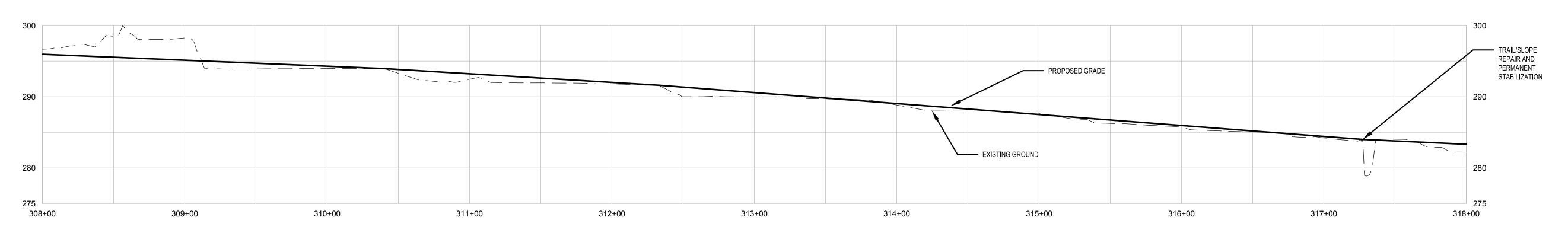
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W&S File No.: ----

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EXISTING PROFILE STA. 268+00 TO 288+00

Sheet Number:

L609



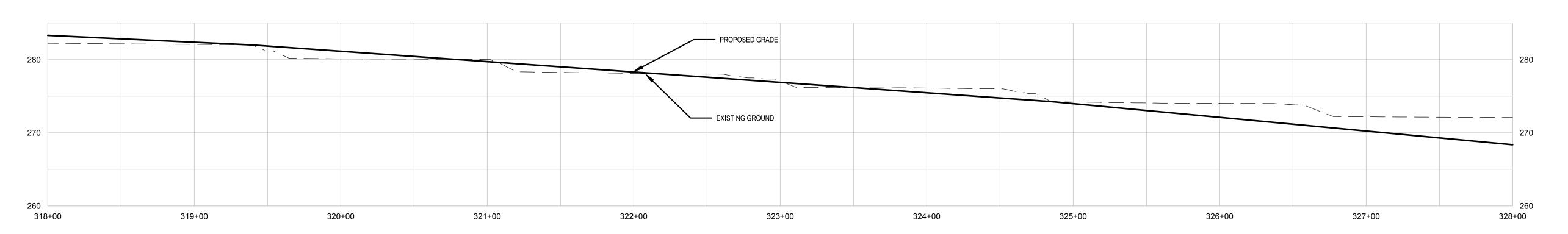
PROFILE

0 20' 40' 80

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

#### AIRLINE NS PARK TRAIL



PROFILE

20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

TAP PROJECT NO. 111-126

TOWNS OF PUTNAM AND POMFRET, CT

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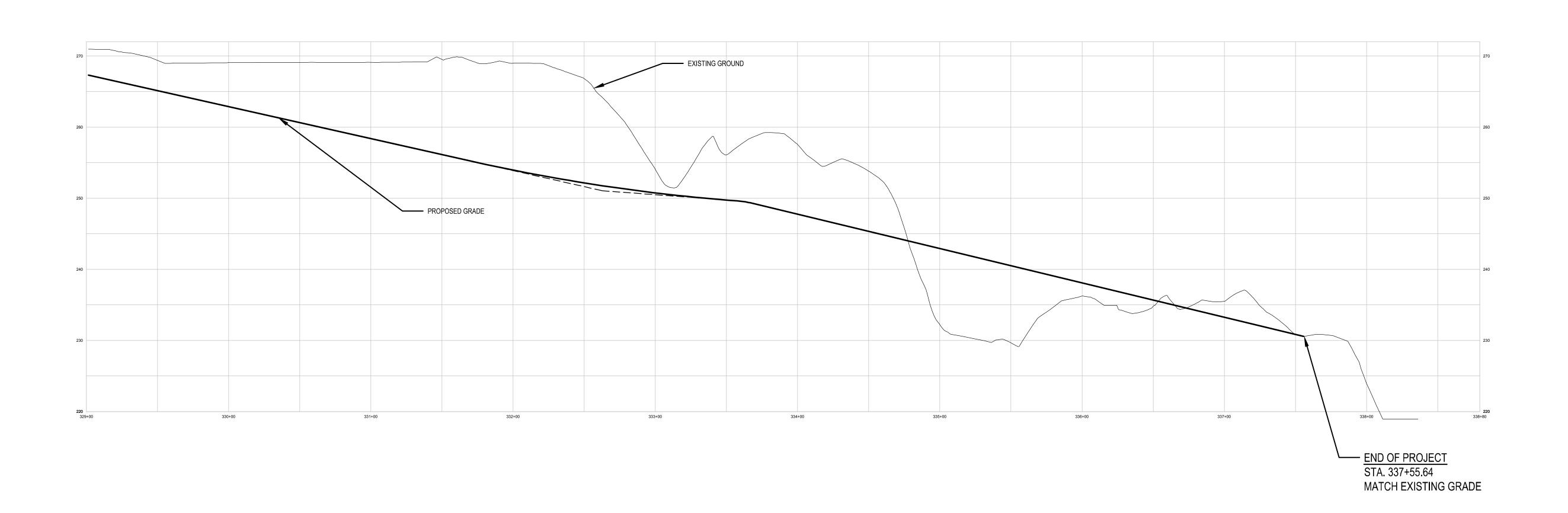
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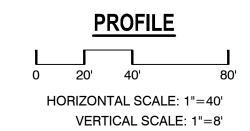
Drawing Title

EXISTING PROFILE STA. 308+00 TO 328+00

Sheet Number:

L610





TAP PROJECT NO. 111-126

Town
Pointret
Conn.

AIR LINE TRAIL

TOWNS OF PUTNAM AND
POMFRET, CT

Weston & Sampson

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ı	No.	Date	Description	
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Scale: AS NOTED

Date: August 28, 2023

Drawn By: AMN

Reviewed By: RC

Approved By: MJJ

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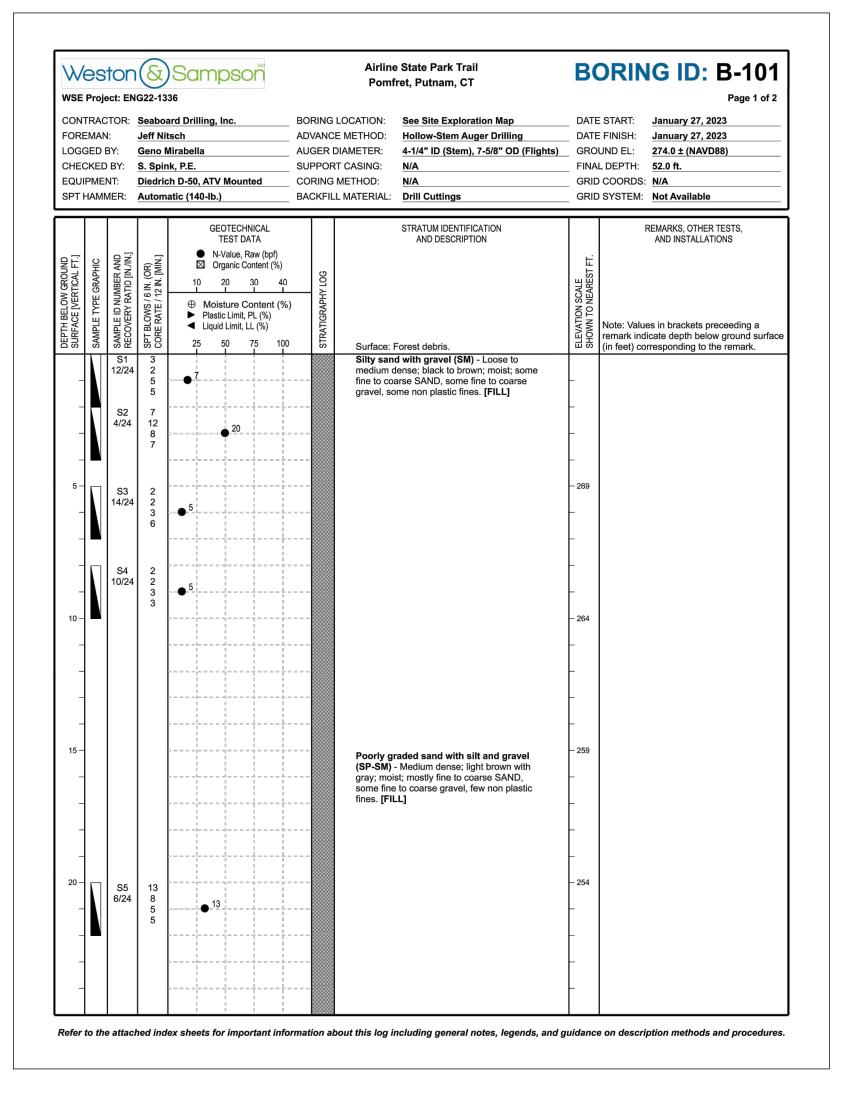
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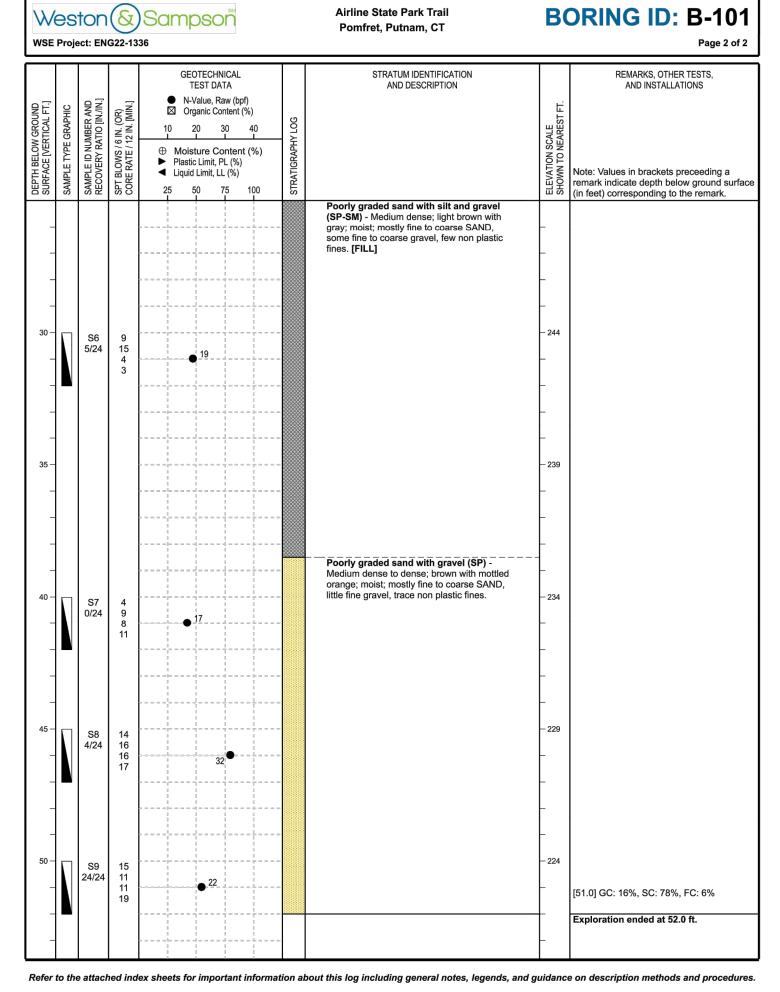
Drawing Title:

EXISTING PROFILE STA. 328+00 TO 334+25

Sheet Number:

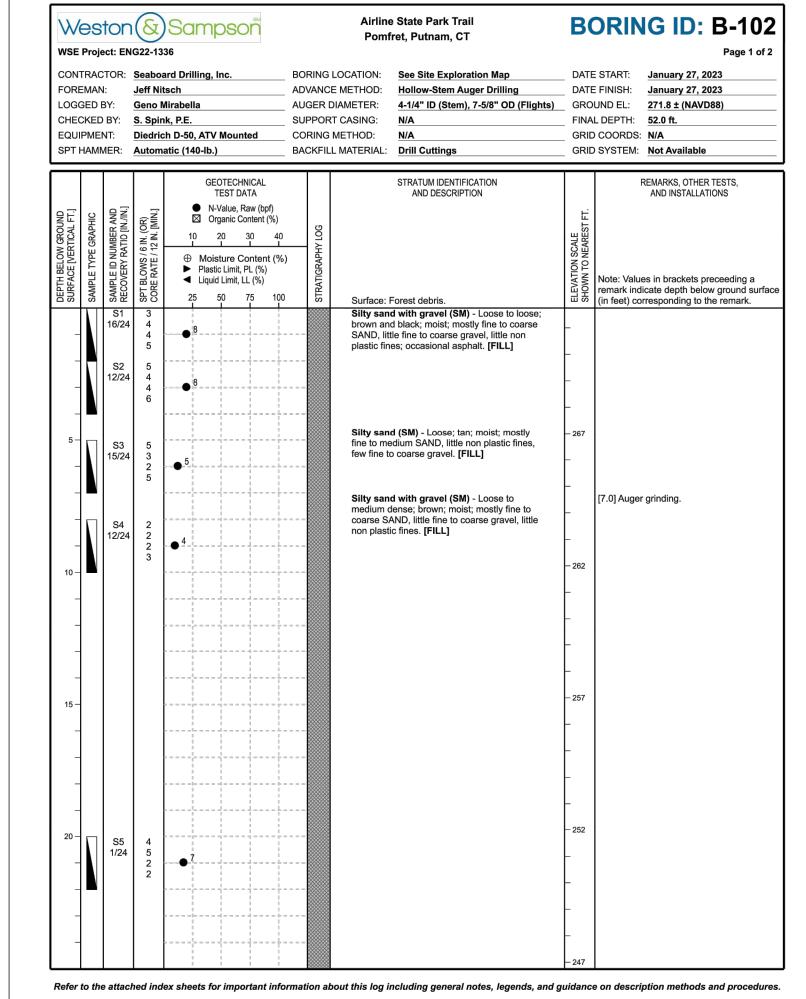
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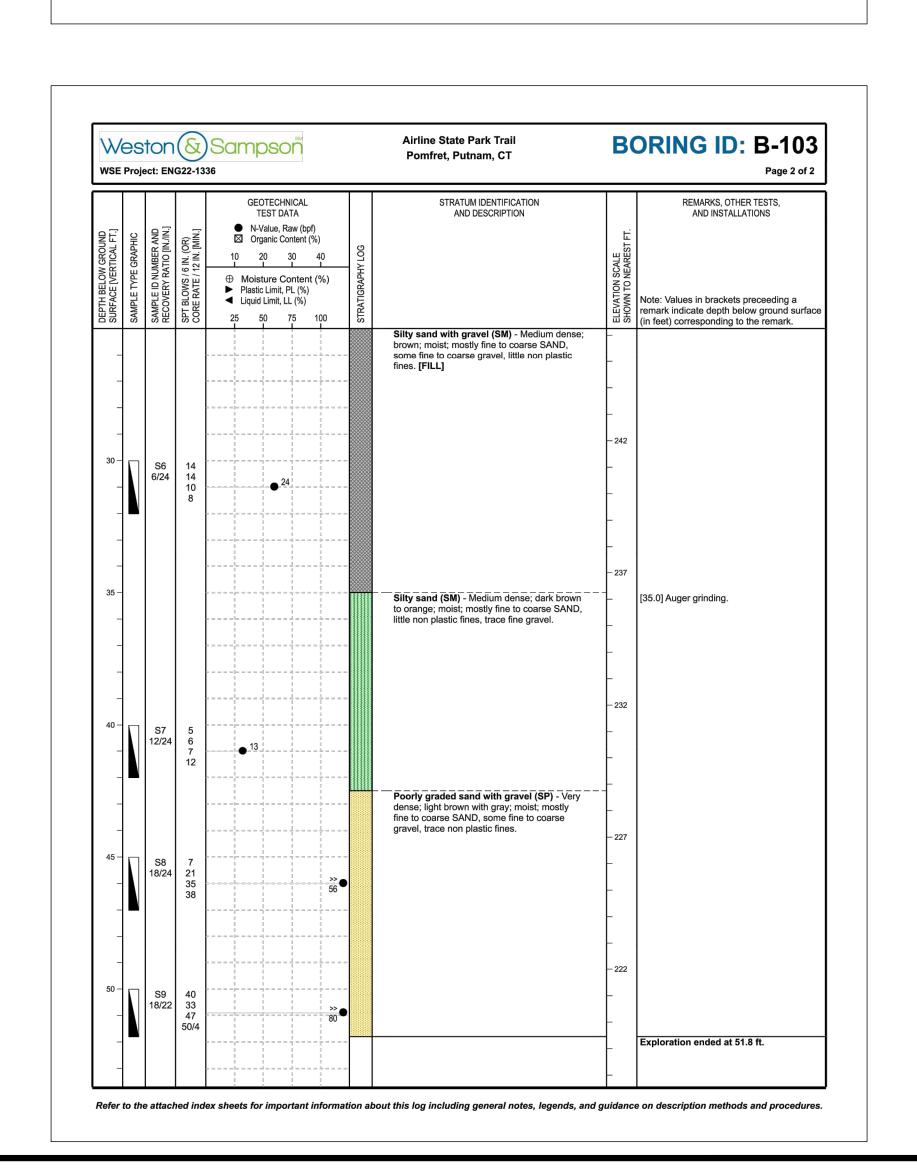


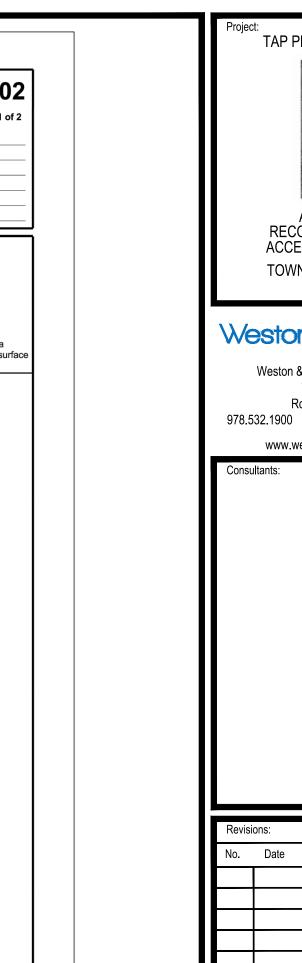


50 - - - - - -	to the	S9 24/24	15 11 11 19	ex sheets for important info	rmation a	bout this log including general notes, legends, and g		[51.0] GC: 16%, SC: 78%, FC: 6%  Exploration ended at 52.0 ft.  e on description methods and procedures.
WSE  CON' FORI LOGG CHEG	Proje TRAC EMAN GED I CKED	N: BY: ) BY: NT:	G22-13 Seaboa Jeff Nit Geno M S. Spin	ard Drilling, Inc. tsch dirabella	ADVANO AUGER SUPPOR CORING	Airline State Park Trail Pomfret, Putnam, CT  LOCATION: See Site Exploration Map  Hollow-Stem Auger Drilling  DIAMETER: 4-1/4" ID (Stem), 7-5/8" OD (Flights)  T CASING: M/A  MMA  MIA  Drill Cuttings	DATE DATE GRO FINA GRID	Page 1 of 2  E START: January 26, 2023  E FINISH: January 26, 2023  UND EL: 271.2 ± (NAVD88)  L DEPTH: 51.8 ft.  O COORDS: N/A  D SYSTEM: Not Available
DEPTH BELOW GROUND SURFACE [VERTICAL FT.]	SAMPLE TYPE GRAPHIC	SAMPLE ID NUMBER AND RECOVERY RATIO [IN./IN.]	SPT BLOWS / 6 IN. (OR) CORE RATE / 12 IN. [MIN.]	GEOTECHNICAL TEST DATA  N-Value, Raw (bpf)  Organic Content (%)  10 20 30 40  Moisture Content (%)  Plastic Limit, PL (%)  Liquid Limit, LL (%)  25 50 75 100	STRATIGRAPHY LOG	STRATUM IDENTIFICATION AND DESCRIPTION  Surface: Forest debris.	ELEVATION SCALE SHOWN TO NEAREST FT.	REMARKS, OTHER TESTS, AND INSTALLATIONS  Note: Values in brackets preceeding a remark indicate depth below ground surface (in feet) corresponding to the remark.
5 —		\$1 14/24 \$2 16/24 \$3 10/24	2 3 4 4 4 4 5 5	8		Poorly graded sand with gravel (SP) - Loose; black and light brown; mostly fine to coarse SAND, little fine to coarse gravel, trace non plastic fines; common asphalt. [FILL]  Poorly graded sand (SP) - Loose; tan; moist; mostly fine to coarse SAND, few fine to coarse gravel, trace non plastic fines. [FILL]  Poorly graded sand with gravel (SP) - Loose to medium dense; brown to light brown; moist; mostly fine to coarse SAND, some fine gravel, trace non plastic fines. [FILL]	_ _ _ _ 267 _	[6.0 - 7.0] Auger grinding. [6.0] GC: 31%, SC: 61%, FC: 8%
10		S4 7/24	2 2 2 4	4		Change to little gravel at 8 ft.	_ _ 262 _ _ _	
15		S5 12/24	4 5 6 4	11		Change to some gravel at 20 ft.	- 257 - - - - - 252 -	[18.0 - 19.0] Auger grinding.

Refer to the attached index sheets for important information about this log including general notes, legends, and guidance on description methods and procedures.







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PF	RELIMI	NARY DESIG
Scale	: 1"=20'	
Date:		APRIL 28, 2023
Drawı	n Rv <sup>.</sup>	GB
	· Dy.	
Revie	wed By:	RC
	-	RC MJJ

TAP PROJECT NO. 111-126

AIR LINE TRAIL

ACCESSIBILITY PROJECT

TOWNS OF PUTNAM AND

POMFRET, CT

Weston & Sampson Engineers, Inc.

712 Brook Street

800.SAMPSON

Rocky Hill, CT 06067

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RECONSTRUCTION AND

W&S File No.:

BORING LOGS Sheet Number:

DEPTH BELOW GR SURFACE [VERTIC	SAMPLE TYPE GRA	SAMPLE ID NUMBE RECOVERY RATIO	SPT BLOWS / 6 IN. CORE RATE / 12 IN	10 20 30 40  1 1 1 1		ELEVATION SCALE SHOWN TO NEARE	Note: Values in brackets preceeding a
SUR!	SAM	SAM	SPT SPT	25 50 75 100		ELE SPO	remark indicate depth below ground surfaction (in feet) corresponding to the remark.
-		S6 24/24	5	18	Silty sand with gravel (SM) - Loose to medium dense; brown; moist; mostly fine to coarse SAND, little fine to coarse gravel, little non plastic fines. [FILL]  Change to some non plastic fines at 25 ft.	- -	
30 -		S7 5/24	5 5 5	10		- - 242 -	
_			5			-	
_						_	
35 — —						- 237 -	
_						- -	
40 —		S8	10			_ _ 232	
-		16/24		31	Poorly graded sand with silt and gravel (SP-SM) - Medium dense to dense; light brown with gray; moist; mostly fine to coarse SAND, some fine to coarse gravel, few non plastic fines.	<u>-</u> -	
- 45		S9 18/24	6 17			- - 227	
-			18 15	35		_	
_						_	
50 — —						- 222 -	
-						<u>-</u> -	Exploration ended at 52.0 ft.
- Refer	to the	e attacl	hed ind	ex sheets for important information ab	out this log including general notes, legends, and g	-	

Airline State Park Trail

Pomfret, Putnam, CT

STRATUM IDENTIFICATION

AND DESCRIPTION

WSE Project: ENG22-1336

TEST DATA

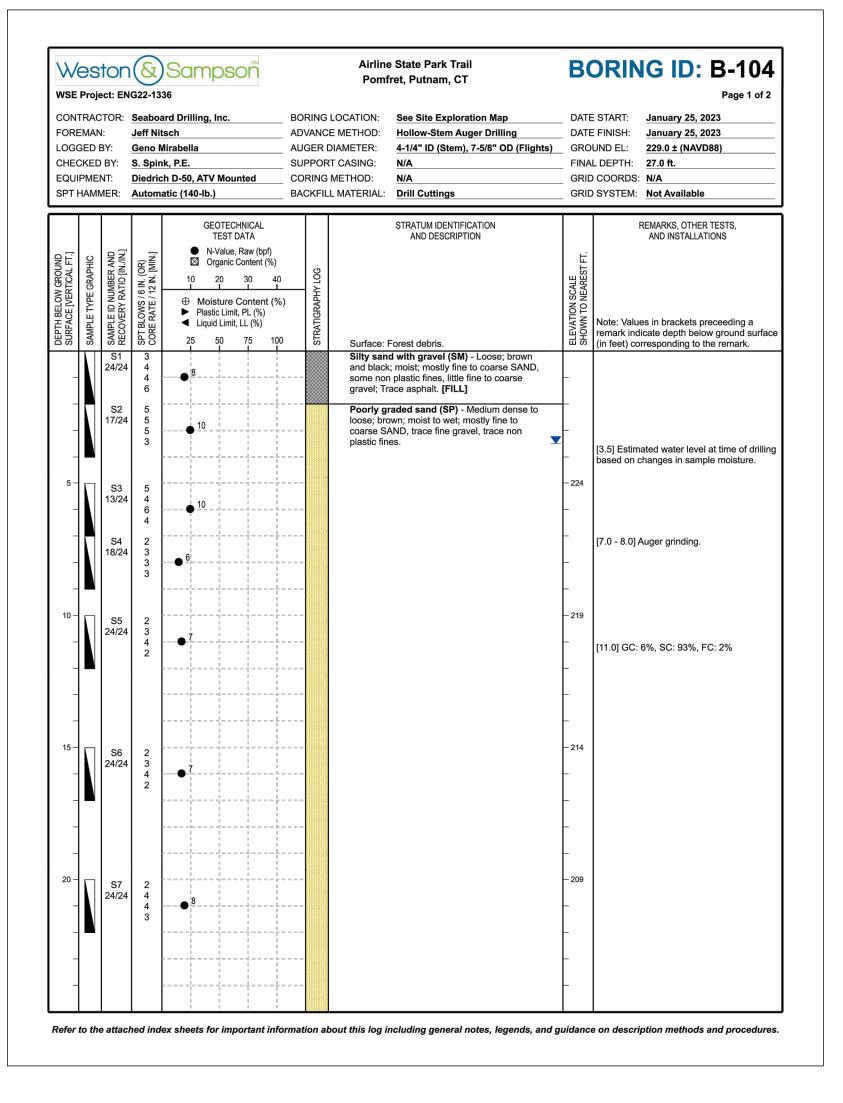
N-Value, Raw (bpf)Organic Content (%)

**BORING ID: B-102** 

REMARKS, OTHER TESTS,

AND INSTALLATIONS

Page 2 of 2



Airline State Park Trail

Pomfret, Putnam, CT

Refer to the attached index sheets for important information about this log including general notes, legends, and guidance on description methods and procedures.

STRATUM IDENTIFICATION

AND DESCRIPTION

WSE Project: ENG22-1336

TEST DATA

N-Value, Raw (bpf)

Moisture Content (%)

SAMPLE TO HE PELO

SAMPLE TO HE PELO

SAMPLE TO HE PELO

Woisture Content (%)

Plastic Limit, PL (%)

Liquid Limit, LL (%)

25 50 75 100

☑ Organic Content (%) 10 20 30 40 **BORING ID: B-105** 

Note: Values in brackets preceeding a

remark indicate depth below ground surface (in feet) corresponding to the remark.

Heaving sand at 25.0 ft. (exploration ended).

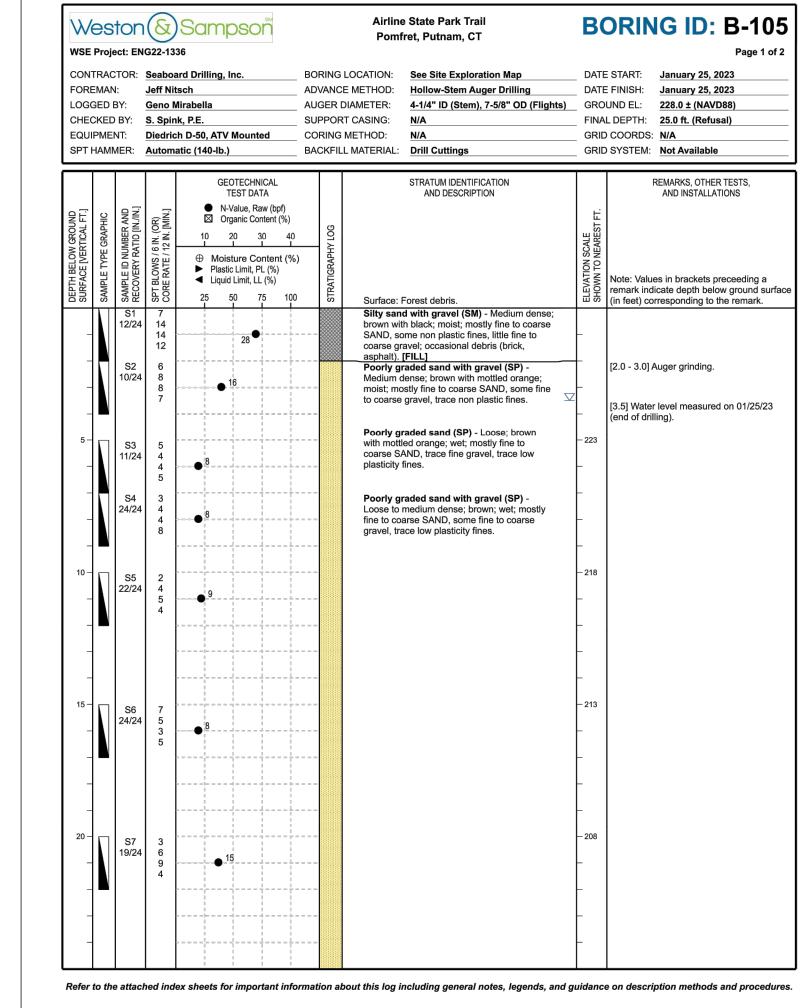
AND INSTALLATIONS

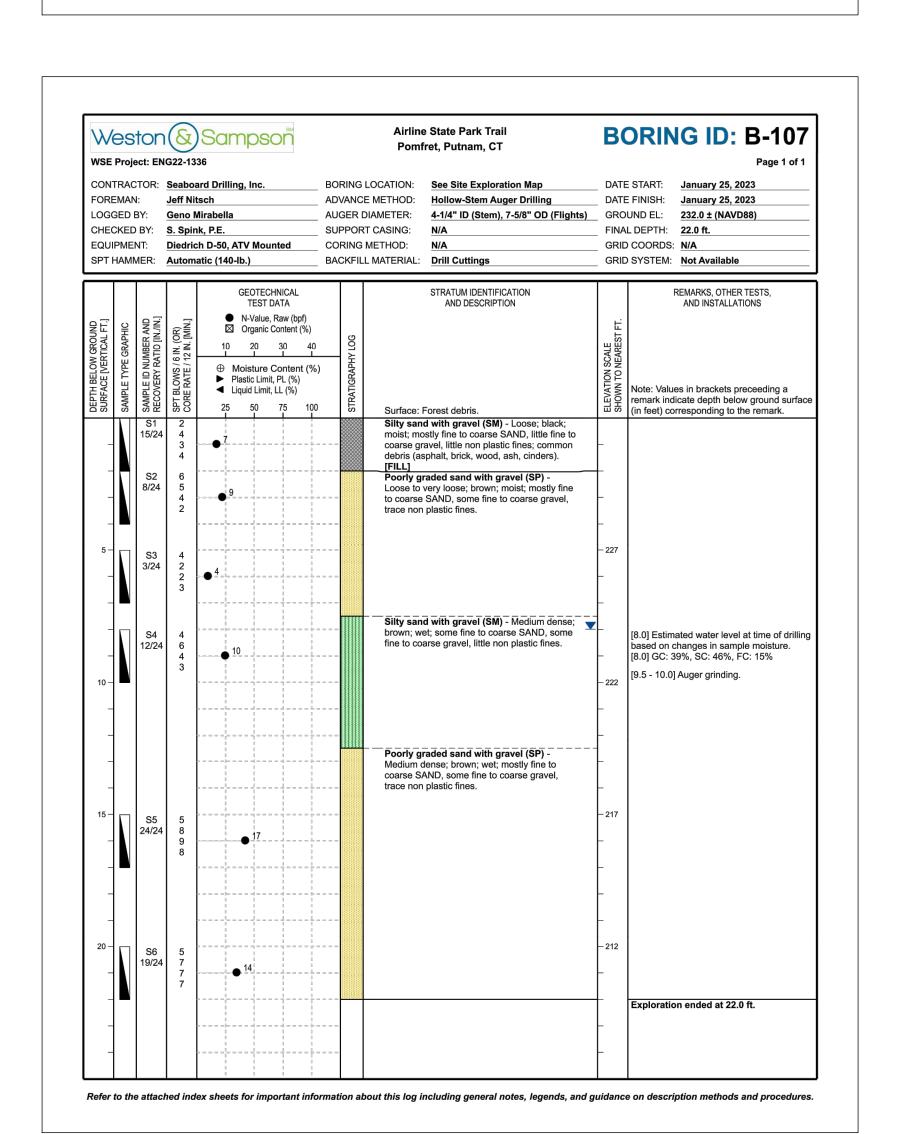
Page 2 of 2

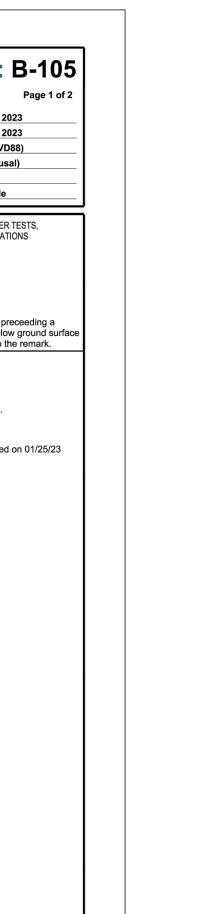
				GEOTECHNICAL TEST DATA		STRATUM IDENTIFICATION AND DESCRIPTION		REMARKS, OTHER TESTS, AND INSTALLATIONS
OUND L FT.]	PHIC	SAMPLE ID NUMBER AND RECOVERY RATIO [IN./IN.]	OR) [MIN.]	● N-Value, Raw (bpf) ☑ Organic Content (%)	9		ST FT.	
DEPTH BELOW GROUND SURFACE [VERTICAL FT.]	SAMPLE TYPE GRAPHIC	UMBEF SATIO [	SPT BLOWS / 6 IN. (OR) CORE RATE / 12 IN. [MIN.]	10 20 30 40	STRATIGRAPHY LOG		ELEVATION SCALE SHOWN TO NEAREST FT.	
H BELO	E TYP	E ID N VERY F	LOWS /	● Moisture Content (%)  ► Plastic Limit, PL (%)	IGRAP		NOTN	Note: Values in brackets proceeding a
DEPTF SURF	SAMPI	SAMPI RECO	SPT BI CORE	■ Liquid Limit, LL (%) 25 50 75 100	STRAT		ELEVA	Note: Values in brackets preceeding a remark indicate depth below ground surfact (in feet) corresponding to the remark.
_		S8 12/24	3 4 4	8		Poorly graded sand (SP) - Medium dense to loose; brown; moist to wet; mostly fine to coarse SAND, trace fine gravel, trace non		
_			7			plastic fines.	+	Exploration ended at 27.0 ft.
_							-	Exploration chaca at 21.0 it.
30 —							<b>– 199</b>	
Ī								
35 —							<del>-</del> 194	
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		ton		Sampson			e State Park Trail ret, Putnam, CT	В	ORING ID: B-106
CONT FORE LOGG CHEC	FRACEMAN SED E	STOR: I: BY: BY:	Seaboa Jeff Nit Geno M S. Spin	ard Drilling, Inc. tsch Airabella kk, P.E.	ADVAN AUGER SUPPO	G LOCATION: CE METHOD: DIAMETER: IRT CASING:	See Site Exploration Map Hollow-Stem Auger Drilling 4-1/4" ID (Stem), 7-5/8" OD (Flights) N/A	DATE GRO FINA	E START: January 25, 2023 E FINISH: January 25, 2023 DUND EL: 229.0 ± (NAVD88) LL DEPTH: 22.0 ft.
EQUII SPT H				th D-50, ATV Mounted atic (140-lb.)		G METHOD: ILL MATERIAL:	N/A Drill Cuttings		O COORDS: N/A  Not Available
DEPTH BELOW GROUND  O SURFACE [VERTICAL FT.]	SAMPLE TYPE GRAPHIC	20/24 SAMPLE ID NUMBER AND 12/54 SAMPLE ID NUMBE	8994 805 400 G CORE RATE / 12 IN. [MIN.]	TEST DATA  N-Value, Raw (bpf) Corganic Content (%)  10 20 30 40  Moisture Content (%) Plastic Limit, PL (%) Liquid Limit, LL (%)  25 50 75 100	RATIGR	Silty sand brown and coarse S/fine to coa asphalt). I Poorly gr Loose; brd SAND, little Poorly gr Medium do coarse S/fitrace non Poorly gr light brow	AND DESCRIPTION  Forest debris.  It with gravel (SM) - Medium dense; It dark gray; moist; mostly fine to NND, some non plastic fines, little arse gravel; trace debris (brick, FILL)  Down; moist; mostly fine to coarse le fine gravel, trace non plastic fines.  Added sand with gravel (SP) - ense; brown; wet; mostly fine to NND, some fine to coarse gravel, plastic fines.  Added sand (SP) - Medium dense; n; wet; mostly fine to medium ice non plastic fines.	ELEVATION SCALE SHOWN TO NEAREST FT.	Note: Values in brackets preceeding a remark indicate depth below ground surfa (in feet) corresponding to the remark.  [3.0] GC: 17%, SC: 76%, FC: 6%  [4.0] Water level measured on 01/25/23 (end of drilling).  [5.0 - 6.0] Auger grinding.
- - - 15 - - - - 20 -		S5 17/24 S6 18/24	15 19 23 11 4 5	42		Dense; br SAND, so plastic fin Poorly gr light brow	aded sand with gravel (SP) - own; wet; mostly fine to coarse me fine to coarse gravel, trace non es.  aded sand (SP) - Medium dense; n; wet; mostly fine to coarse SAND, ravel, trace non plastic fines.	_ _ _ _ 214 _ _ _ _ _	[15.0 - 18.0] Auger grinding.

Refer to the attached index sheets for important information about this log including general notes, legends, and guidance on description methods and procedures.







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TAP PROJECT NO. 111-126

AIR LINE TRAIL RECONSTRUCTION AND **ACCESSIBILITY PROJECT** 

TOWNS OF PUTNAM AND

POMFRET, CT

Weston & Sampson Engineers, Inc.

712 Brook Street

800.SAMPSON

Rocky Hill, CT 06067

www.westonandsampson.com

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Consultants:

GB

MJJ

**BORING LOGS** 

W&S Project No.: ENG22-1336

Drawn By:

Reviewed By:

Approved By:

W&S File No.:

Sheet Number:

ORE OGO HEO QUI	RAC MAN BED E CKED PMEN	l: 3Y: BY: NT:	Jeff Nit Geno M S. Spin Diedric	lirabella	ADVAI AUGE SUPP CORIN	NG LOCATION: NCE METHOD: R DIAMETER: ORT CASING: NG METHOD: FILL MATERIAL:	See Site Exploration Map Hollow-Stem Auger Drilling 4-1/4" ID (Stem), 7-5/8" OD (Flights N/A N/A Drill Cuttings	DATE ) GRO FINA GRID	START:   January 26, 2023
SURFACE [VERTICAL FT.]	SAMPLE TYPE GRAPHIC	SAMPLE ID NUMBER AND RECOVERY RATIO [IN./IN.]	SPT BLOWS / 6 IN. (OR) CORE RATE / 12 IN. [MIN.]	GEOTECHNICAL TEST DATA  N-Value, Raw (bpf)  Organic Content (%)  10 20 30 40  Moisture Content (%)  Plastic Limit, PL (%)  Liquid Limit, LL (%)  25 50 75 100	OCINTRODUCTO	Ouridoc. i	STRATUM IDENTIFICATION AND DESCRIPTION  Forest debris.	ELEVATION SCALE SHOWN TO NEAREST FT.	REMARKS, OTHER TESTS, AND INSTALLATIONS  Note: Values in brackets preceeding a remark indicate depth below ground surface (in feet) corresponding to the remark.
		S1 18/24 S2 6/24	4 7 6 1 3 2	11		to loose; l coarse S/ some non (asphalt, l	d with gravel (SM) - Medium dense prown and black; moist; some fine to AND, some fine to coarse gravel, plastic fines; occasional debris prick). [FILL]	-	[3.0 - 5.0] Auger grinding.
5 —		S3 8/24	10 7 4 8	11		light brow	d with gravel (SM) - Medium dense; n; moist; mostly fine to coarse me fine to coarse gravel, little non es.	228  	
_ _ IO _		S4 9/24	8 9 6 7	15				- - - 223	[10.0 - 13.0] Auger grinding.
5 -		S5 0/1	50/1						[13.0] Blow-in material in split spoon is wet.  Sampler and auger refusal at 13.1 ft. (exploration ended).
,								-	
- 0.0								- 213 -	
1								-	

CONTRACTO FOREMAN: LOGGED BY: CHECKED B' EQUIPMENT: SPT HAMME	': <u>C</u> IY: <u>S</u> I: <u>I</u>	Jeff Ni Geno I S. Spir Diedric	tsch AD Mirabella AU nk, P.E. SU ch D-50, ATV Mounted CO	VANC IGER IPPOF DRING	LOCATION: See Site Exploration Map  Hollow-Stem Auger Drilling  Hollow-Stem, 7-5/8" OD (Flights)  A-1/4" ID (Stem), 7-5/8" OD (Flights)  N/A  METHOD: N/A  LL MATERIAL: Drill Cuttings	DATE GRO FINA GRIE	E START: January 26, 2023  E FINISH: January 26, 2023  DUND EL: 233.0 ± (NAVD88)  L DEPTH: 10.0 ft. (Refusal)  D COORDS: N/A  Not Available
SURFACE [VERTICAL FT.]  SAMPLE TYPE GRAPHIC  CAMPIE IT NIIMBEED AND		SPT BLOWS / 6 IN. (OR) SPT BLOWS / 6 IN. (OR) CORE RATE / 12 IN. [MIN.]	GEOTECHNICAL TEST DATA  N-Value, Raw (bpf) Grganic Content (%)  10 20 30 40  Moisture Content (%) Plastic Limit, PL (%) Liquid Limit, LL (%)  25 50 75 100  12  12	STRATIGRAPHY LOG	Surface: Forest debris.  Silty sand with gravel (SM) - Medium dense; dark brown; moist; some fine to coarse SAND, some fine to coarse gravel, some non plastic fines. [FILL]  Silty sand with gravel (SM) - Medium dense; brown; moist; some fine to coarse SAND, some fine to coarse gravel, little non plastic fines.  Change to brown and black; some non plastic fines	ELEVATION SCALE	REMARKS, OTHER TESTS, AND INSTALLATIONS

FORE LOGG	EMAN GED E CKED PMEI	: BY: BY: NT:	Jeff Nit Geno M S. Spin Diedric	/lirabella	V Moun	ted	A[ Al Sl C(	OVANO JGER JPPOF DRING	ELOCATION: DE METHOD: DIAMETER: RT CASING: B METHOD: LL MATERIAL:	See Site Exploration Map Hollow-Stem Auger Drilling 4-1/4" ID (Stem), 7-5/8" OD (Flig N/A N/A Drill Cuttings	DATE  Introduction properties  Introduction pr	E START: E FINISH: UND EL: L DEPTH: COORDS: SYSTEM:	January 26, 2023 January 26, 2023 233.0 ± (NAVD88) 10.0 ft. (Refusal) N/A Not Available
DEPTH BELOW GROUND SURFACE [VERTICAL FT.]	SAMPLE TYPE GRAPHIC	SAMPLE ID NUMBER AND RECOVERY RATIO [IN./IN.]	SPT BLOWS / 6 IN. (OR) CORE RATE / 12 IN. [MIN.]	10 ⊕ Mo	oisture Co stic Limit, I uid Limit, L	ata (bpt aw (bpt ontent (' 30 L ontent PL (%) L (%)	%) 40 1	STRATIGRAPHY LOG	Surface: F	STRATUM IDENTIFICATION AND DESCRIPTION  Forest debris.	ELEVATION SCALE SHOWN TO NEAREST FT.	Note: Value	REMARKS, OTHER TESTS, AND INSTALLATIONS  es in brackets preceeding a cate depth below ground surresponding to the remark.
					-4				Refer to E	3-109 for soil conditions.			set 5ft south from B-109.
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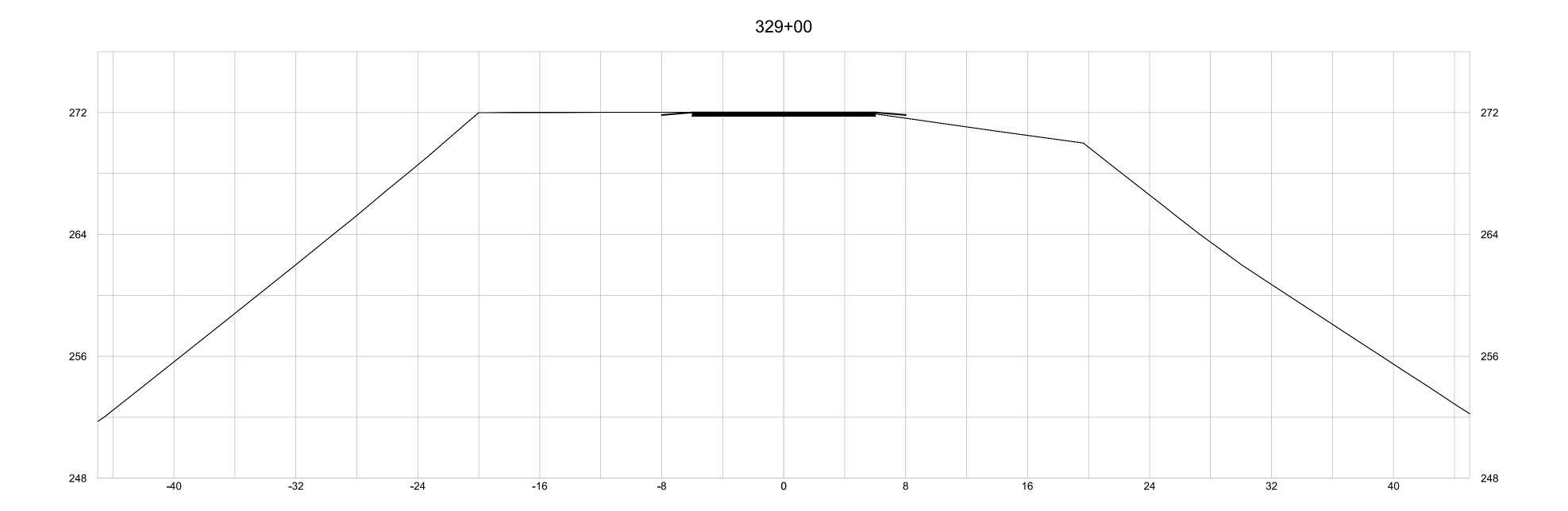
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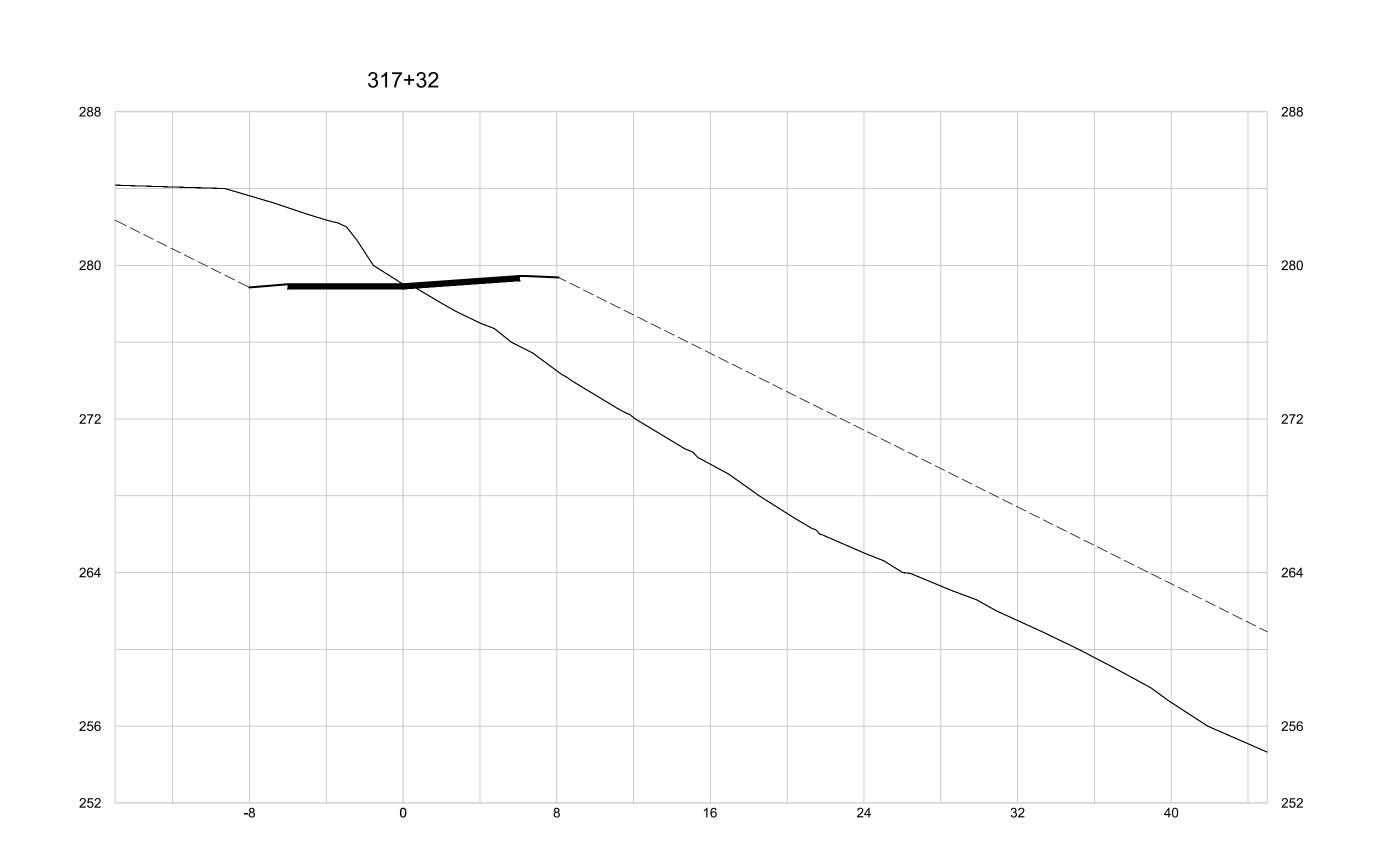
BORING LOGS

Sheet Number:

702

	P:\CT\Pomfret CT\Airline Trail SPN 111-126\CAD\ANS-PARK TRAIL_Boring Logs B-101	
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TAP PROJECT NO. 111-126 TOWNS OF PUTNAM AND POMFRET, CT

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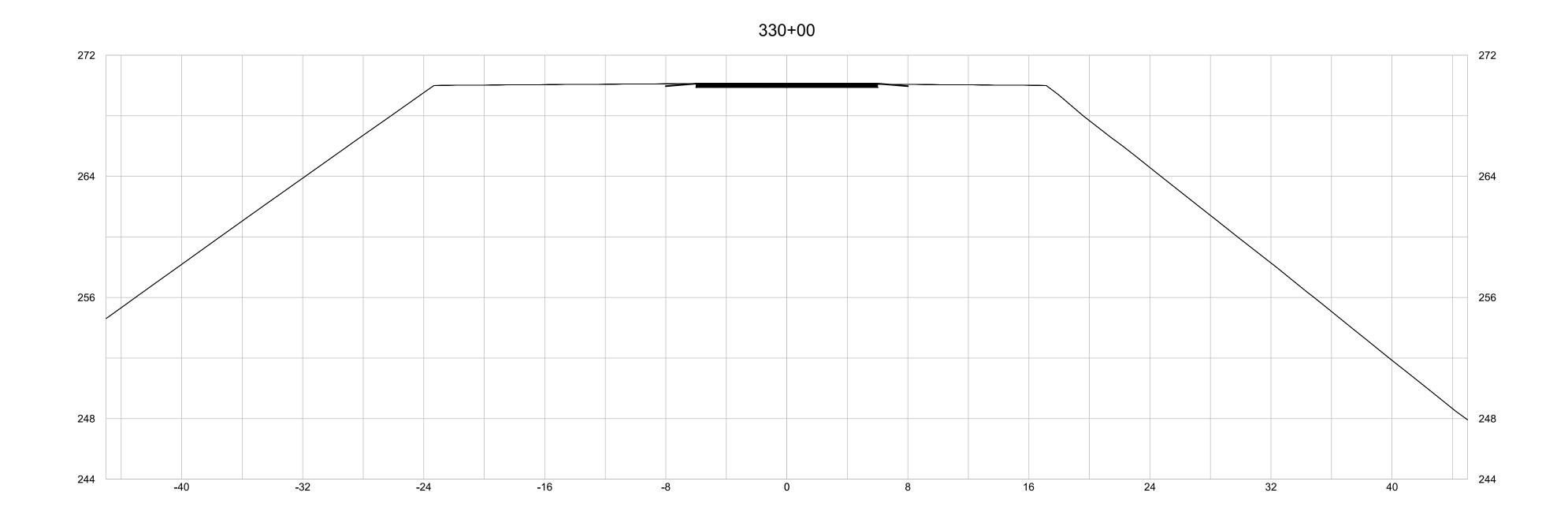
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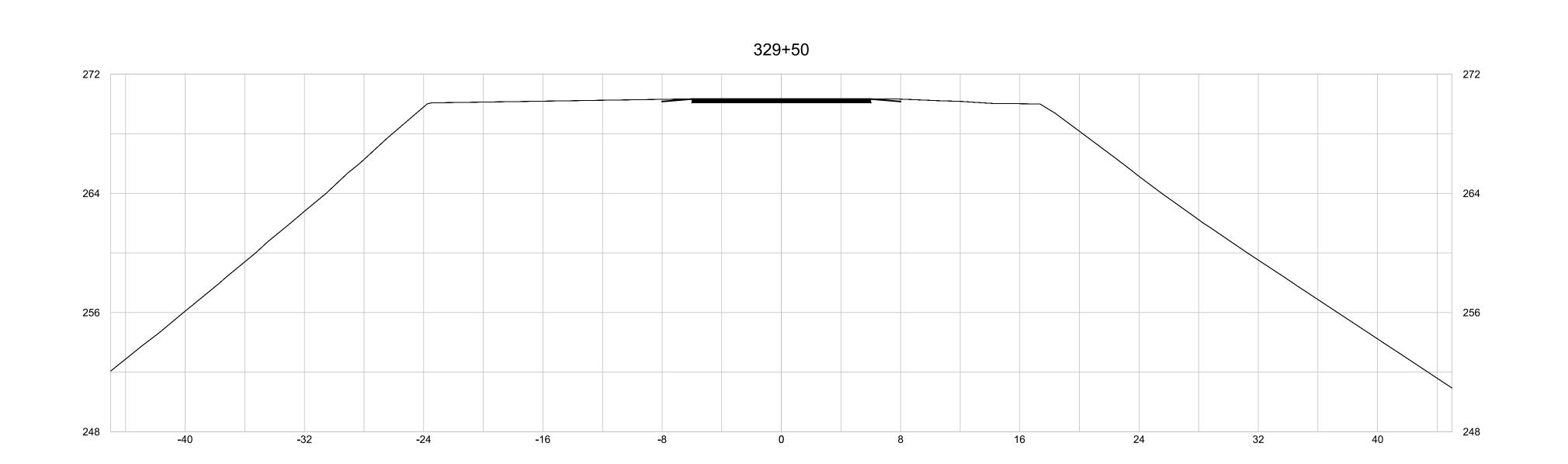
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W&S Project No.: ENG22-1336

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0 2.5' 5' HORIZONTAL SCALE: 1"=5' VERTICAL SCALE: 1"=5'





PROFILE

0 2.5' 5' 10

HORIZONTAL SCALE: 1"=5'

VERTICAL SCALE: 1"=5'

TAP PROJECT NO. 111-126

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Conn.

AIR LINE TRAIL

TOWNS OF PUTNAM AND
POMFRET, CT

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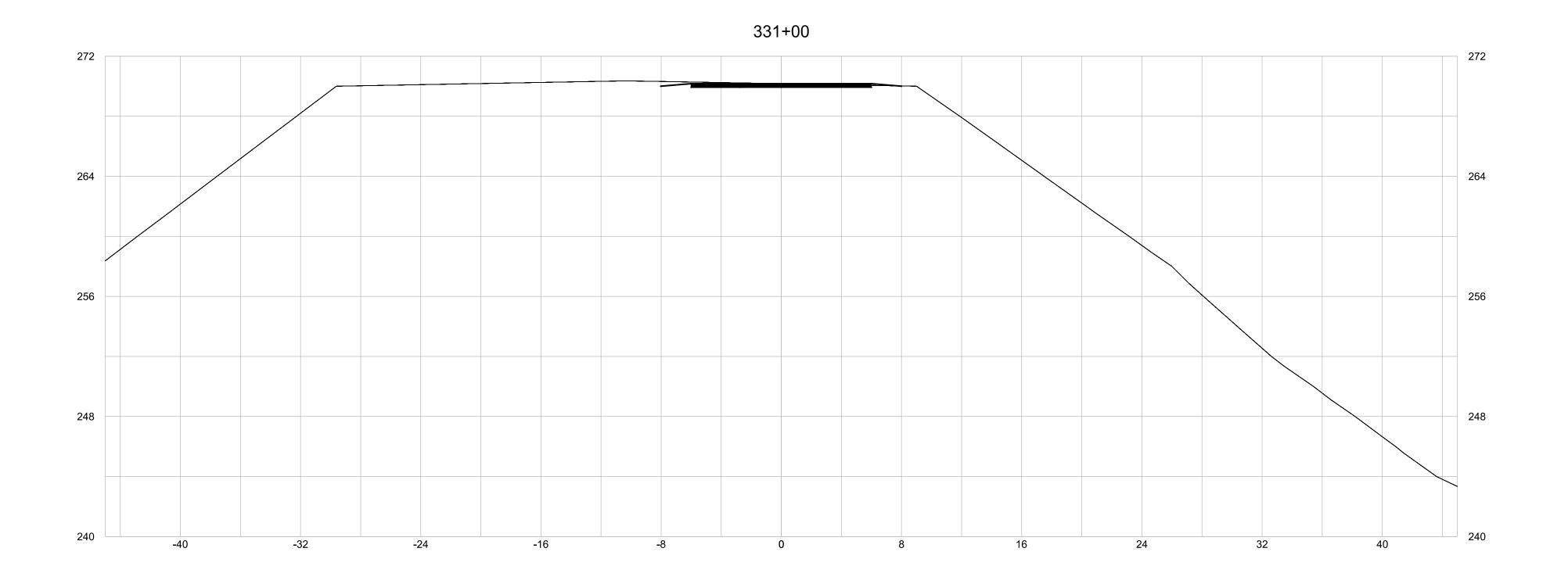
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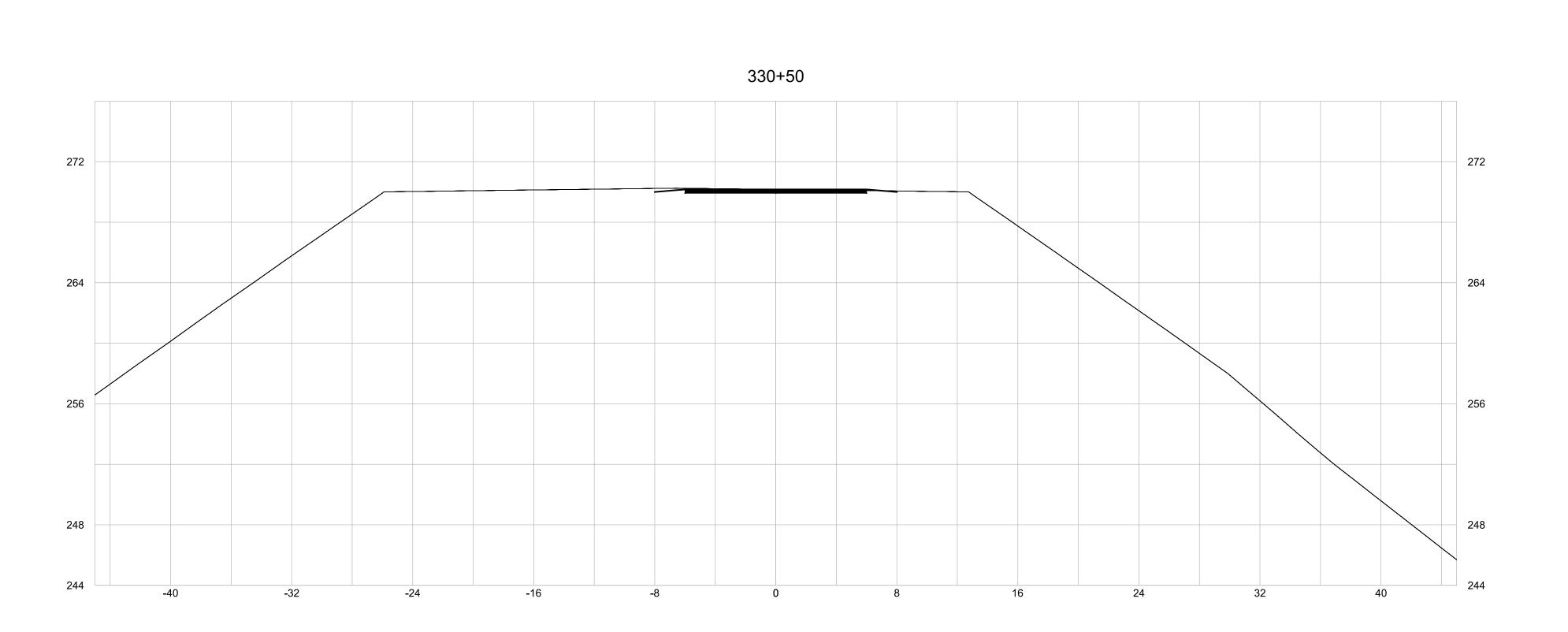
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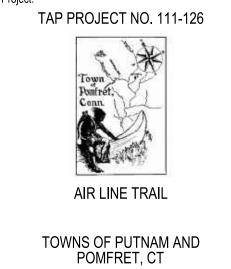
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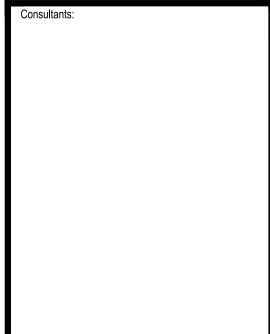
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0 2.5' 5'



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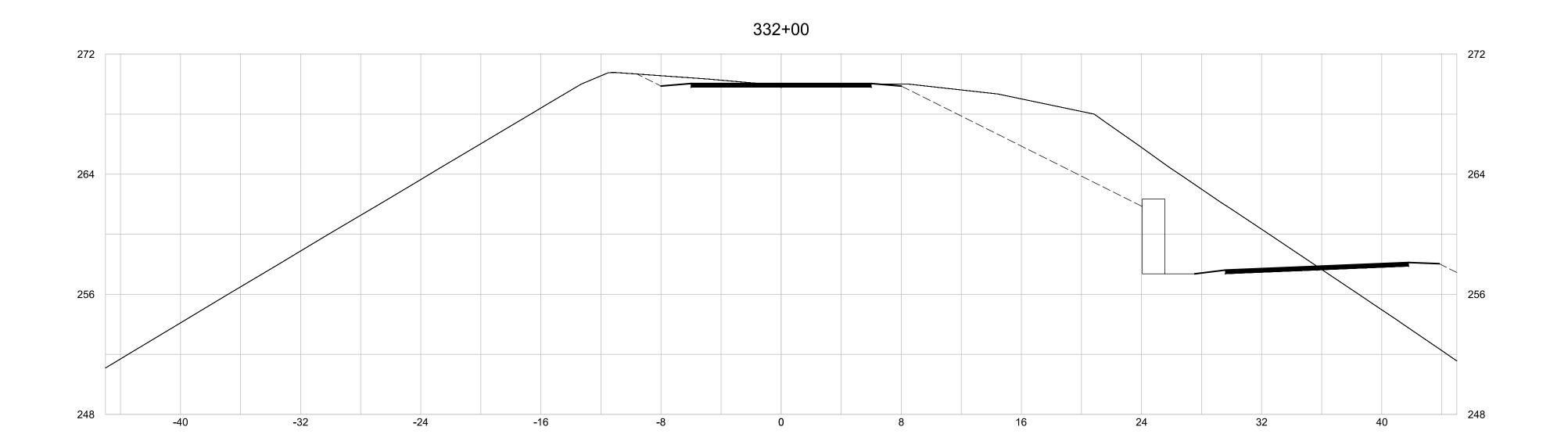
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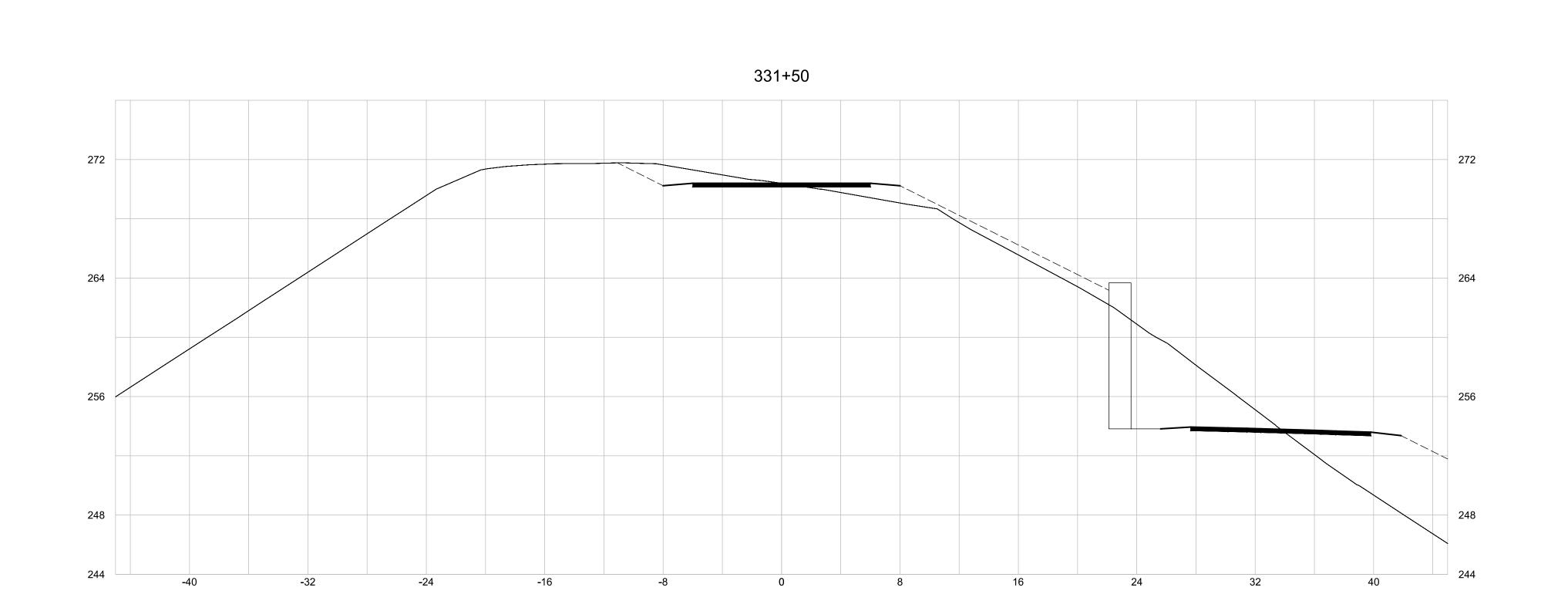
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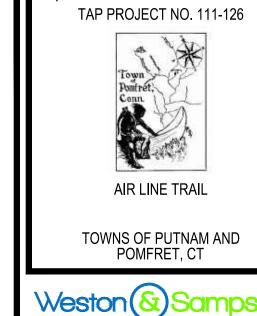
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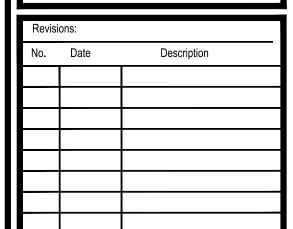




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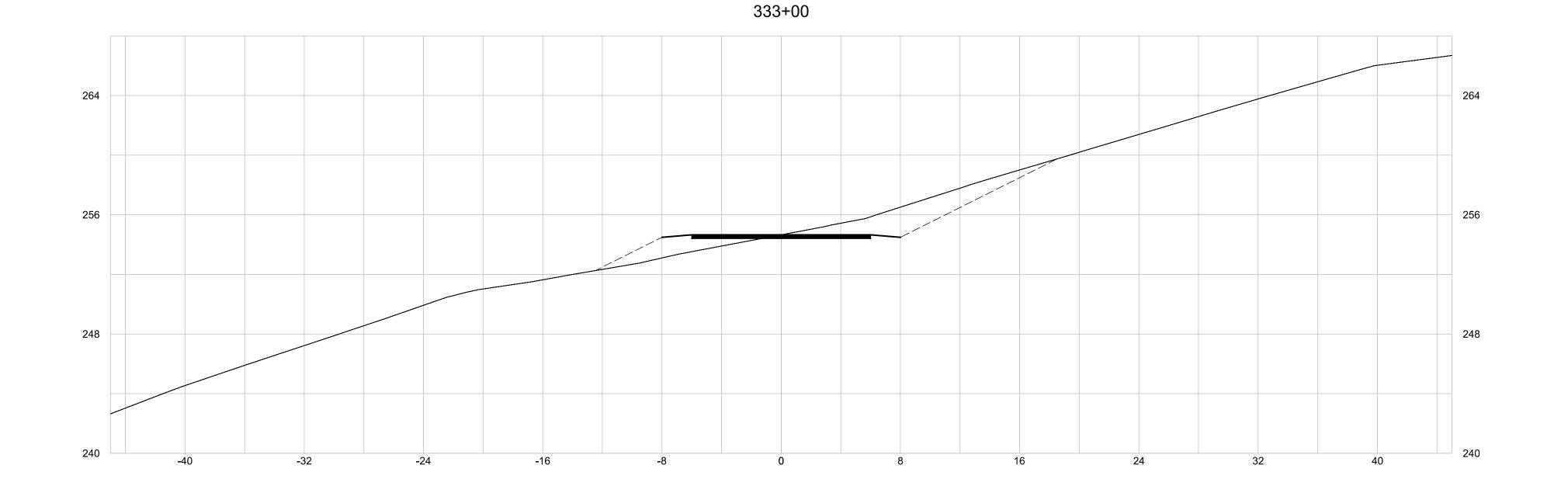
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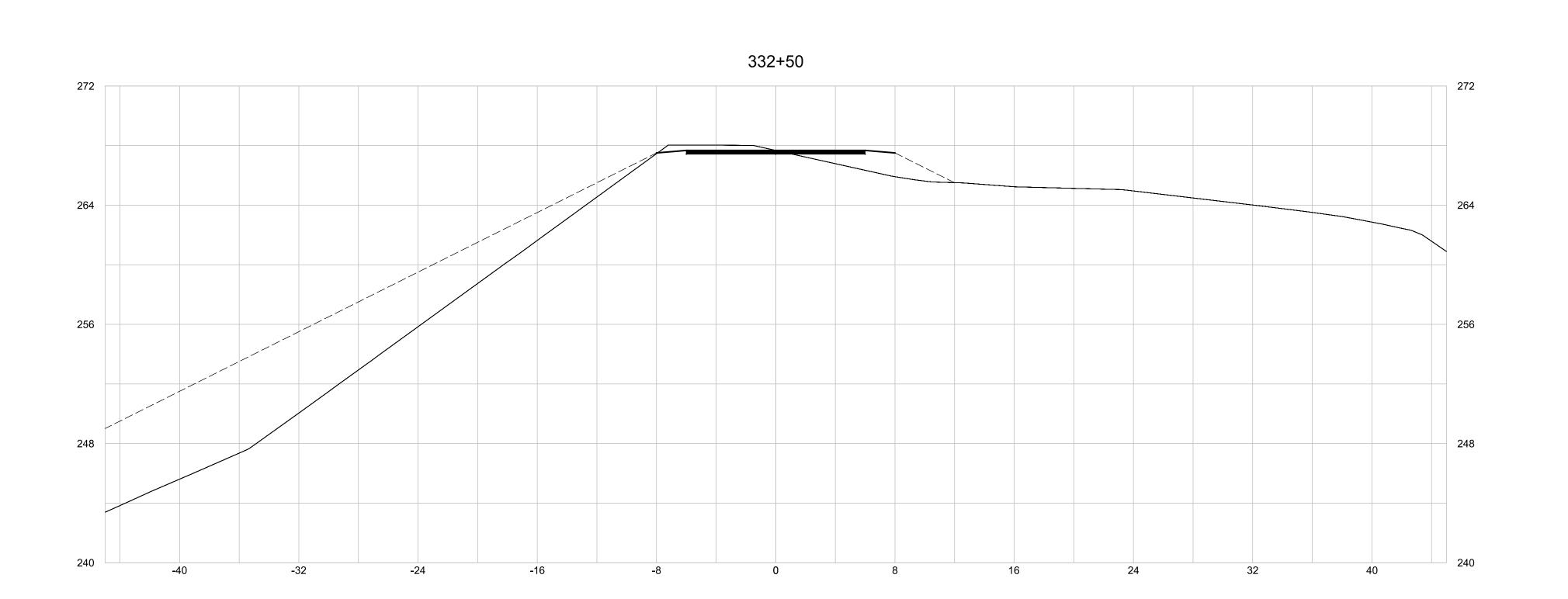
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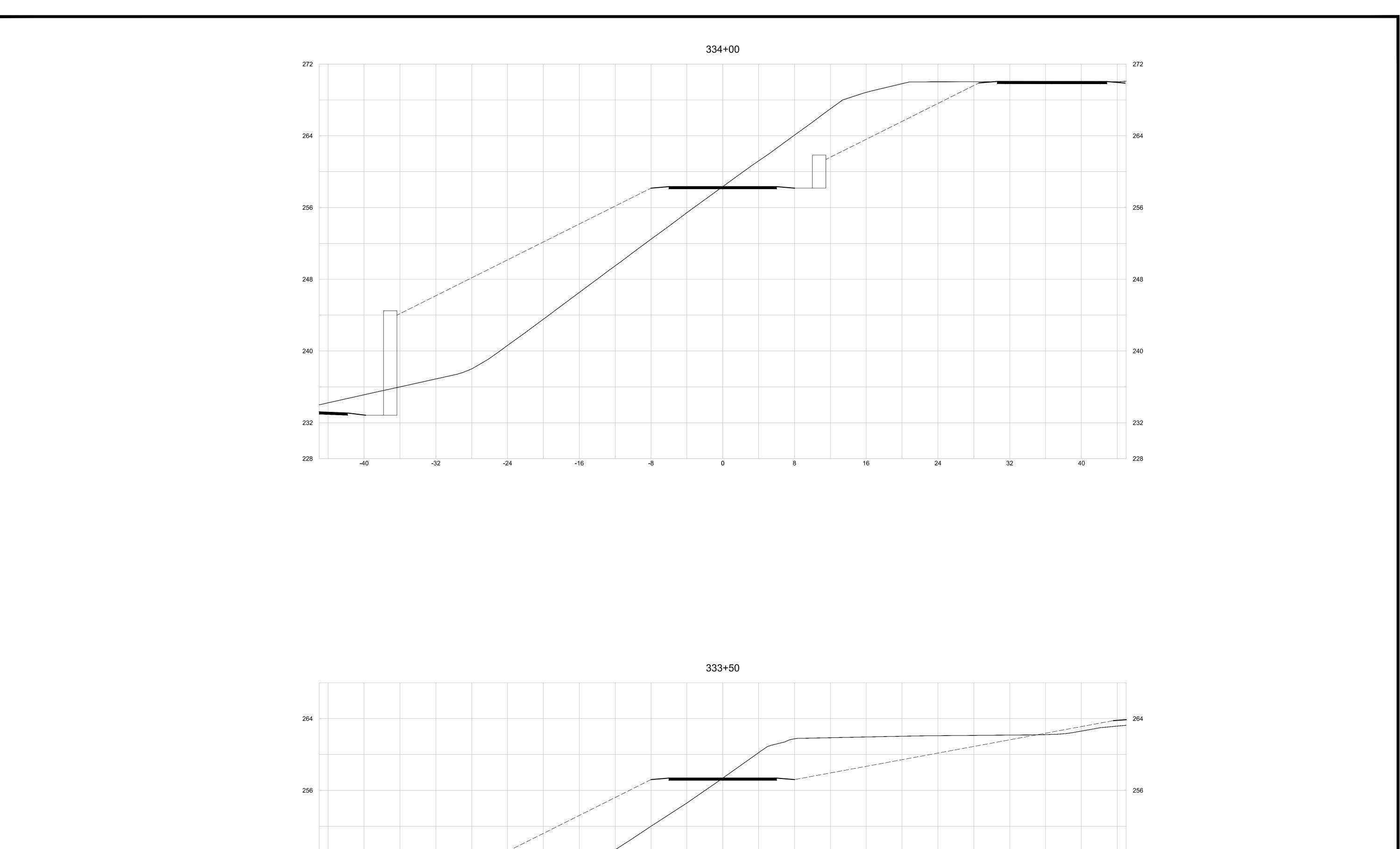
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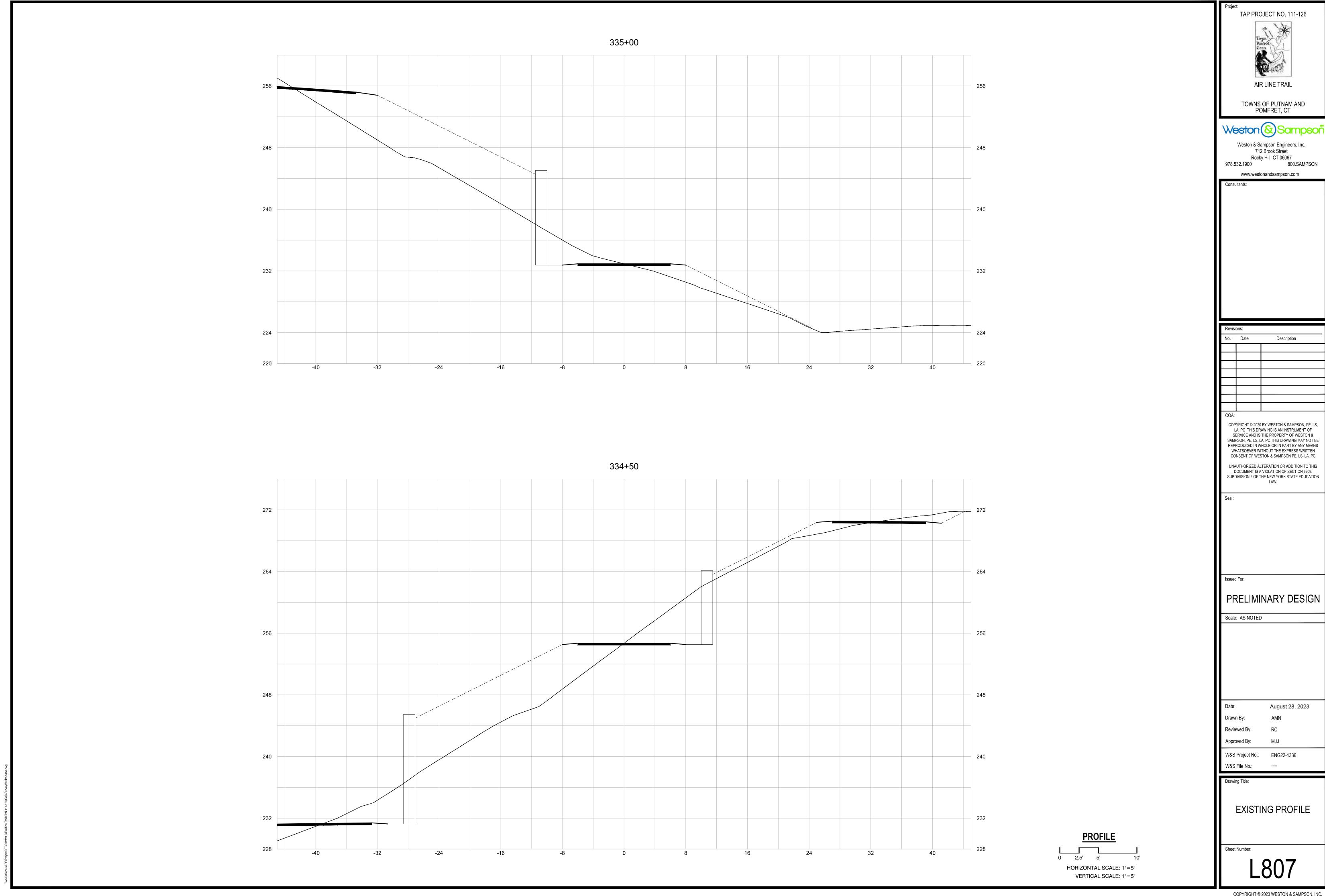
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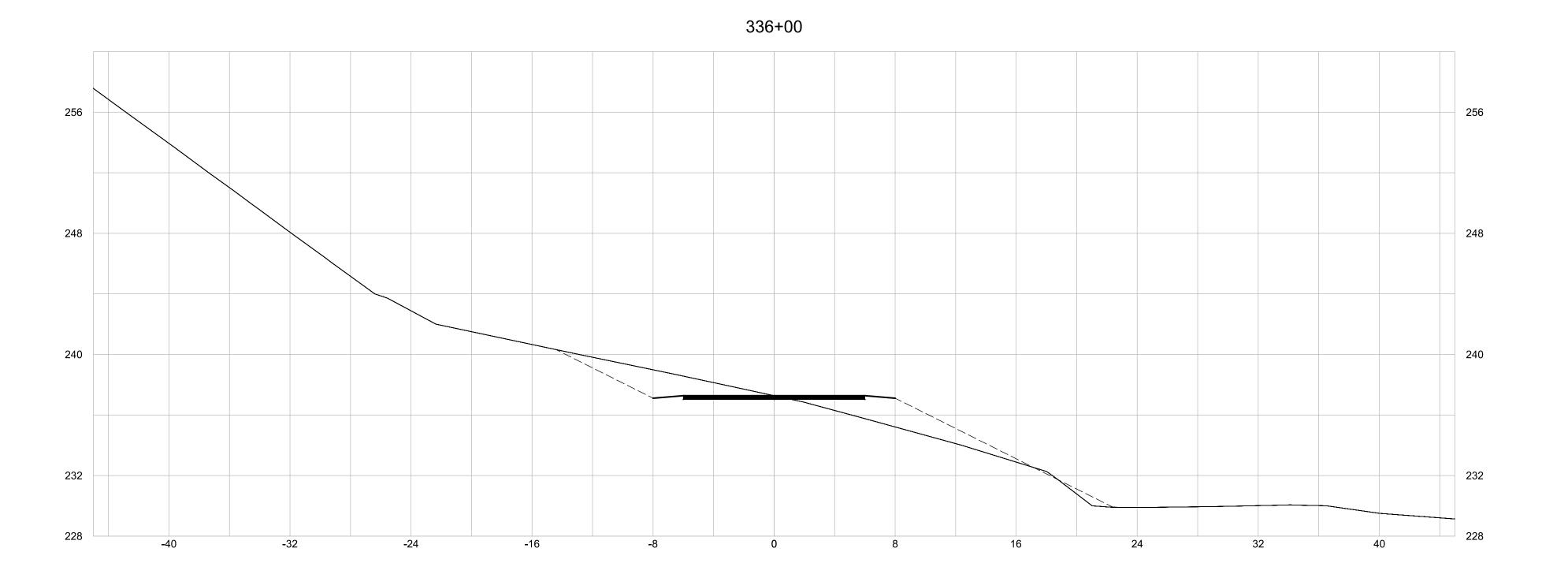
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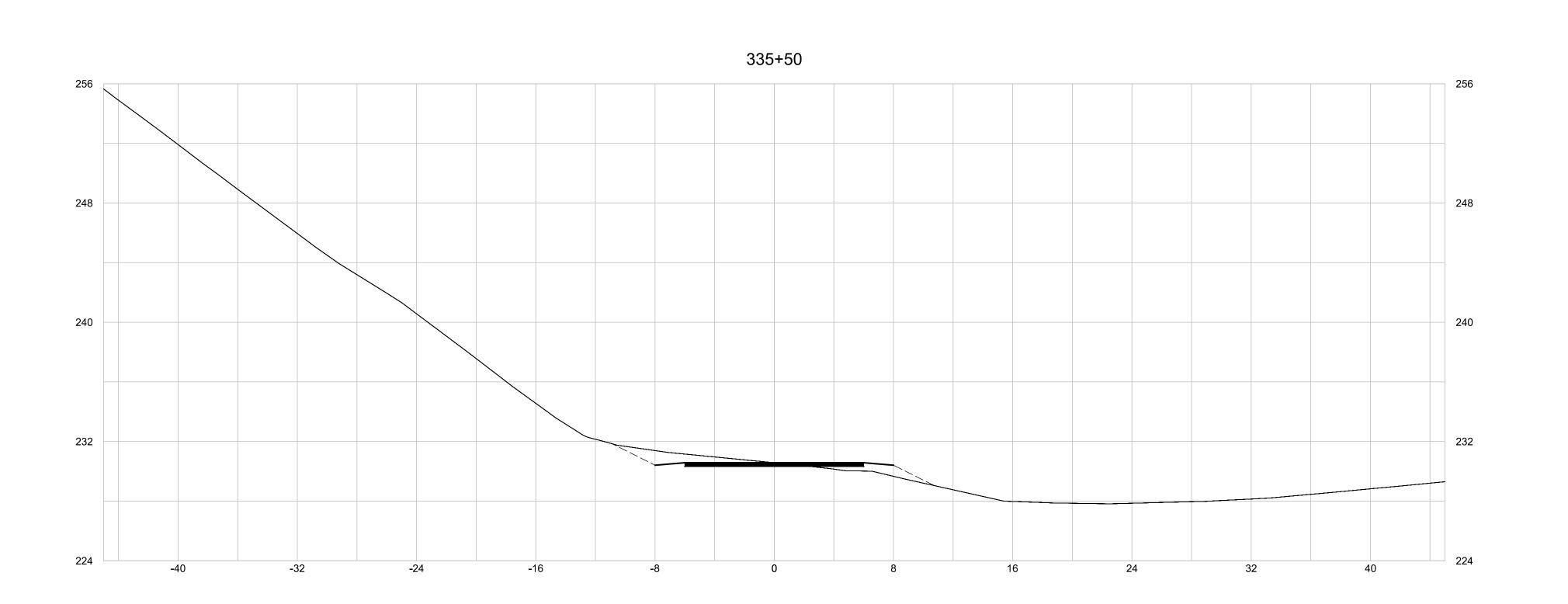
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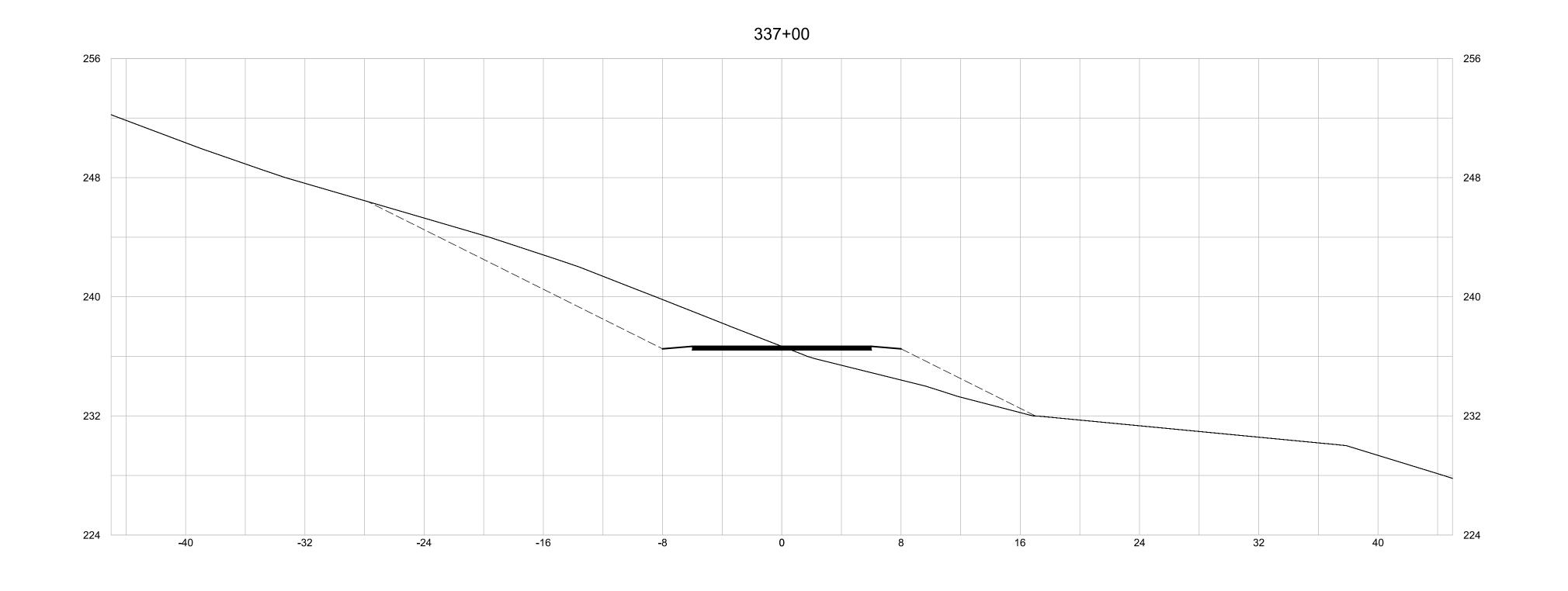
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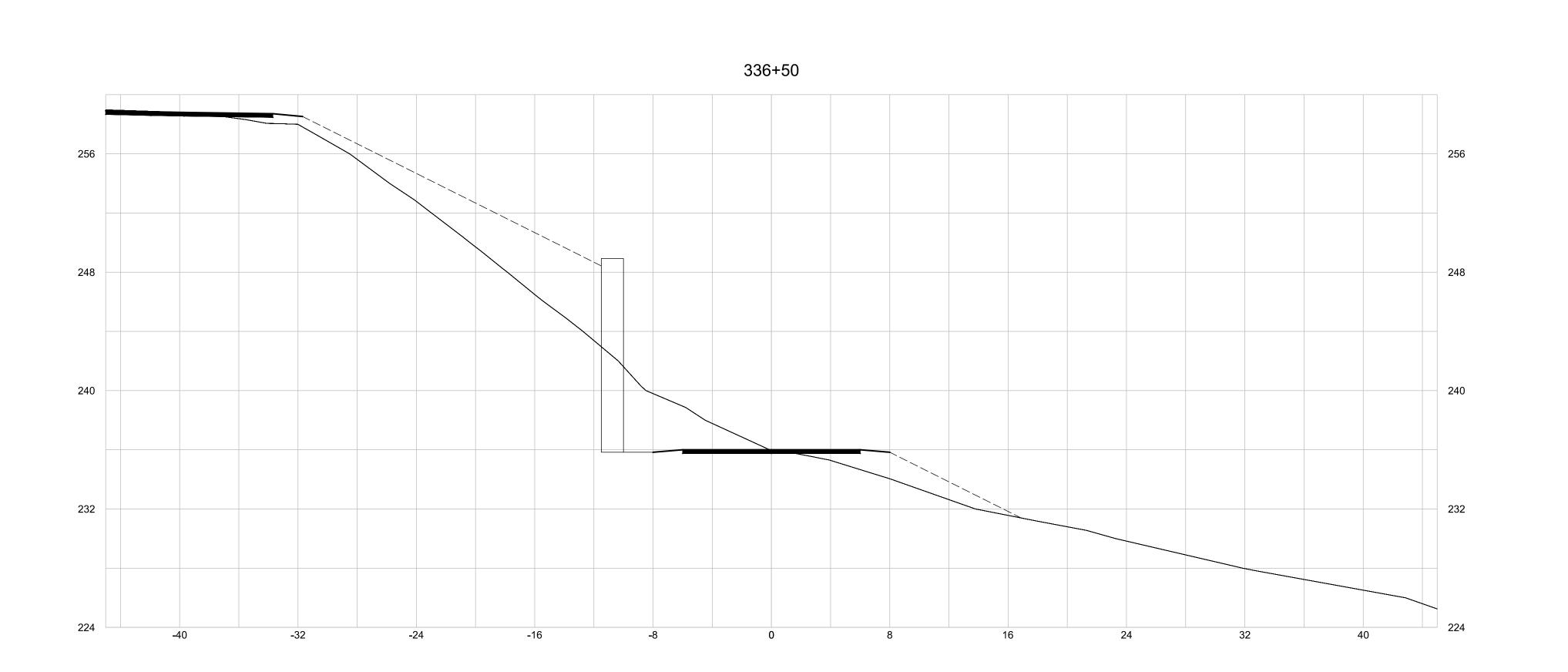
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PROFILE

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HORIZONTAL SCALE: 1"=5'

VERTICAL SCALE: 1"=5'

AIR LINE TRAIL

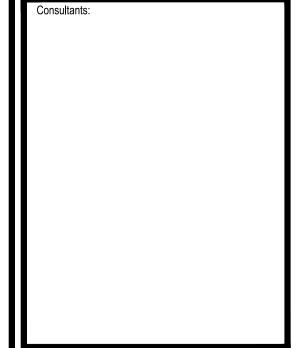
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TAP PROJECT NO. 111-126

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