

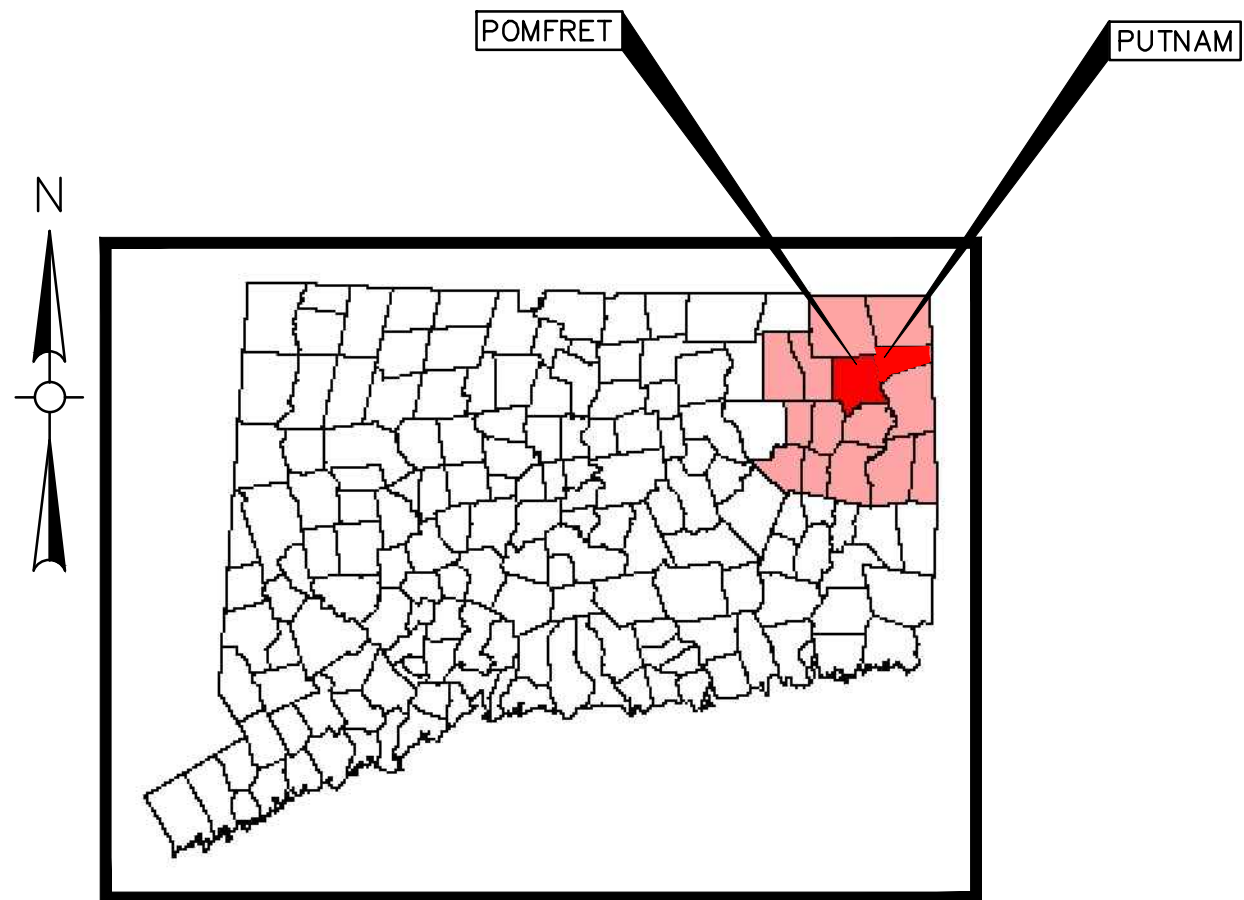
# AIR LINE TRAIL

## RECONSTRUCTION AND ACCESSIBILITY PROJECT

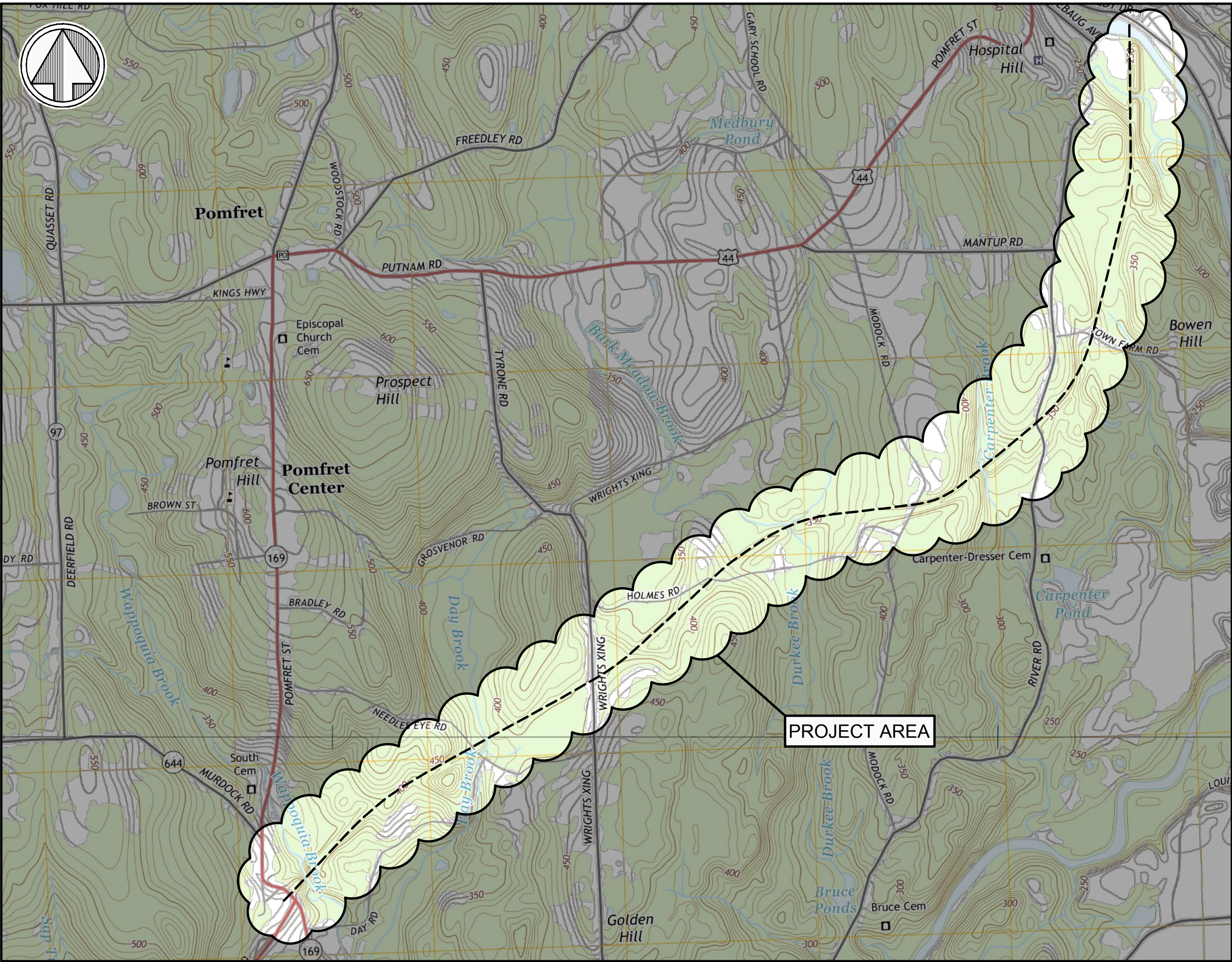
TAP PROJECT NO. 111-126

W & S PROJECT NO. ENG22-1336

TOWNS OF PUTNAM AND POMFRET, CT



NOT TO SCALE



LOCATION MAP  
SCALE: 1"=1500'

**AUGUST 28, 2023**

**Prepared By**

**Weston & Sampson<sup>SM</sup>**

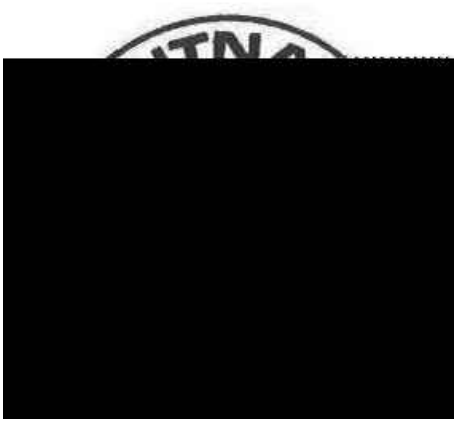
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MAUREEN A. NICHOLSON  
FIRST SELECTMAN



BARNEY SENEY  
MAYOR

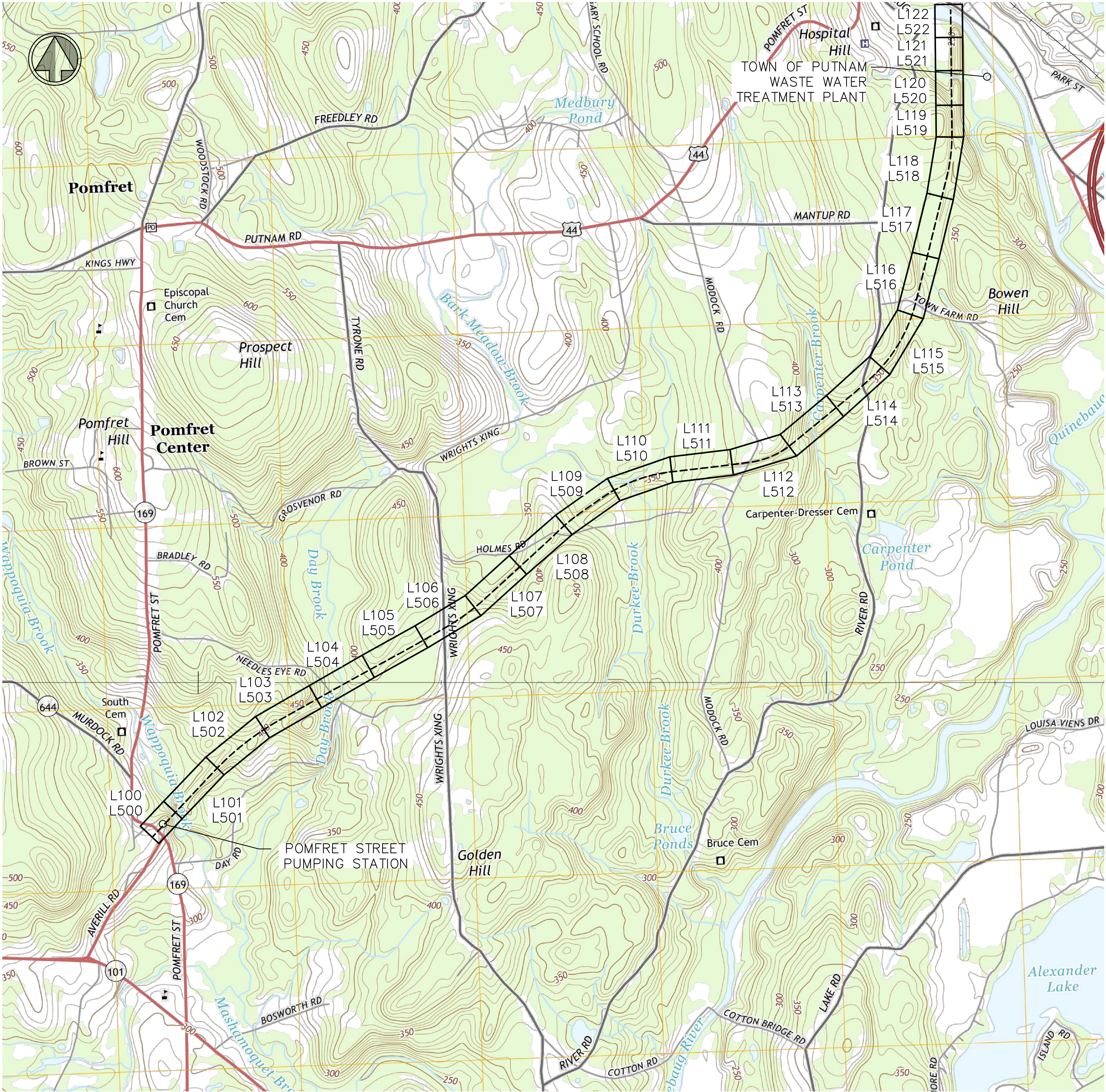
### SHEET INDEX

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L800-L810.....	CRITICAL CROSS SECTIONS



Know what's below.  
Call before you dig.





DRAWING INDEX MAP  
SCALE: 1"=1000'

Project:  
TAP PROJECT NO. 111-126



AIR LINE TRAIL  
RECONSTRUCTION AND  
ACCESSIBILITY PROJECT  
TOWNS OF PUTNAM AND  
POMFRET, CT

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Reviewed By: RC  
Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ----

Drawing Title:  
**INDEX PLAN**

Sheet Number:  
**L001**



P:\CT\Projects\CT\Drawings\111-2052\CONSTRUCTION\TAP\Legend.dwg, Legend.dwg, Abbreviations and Notes.dwg

LEGEND		
DESCRIPTION	EXISTING	PROPOSED
SANITARY SEWER	----	==8"S PVC==
FORCE MAIN	----	==6"FM PVC==
LOW PRESSURE FORCE MAIN	----	==6"LP PVC==
WATER MAIN	----	==8"W DI==
STORM DRAIN	----	==18"D RCP==
GAS	----	==4"G==
ELECTRIC	----	==E==
TELEPHONE	----	==T==
HOUSE CONNECTION		
WATER SERVICE		
GRINDER PUMP	○ GP	● GP
SANITARY SEWER MANHOLE	⊙	● SMH
STORM DRAIN MANHOLE	⊙	● DMH
ELECTRICAL MANHOLE	⊙	
TELEPHONE MANHOLE	⊙	
AIR RELEASE VALVE MANHOLE		● ARMH
FORCE MAIN CLEANOUT MANHOLE		● FMMH
CLEANOUT		● CO
CATCH BASIN	■	■
HYDRANT	⌵	⬆
TEMPORARY HYDRANT		⊕
GATE VALVE	⌵	⌵
CHECK VALVE	⌵	⌵
BUTTERFLY VALVE	⌵	⌵
BALL VALVE	⌵	⌵
CURB STOP	⌵	⌵
REDUCER	◁	◁
CAP OR PLUG	⌈	⌈
GAS GATE VALVE	⌵	
UTILITY POLE	⌵	
GUY POLE	⌵	
GUY WIRE	---	
OVERHEAD WIRES	----	
LIGHT POST	☆	
EDGE OF PAVEMENT	----	----
EDGE OF UNPAVED ROAD	----	----
CURB	----	----
SIDEWALK	○○○○○○○○○○	----
STONE WALL	○○○○○○○○○○	----
RETAINING WALL	RET WALL	RET WALL
WIRE FENCE	---	---
CHAIN LINK FENCE	---	---
DECIDUOUS TREE	⊙	⊙
EVERGREEN TREE	⊙	⊙
TREE LINE	~~~~~	~~~~~
SURVEY MARKER	□	
CONTROL POINT	△	
BENCH MARK	⊙	
PROPERTY LINE	----	----
EASEMENT LINE	----	----
CONTOUR	56	56
SPOT ELEVATIONS	x 100.2	x101.5
BUILDING/BUILDING NUMBER	#35	
FLOOR ELEVATION	FF=56.7	
SILL ELEVATION	S=56.7	
LIMIT OF WORK		
WETLAND/WETLAND FLAG	---	---
EDGE OF WATER	---	---
DRAINAGE DITCH/SWALE	---	---
RIP RAP	⊙	⊙
BOLLARD	○ B	
SIGN	---	
GUIDE RAIL	---	---
PERCOLATION TEST	PT	PT
TEST PIT	TP1	TP1
BORING	B1	B4
PROBE	P-1	P-1
GROUNDWATER MONITORING WELL	WSE-1	CW-1
HAY BALES		
SILT FENCE		
ROCK OUTCROP		

NOTE: ITEMS SHOWN IN THE LEGEND MAY NOT BE PRESENT IN THESE PLANS

ABBREVIATIONS

AC	ASBESTOS CEMENT PIPE, TRANSITE
ACCOMP	ASPHALT COATED CORRUGATED METAL PIPE
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS
BC	BITUMINOUS CONCRETE
BIT	BITUMINOUS
BLDG	BUILDING
BM	BENCH MARK
C900	C900 POLYVINYL CHLORIDE
CATV	CABLE TELEVISION
CB	CATCH BASIN
CC	CONCRETE CURB
CI	CAST IRON
CL	CENTERLINE
CMP	CORRUGATED METAL PIPE
CNO	COULD NOT OPEN
CONC	CONCRETE
CTDOT	CONNECTICUT DEPARTMENT OF TRANSPORTATION
CU FT	CUBIC FEET
CY	CUBIC YARD
D	STORM DRAIN
DI	DROP INLET, DUCTILE IRON
DIA, Ø	DIAMETER
D INV	DEPTH TO INVERT OF EXISTING PIPE
DMH	DRAIN MANHOLE
E	ELECTRIC, EAST
EA	EACH
ELEV	ELEVATION
EOP	EDGE OF PAVEMENT
EW	EACH WAY
EX	EXISTING
FM	FORCE MAIN
FT	FEET, FOOT
G	NATURAL GAS
GALV	GALVANIZED
GC	GRANITE CURB
GM	GAS MAIN
GS	GAS SERVICE
HC	HOUSE CONNECTION
HDD	HORIZONTAL DIRECTIONAL DRILLING
HDPE	HIGH DENSITY POLYETHYLENE PIPE
HP	HIGH PRESSURE
HYD	FIRE HYDRANT
INV	INVERT
ID	INSIDE DIAMETER
IP	IRON PIPE
LF	LINEAR FEET
LS	LUMP SUM
MAX	MAXIMUM
MB	MAIL BOX
MECH	MECHANICAL
MH	MANHOLE
MIN	MINIMUM
MISC	MISCELLANEOUS
MJ	MECHANICAL JOINT
MWT	MERIDEN-WATERBURY TURNPIKE
N	NORTH
NF	NOT FOUND
NO OR #	NUMBER
PE	POLYETHYLENE
PL	PROPERTY LINE
PVC	POLYVINYL CHLORIDE
PR	PROPOSED
PVMT	PAVEMENT
RC	REINFORCED CONCRETE
ROW	RIGHT-OF-WAY
S	SEWER, SOUTH
SCAV	SEWER COMBINATION AIR VALVE STRUCTURE
SERV	SERVICE
SF	SQUARE FEET
SM	SEWER MAIN
SPEC	SPECIFICATIONS
SQ FT	SQUARE FEET
SS	SEWER SERVICE
STA	STATION
SW	SIDEWALK
T	TELEPHONE
TBM	TEMPORARY BENCH MARK
TNPK	TURNPIKE
TR	TRAFFIC, TRAFFIC CONDUIT
TYP	TYPICAL
UP	UTILITY POLE
VC	VITRIFIED CLAY
VERT	VERTICAL
W	WATER, WEST
WM	WATER MAIN
WS	WATER SERVICE

CONSTRUCTION NOTES:

- THE CONTRACTOR SHALL CONTACT "CALL BEFORE YOU DIG" (CBYD) AT 1-800-922-4455 AT LEAST 72 HOURS, SATURDAYS, SUNDAYS, AND HOLIDAYS EXCLUDED, PRIOR TO EXCAVATING AT ANY LOCATION. A COPY OF THE CBYD PROJECT REFERENCE NUMBER(S) SHALL BE GIVEN TO THE OWNER PRIOR TO EXCAVATION.
- LOCATIONS OF EXISTING PIPES, CONDUITS, UTILITIES, FOUNDATIONS AND OTHER UNDERGROUND OBJECTS ARE NOT WARRANTED TO BE CORRECT AND THE CONTRACTOR SHALL HAVE NO CLAIM ON THAT ACCOUNT SHOULD THEY BE OTHER THAN SHOWN.
- TEST PITS TO LOCATE EXISTING UTILITIES MAY BE REQUIRED BY THE ENGINEER.
- ALL PAVEMENT OUTSIDE OF THE LIMITS SHOWN WHICH IS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED IN ACCORDANCE WITH THE SPECIFICATIONS AND AS SHOWN ON THE DRAWINGS.
- ALL AREAS DISTURBED BY THE CONTRACTOR BEYOND PAYMENT LIMITS SHALL BE RESTORED AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR SHALL MAINTAIN SIDE SLOPES AND DRAINAGE SWALES DURING CONSTRUCTION TO PREVENT PONDING AND EROSION.
- THE CONTRACTOR SHALL NOT STORE ANY APPARATUS, MATERIALS, SUPPLIES, AND EQUIPMENT ON DRAINAGE STRUCTURES OR WITHIN 100 FEET OF WETLANDS.
- THE CONTRACTOR SHALL INSTALL THE EROSION CONTROL MEASURES BEFORE BEGINNING OTHER WORK ON SITE.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET PROTECTION ON ALL EXISTING CATCH BASINS THROUGHOUT THE DURATION OF THE PROJECT.
- THE CONTRACTOR SHALL NOTIFY WATER AND SEWER SERVICE PROVIDERS AT LEAST 72 HOURS PRIOR TO EXCAVATING NEAR SEWER AND WATERLINES.
- ALL STREET EXCAVATIONS SHALL BE COMPLETELY CLOSED AT THE END OF EACH WORKING DAY BY BACKFILLING. COVERING WITH STEEL PLATES MAY BE ALLOWED IF APPROVED BY THE ENGINEER AND CT DOT.
- DURING THE PROCESS OF WORK, THE CONTRACTOR SHALL CONDUCT OPERATIONS AND MAINTAIN THE AREA OF CONSTRUCTION ACTIVITIES, INCLUDING SWEEPING AND SPRINKLING OF STREETS AS NECESSARY OR AS REQUIRED, TO MINIMIZE CREATION AND DISPERSION OF DUST.
- A TRAFFIC CONTROL PLAN SHALL BE FOLLOWED AS STATED IN THE SPECIFICATIONS. SIGNAGE SHALL BE PROVIDED AS NECESSARY OR AS REQUIRED.
- WHERE EXISTING FENCES ARE TO BE REMOVED AND RESET, A TEMPORARY CONSTRUCTION FENCE SHALL BE ERECTED AFTER REMOVAL FOR THE PROTECTION OF THE PUBLIC.
- ALL DOT HIGHWAY LINE MONUMENTATION AND PRIVATE PROPERTY MONUMENTATION WITHIN THE PROJECT LIMITS MUST BE DELINEATED AND PROTECTED FROM DAMAGE OR MOVEMENT. ANY COST ASSOCIATED WITH RESETTING OF THE MONUMENTATION SHALL BE AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL COMPLETE ALL LAYOUTS, SURVEYS, ETC. REQUIRED FOR CONSTRUCTION OF THE PROJECT AS SHOWN AND AS SPECIFIED.
- ALL WORK PERFORMED WITHIN STATE HIGHWAY LIMITS SHALL CONFORM TO THE PERMIT ISSUED FOR THIS PROJECT.
- LIMITS OF CLEARING SHALL BE FIELD LOCATED WITH OWNERS AND ENGINEER PRIOR TO CONSTRUCTION.
- HISTORICAL RAILROAD MONUMENTS MAY BE ENCOUNTERED DURING CONSTRUCTION. ALL MONUMENTS SHALL BE LOCATED AND ADEQUATELY PROTECTED, IF AN EXISTING MONUMENT CONFLICTS WITH THE PROPOSED WORK IT SHALL BE REMOVED AND THE MONUMENT SHALL BE RESET BY A LICENSED SURVEYOR.

Project: TAP PROJECT NO. 111-126



AIR LINE TRAIL  
RECONSTRUCTION AND  
ACCESSIBILITY PROJECT  
TOWNS OF PUTNAM AND  
POMFRET, CT



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Drawn By: AMN  
Reviewed By: RC  
Approved By: MJJ

W&S Project No.: ENG22-1336  
W&S File No.: ---

Drawing Title:

LEGEND,  
ABBREVIATIONS  
AND NOTES

Sheet Number:

L002



[illegible]

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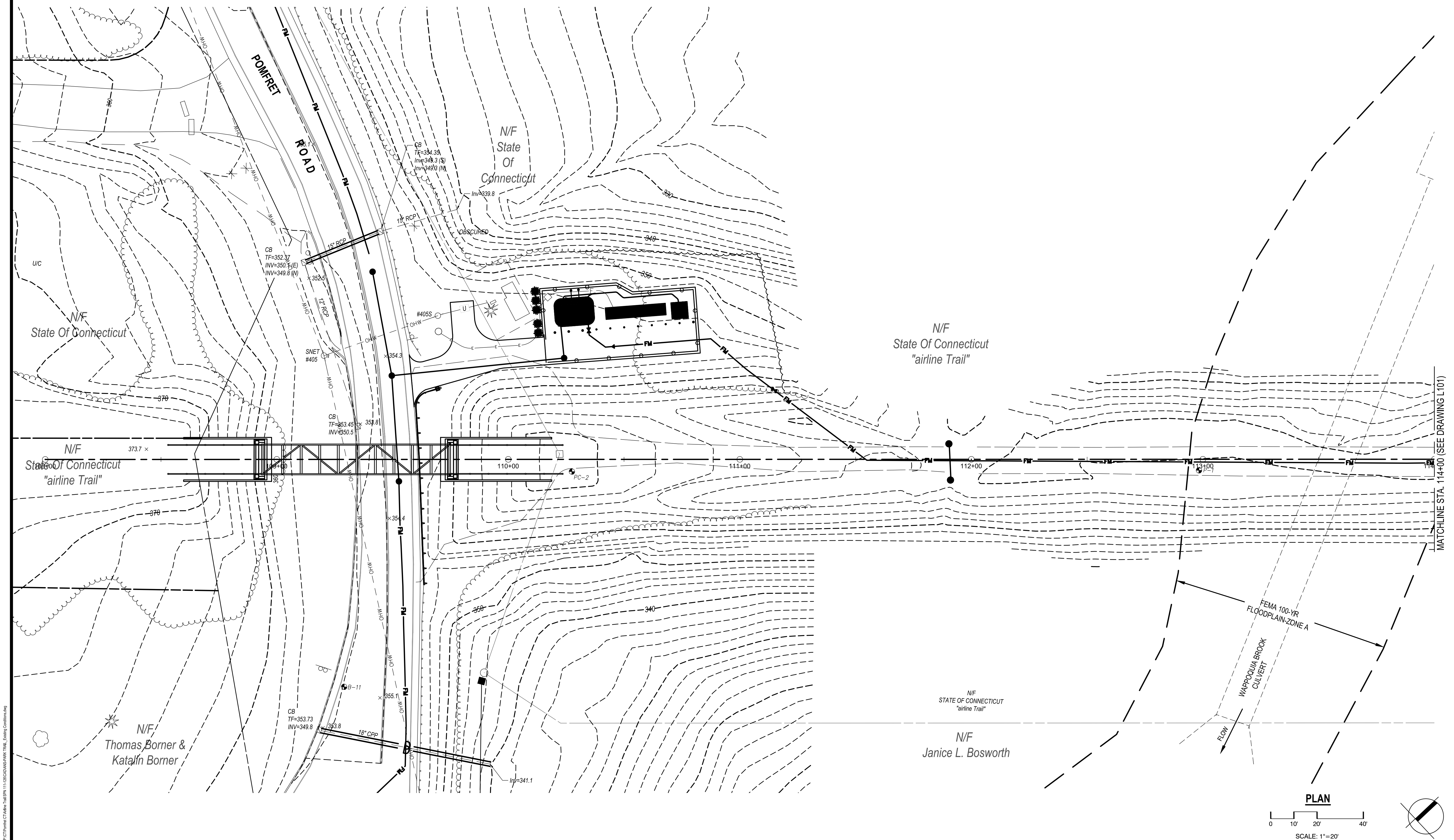
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Approved By:	MJJ
W&S Project No.:	ENG22-1336
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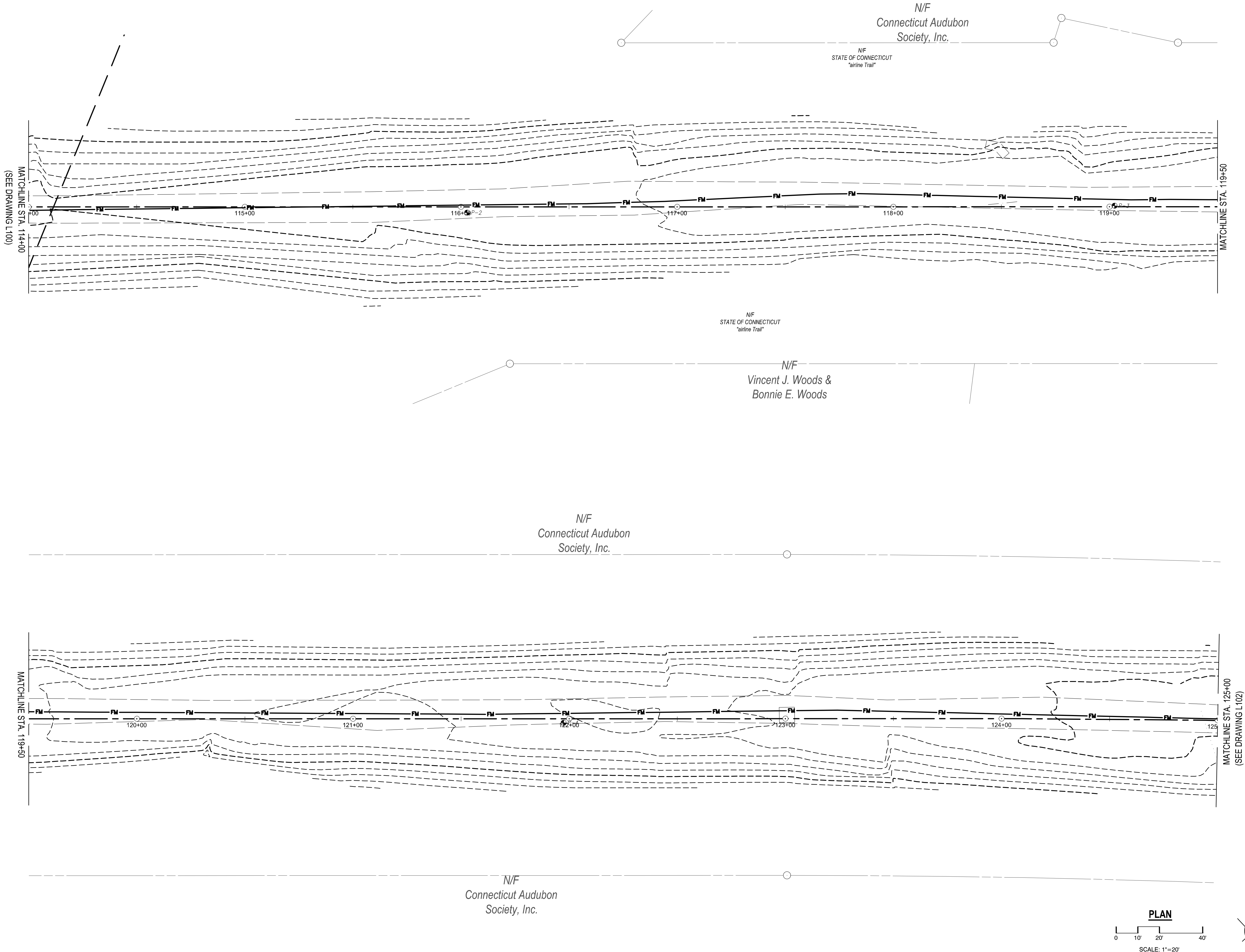
EXISTING  
CONDITIONS  
PLANS AND  
SURVEY TIES

Sheet Number:

# L100







Project:  
TAP PROJECT NO. 111-126

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PLANS AND  
SURVEY TIES**

Sheet Number:  
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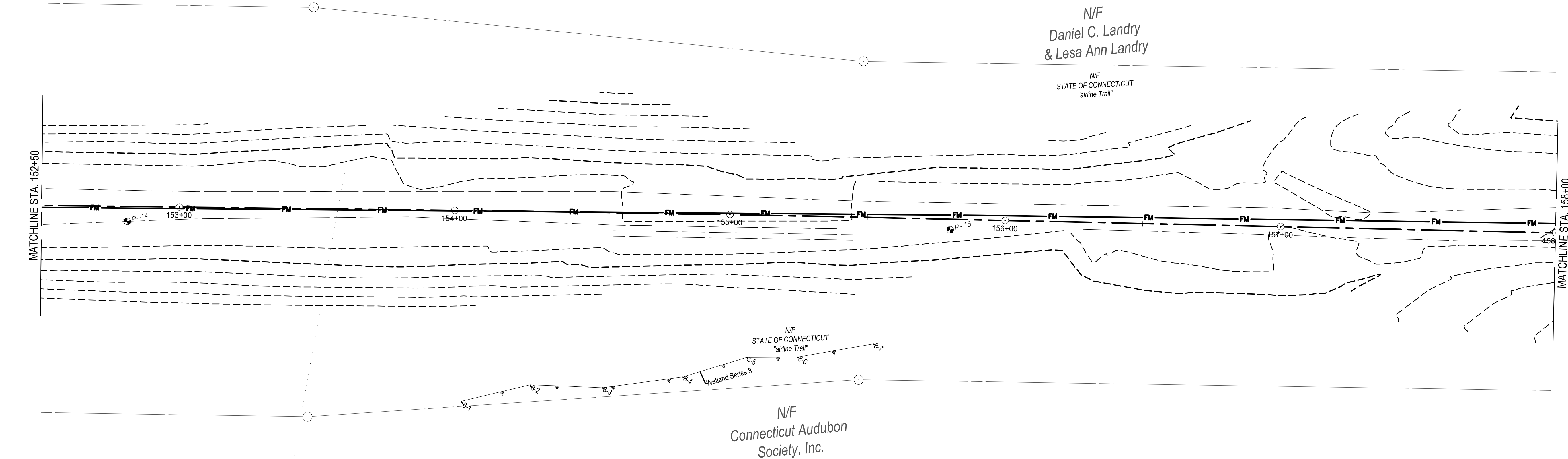
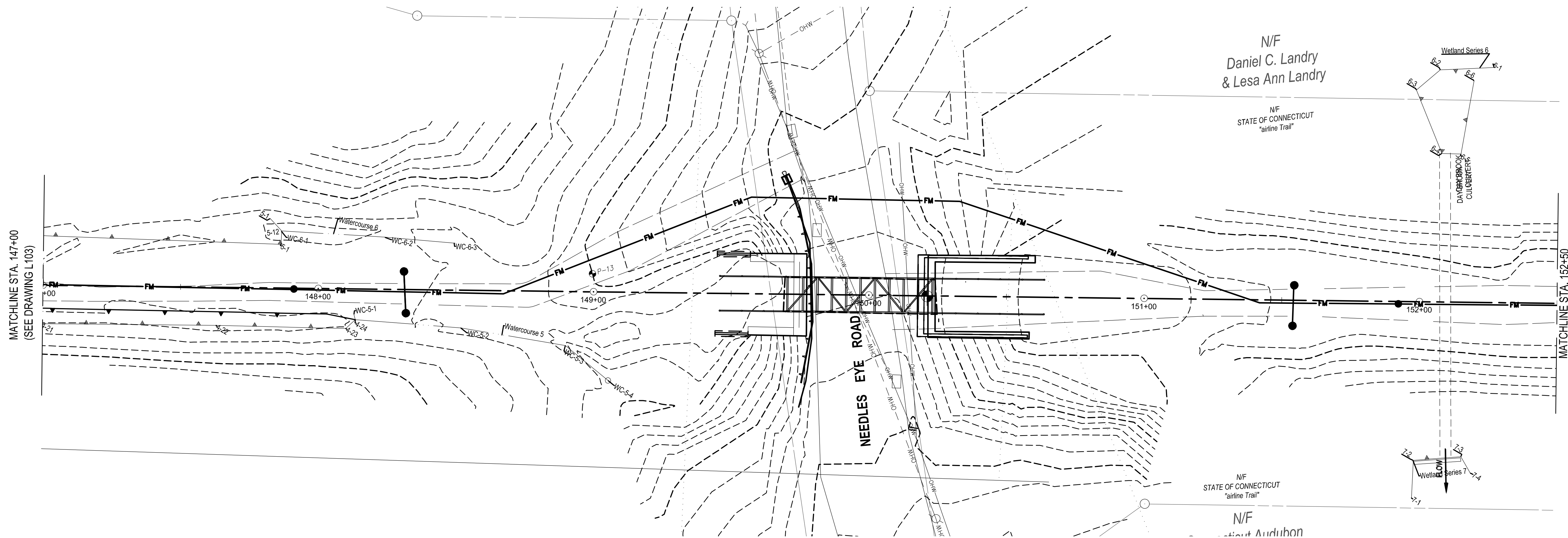




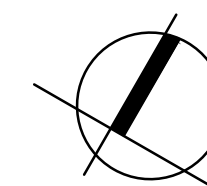




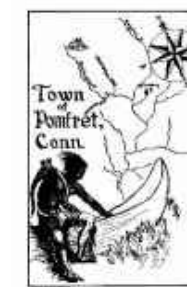




PLAN  
0 10' 20' 40'  
SCALE: 1"=20'



Project: TAP PROJECT NO. 111-126



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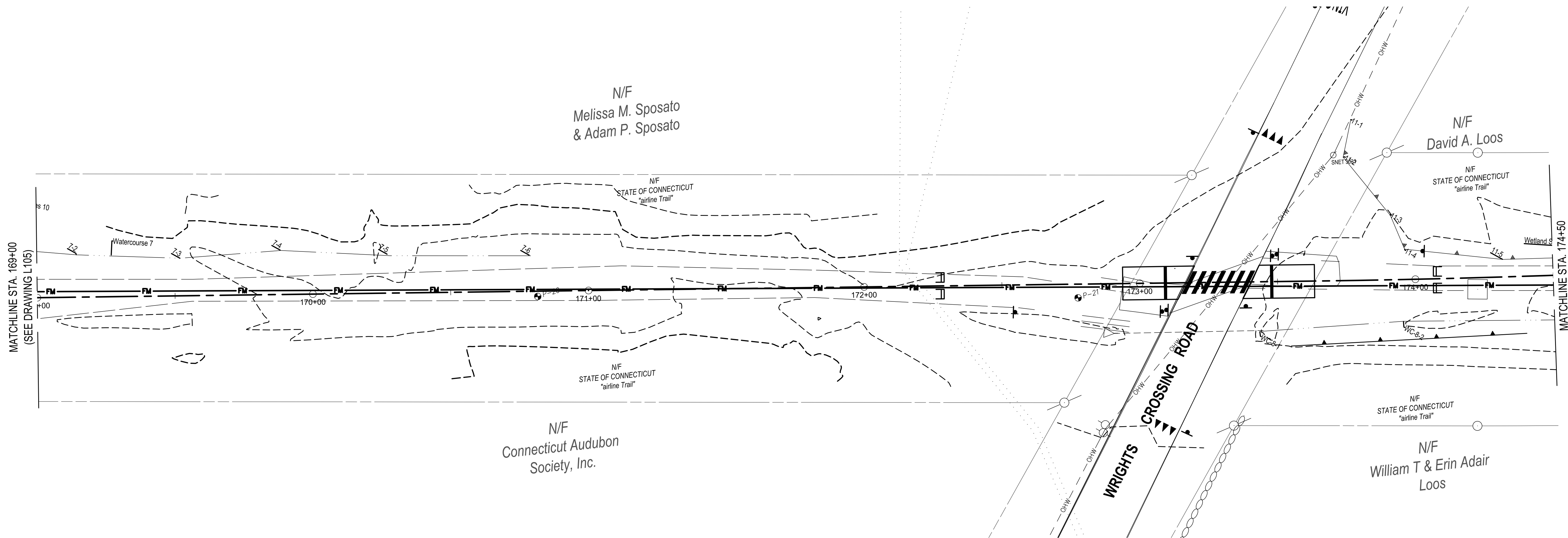
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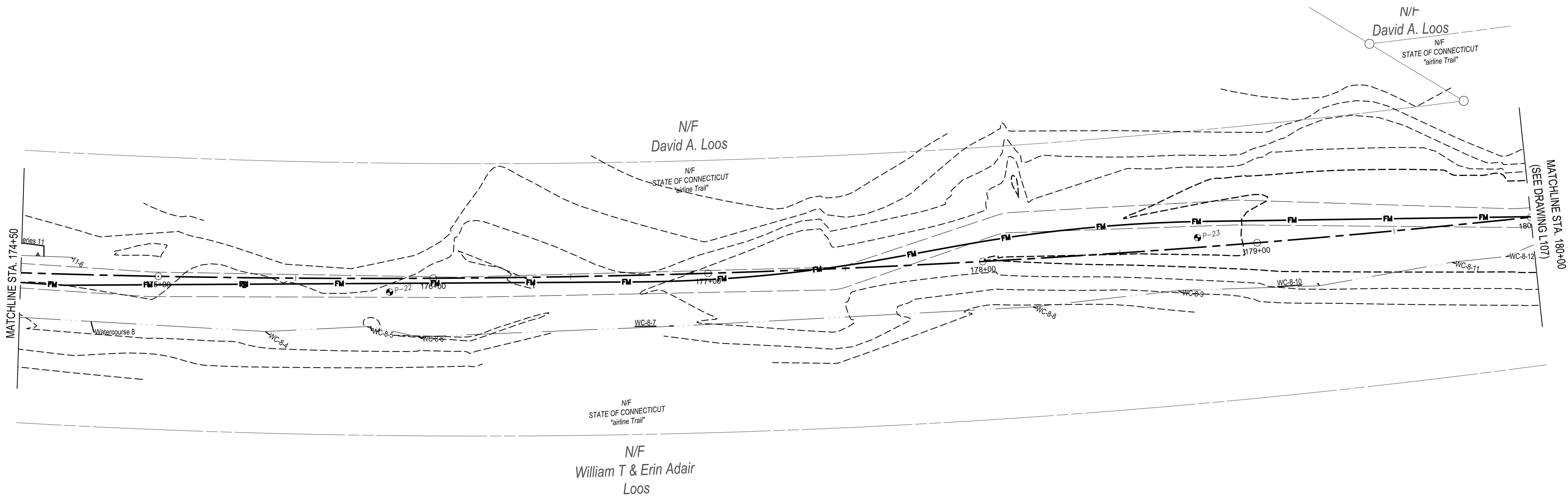








PLAN  
0 10' 20' 40'  
SCALE: 1"=20'



PLAN  
0 10' 20' 40'  
SCALE: 1"=20'

Project:  
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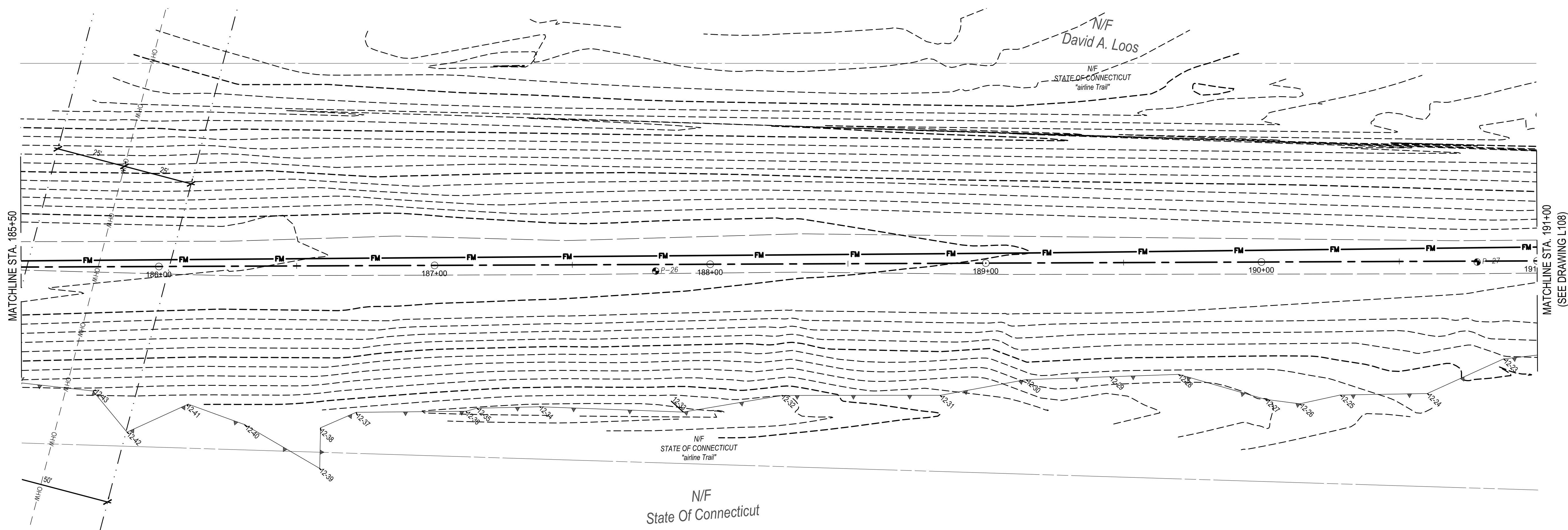
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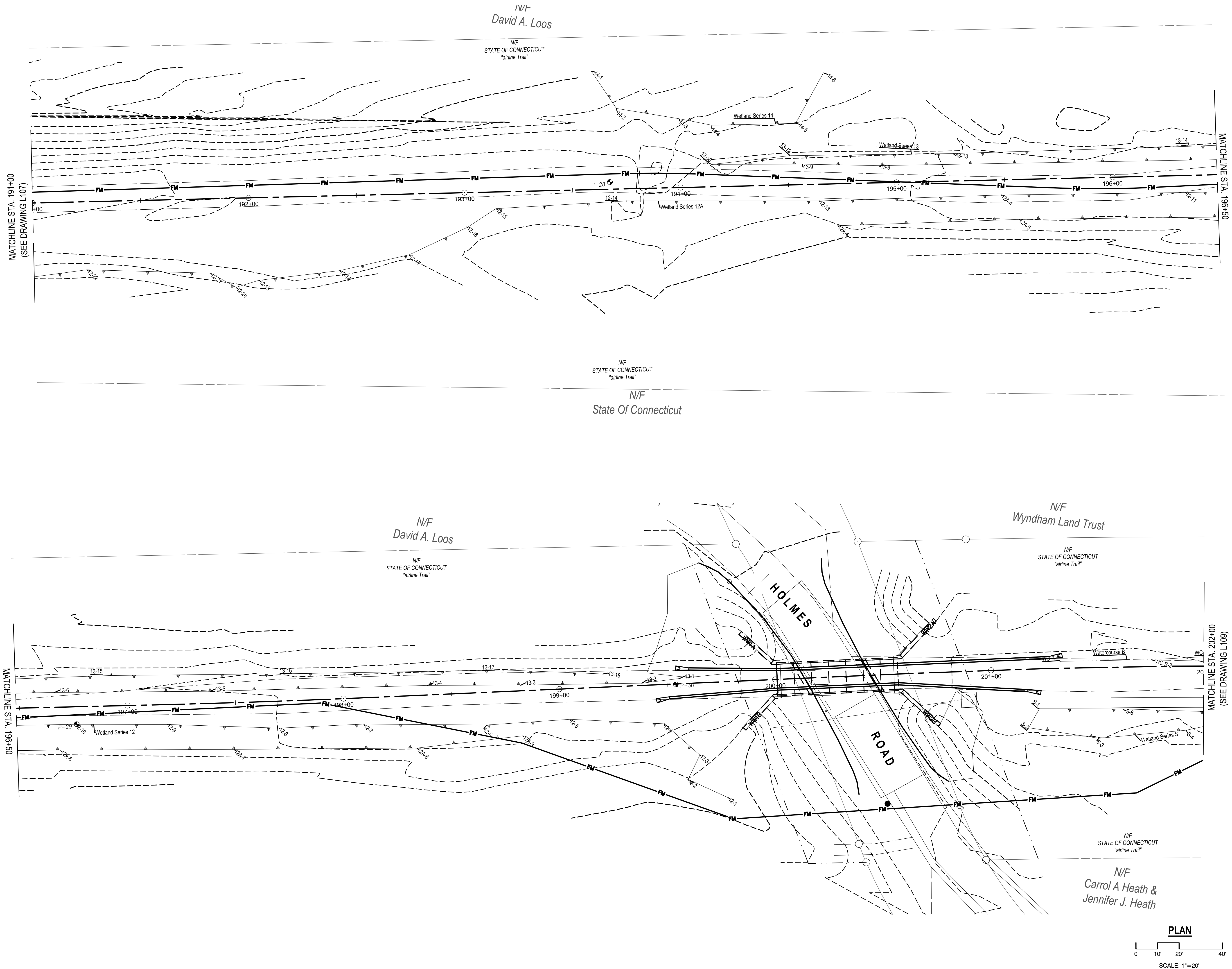
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**L106**



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Project: TAP PROJECT NO. 111-126



AIR LINE TRAIL  
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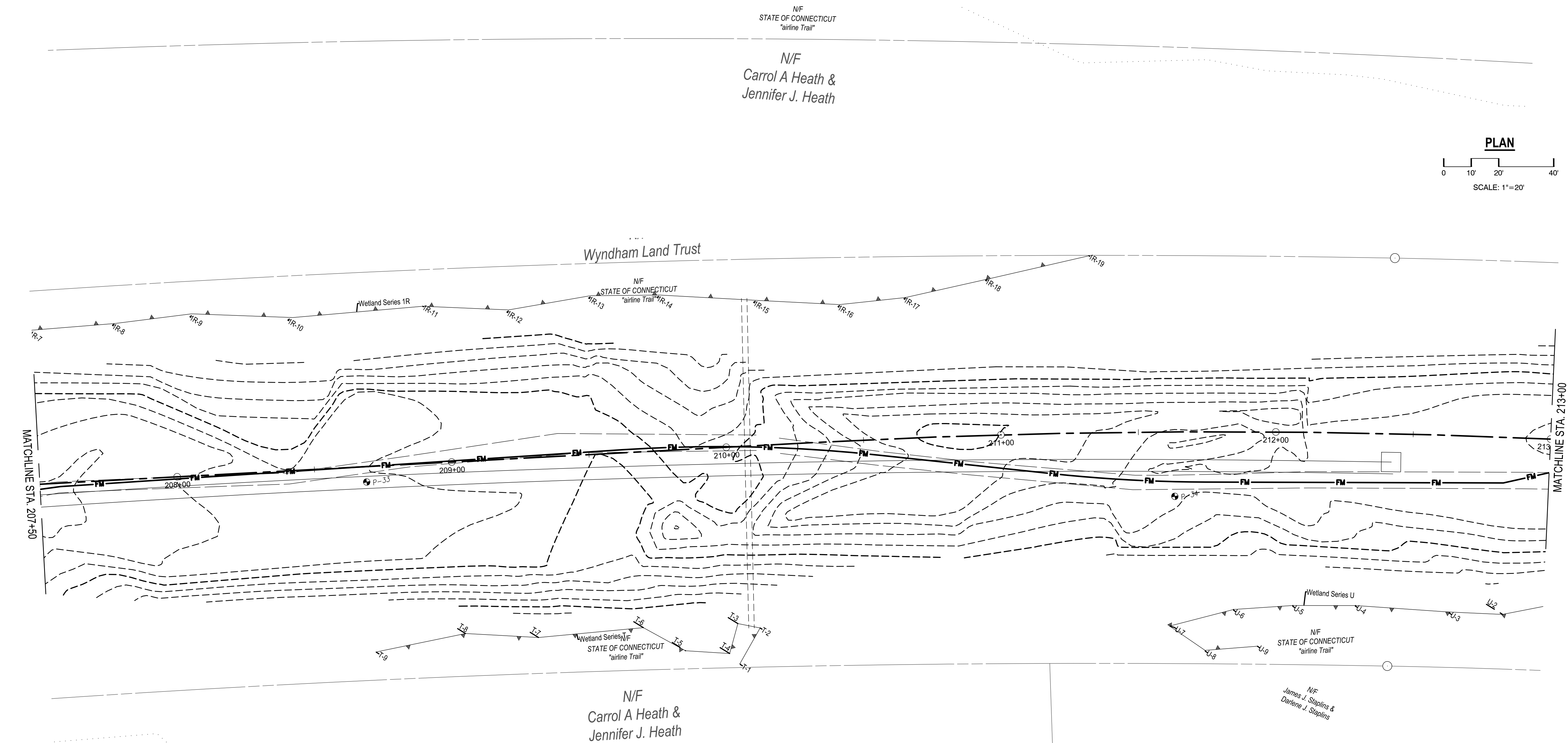
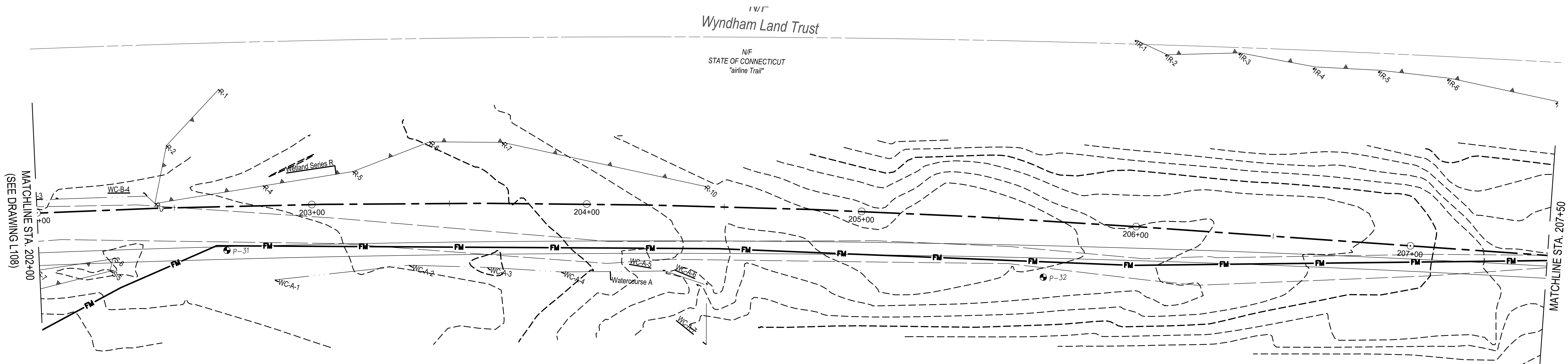
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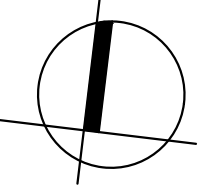
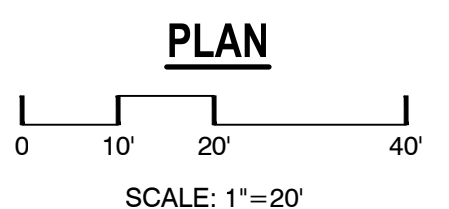
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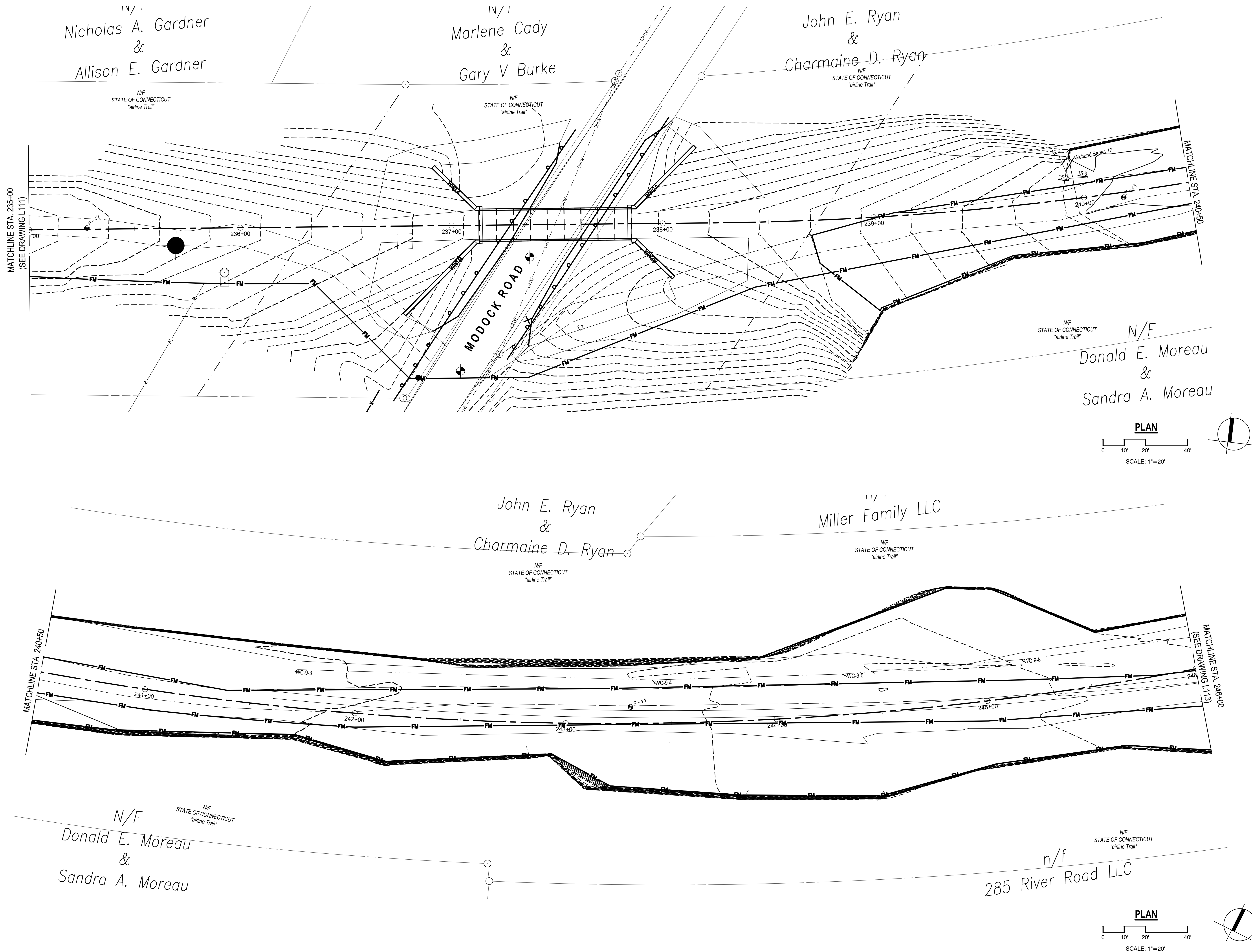
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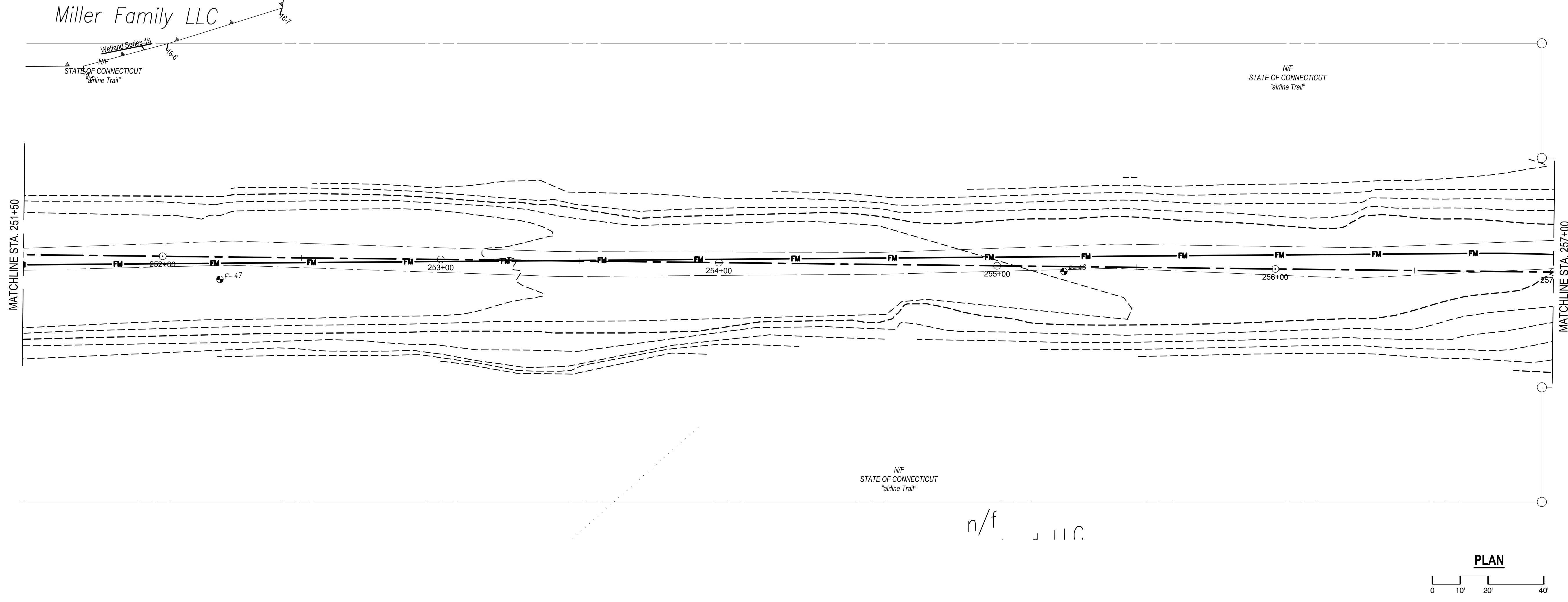
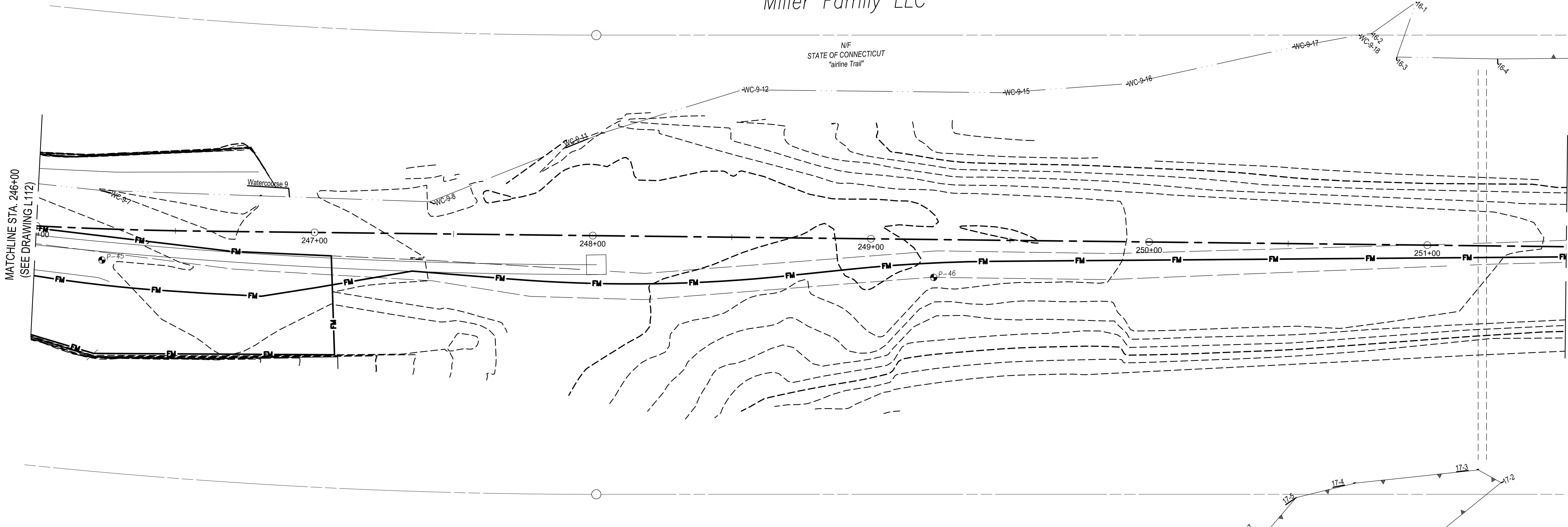
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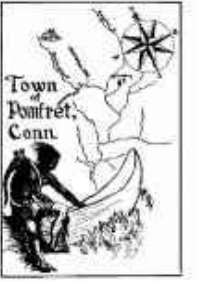
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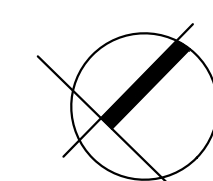
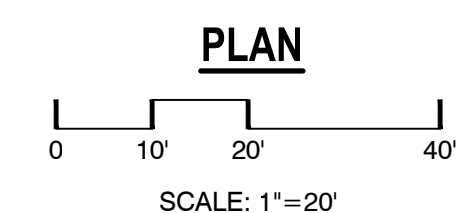
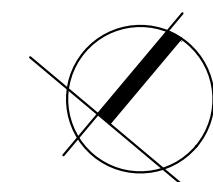
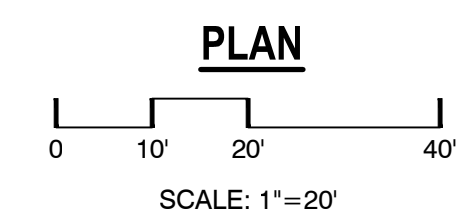
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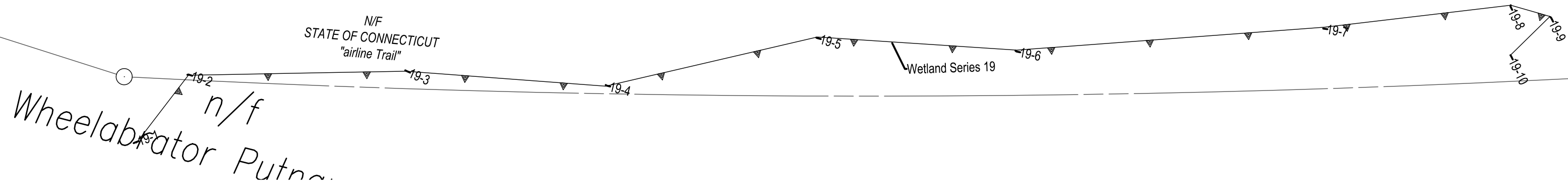
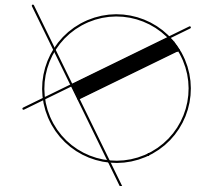
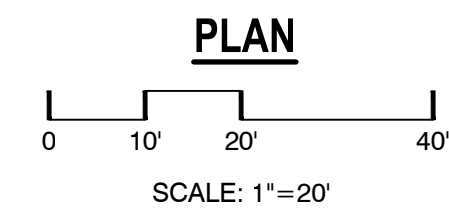
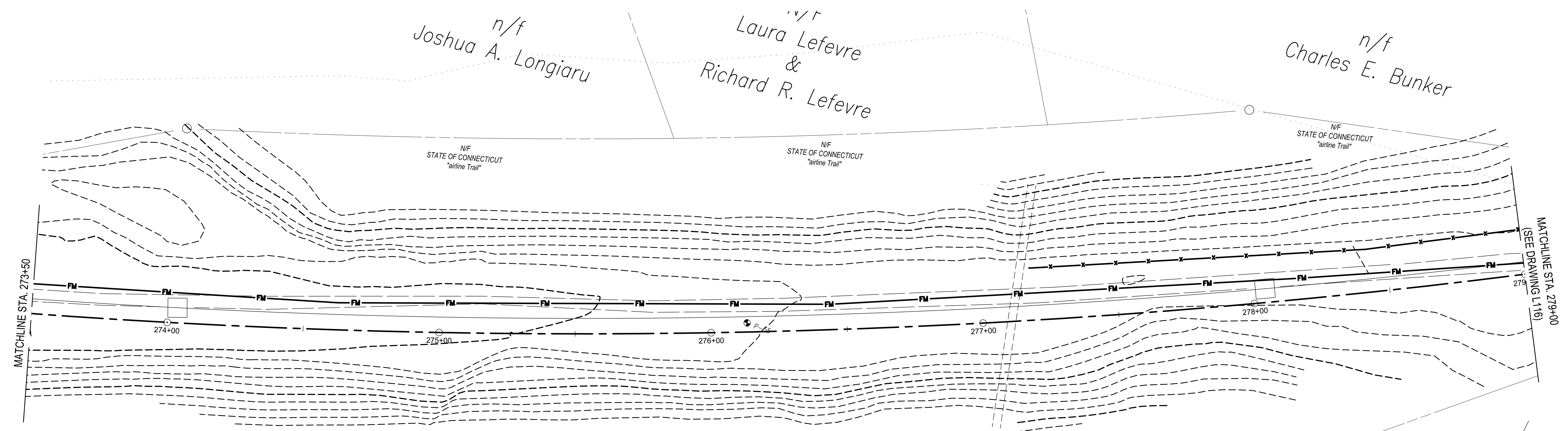
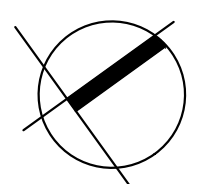
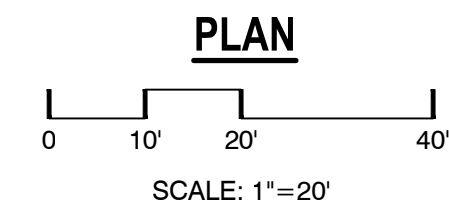
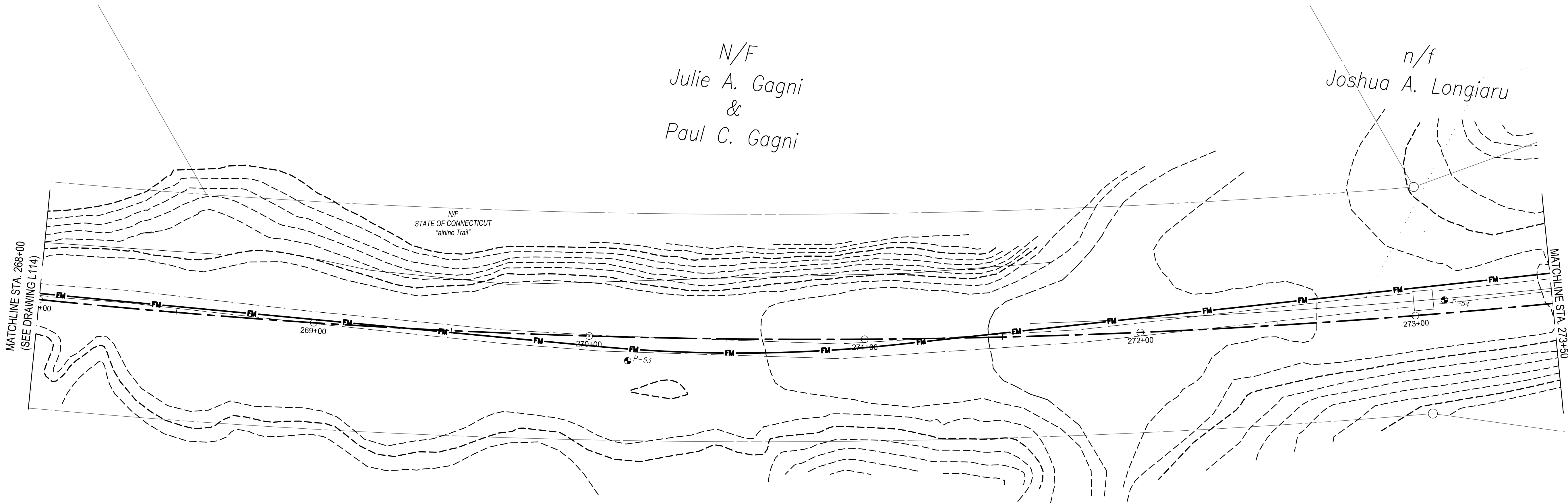
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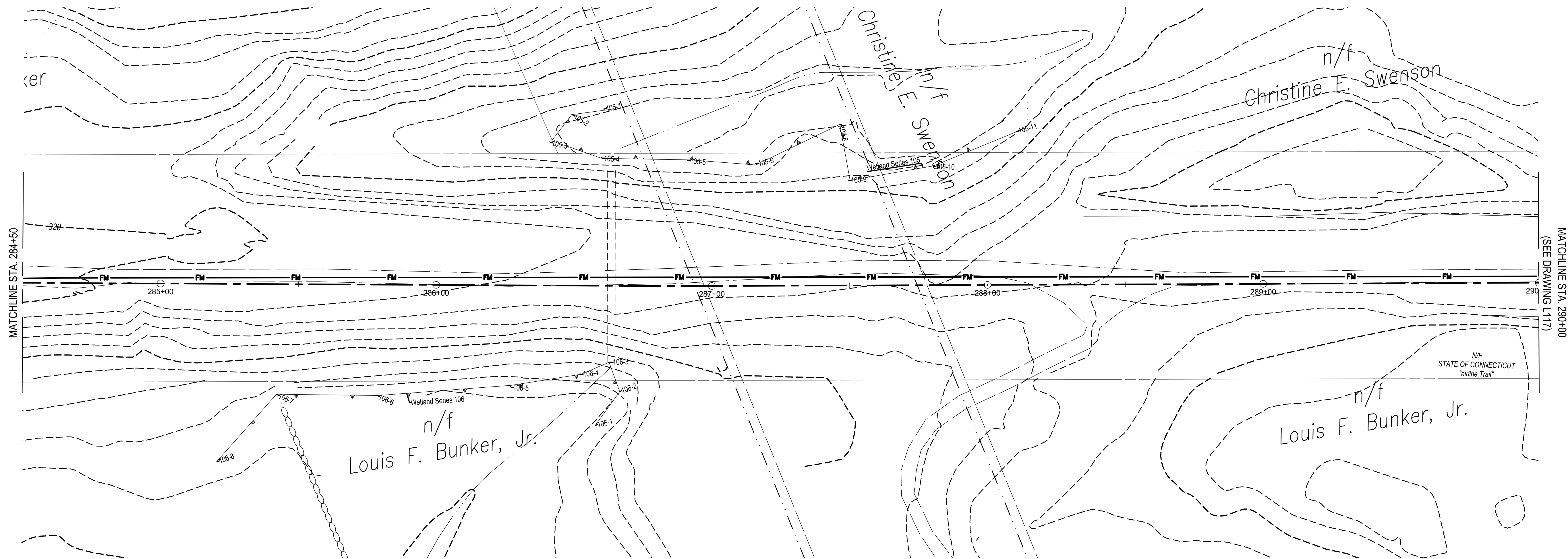
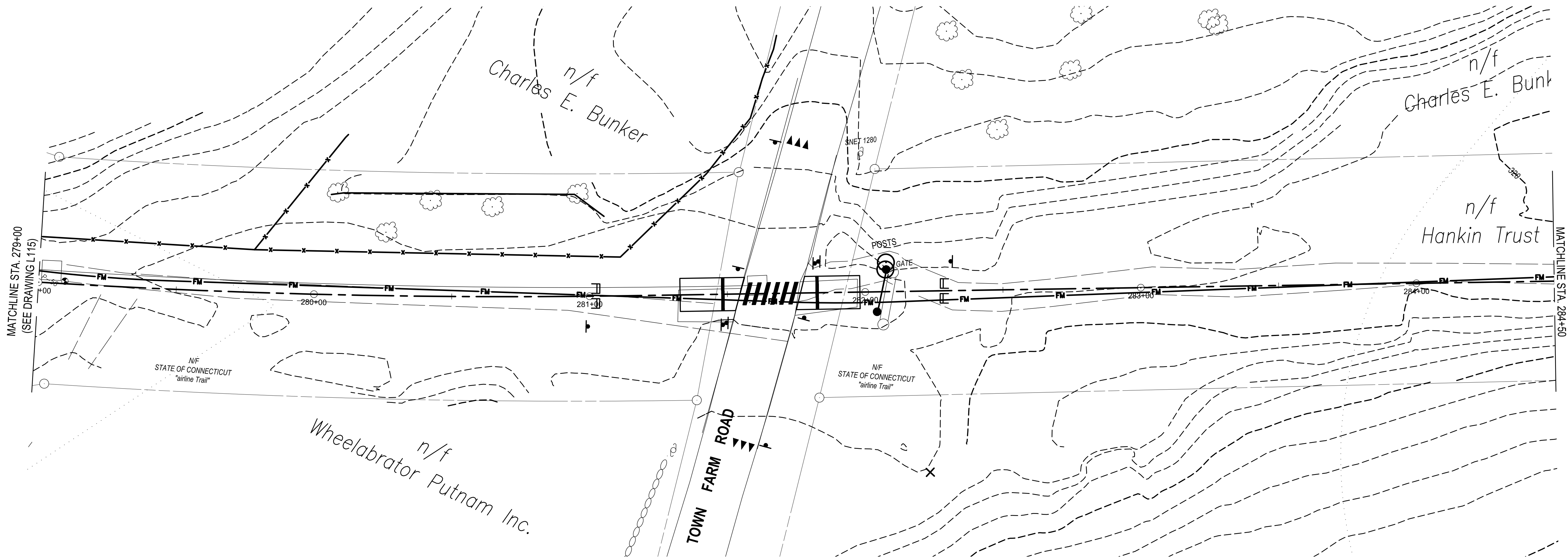
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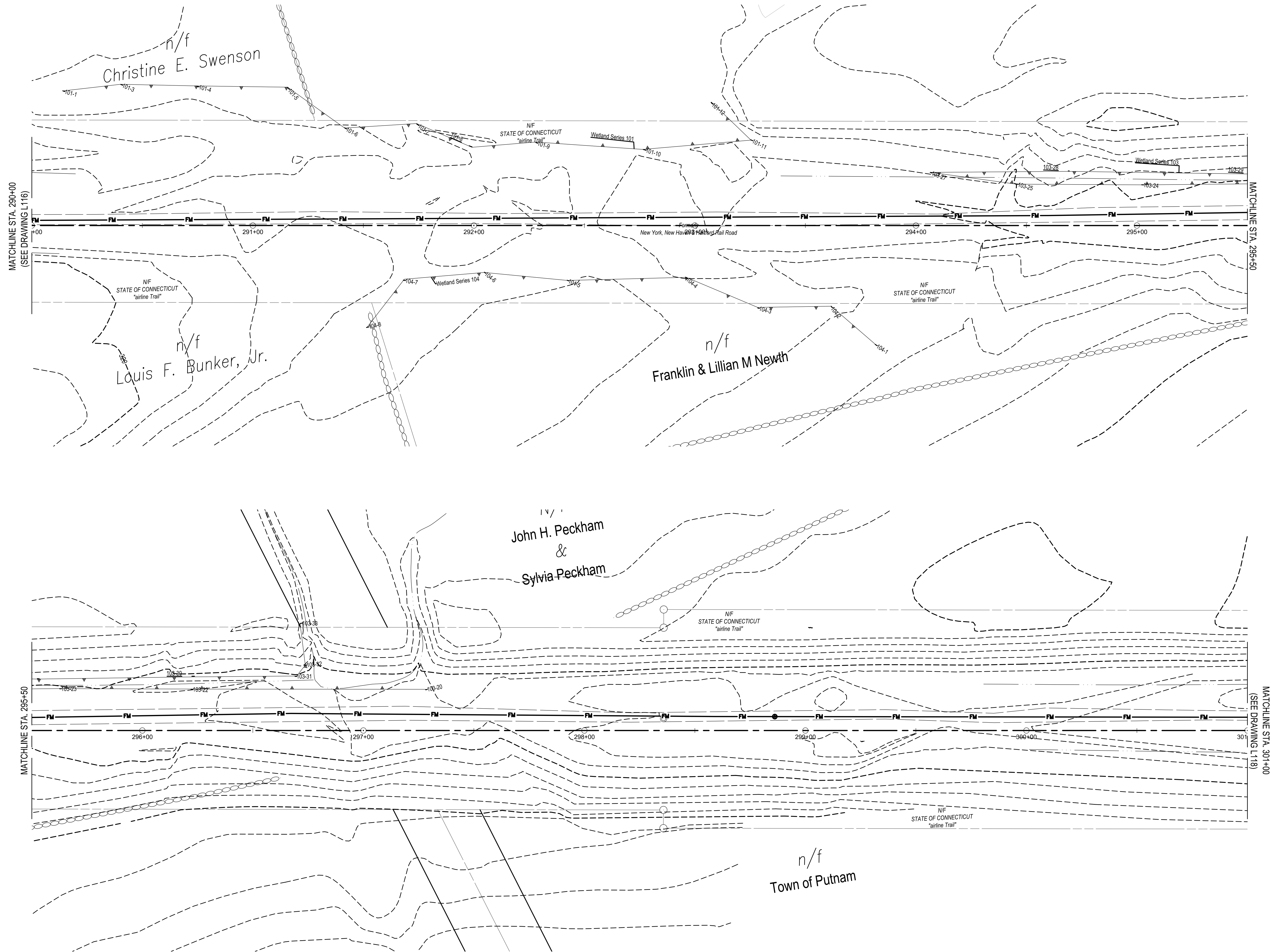
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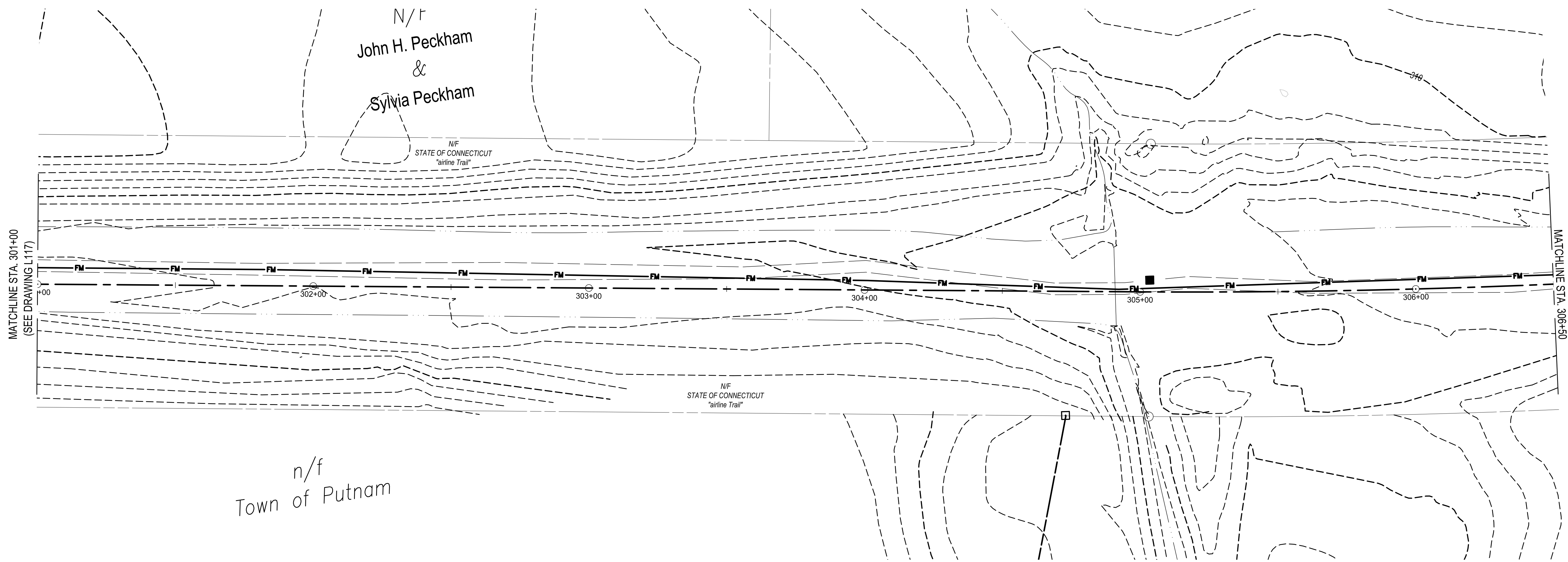
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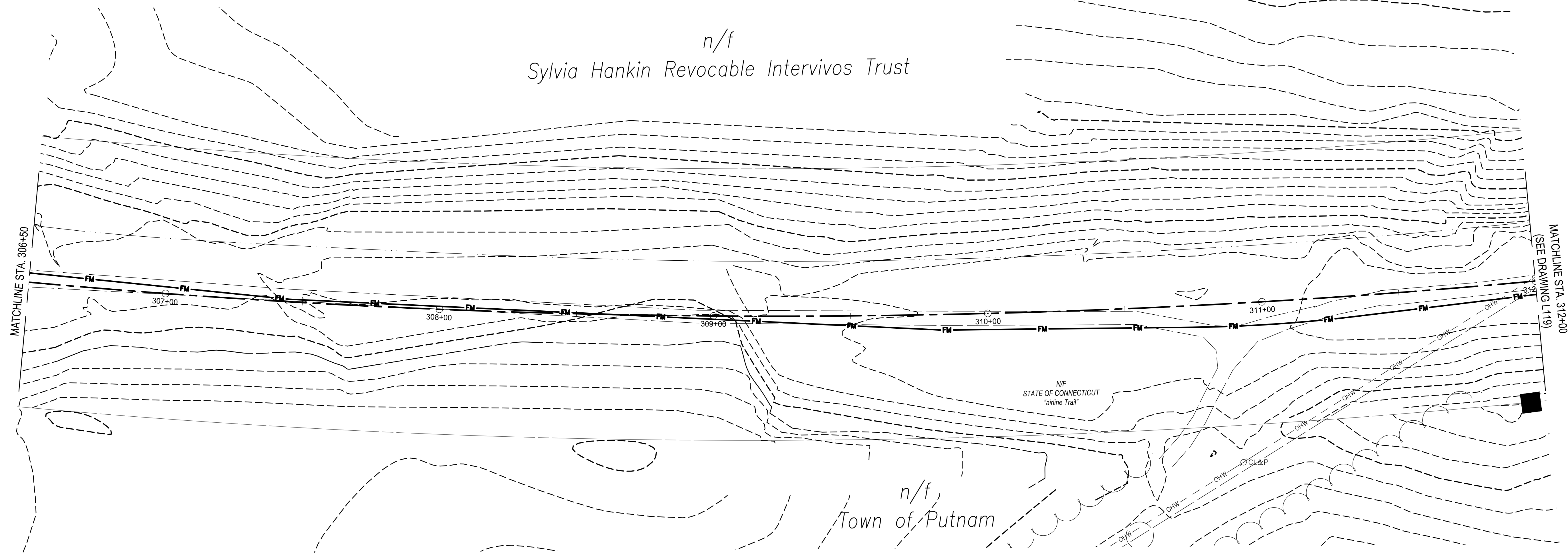
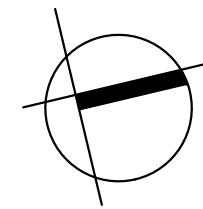
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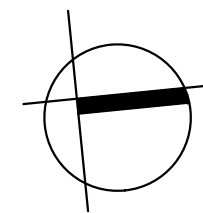
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PLAN  
0 10' 20' 40'  
SCALE: 1"=20'



PLAN  
0 10' 20' 40'  
SCALE: 1"=20'



Project: TAP PROJECT NO. 111-126



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Scale: 1"=20'

Date: August 28, 2023

Drawn By: AMN

Reviewed By: RC

Approved By: MJJ

W&S Project No.: ENG22-1336

W&S File No.: ---

Drawing Title:

EXISTING  
CONDITIONS  
PLANS AND  
SURVEY TIES

Sheet Number:

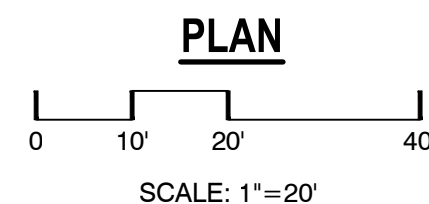
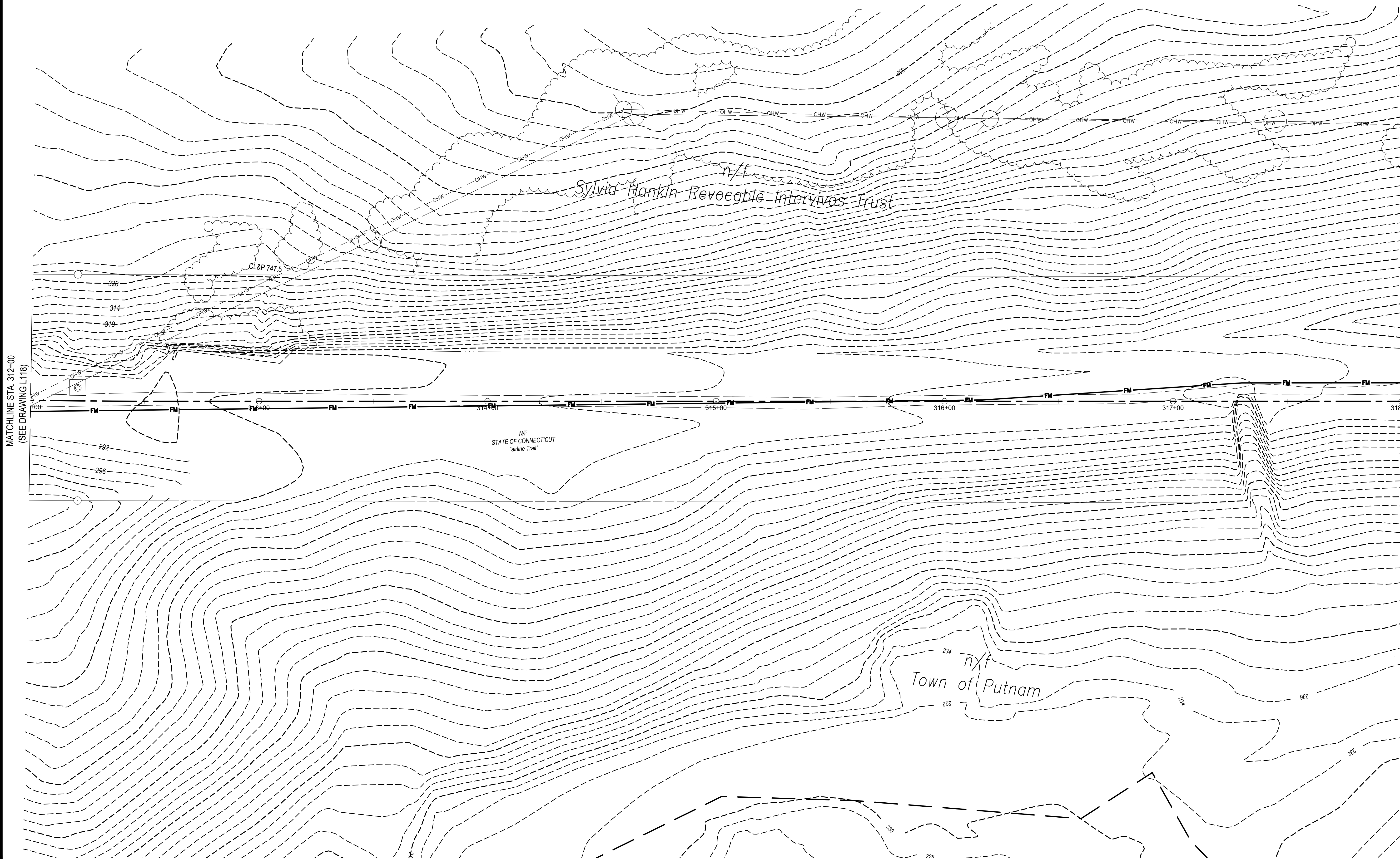
L118

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MATCHLINE STA. 312+00  
(SEE DRAWING L118)

MATCHLINE STA. 318+00  
(SEE DRAWING L120)



Project: TAP PROJECT NO. 111-126



AIR LINE TRAIL  
RECONSTRUCTION AND  
ACCESSIBILITY PROJECT  
TOWNS OF PUTNAM AND  
POMFRET, CT

Weston & Sampson

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Date: August 28, 2023  
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W&S Project No.: ENG22-1336  
W&S File No.: ---

Drawing Title:

EXISTING  
CONDITIONS  
PLANS AND  
SURVEY TIES

Sheet Number:

L119

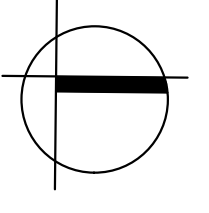
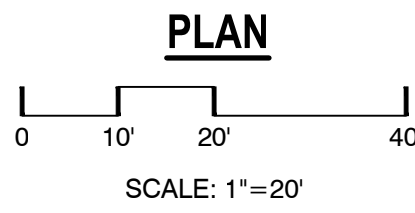
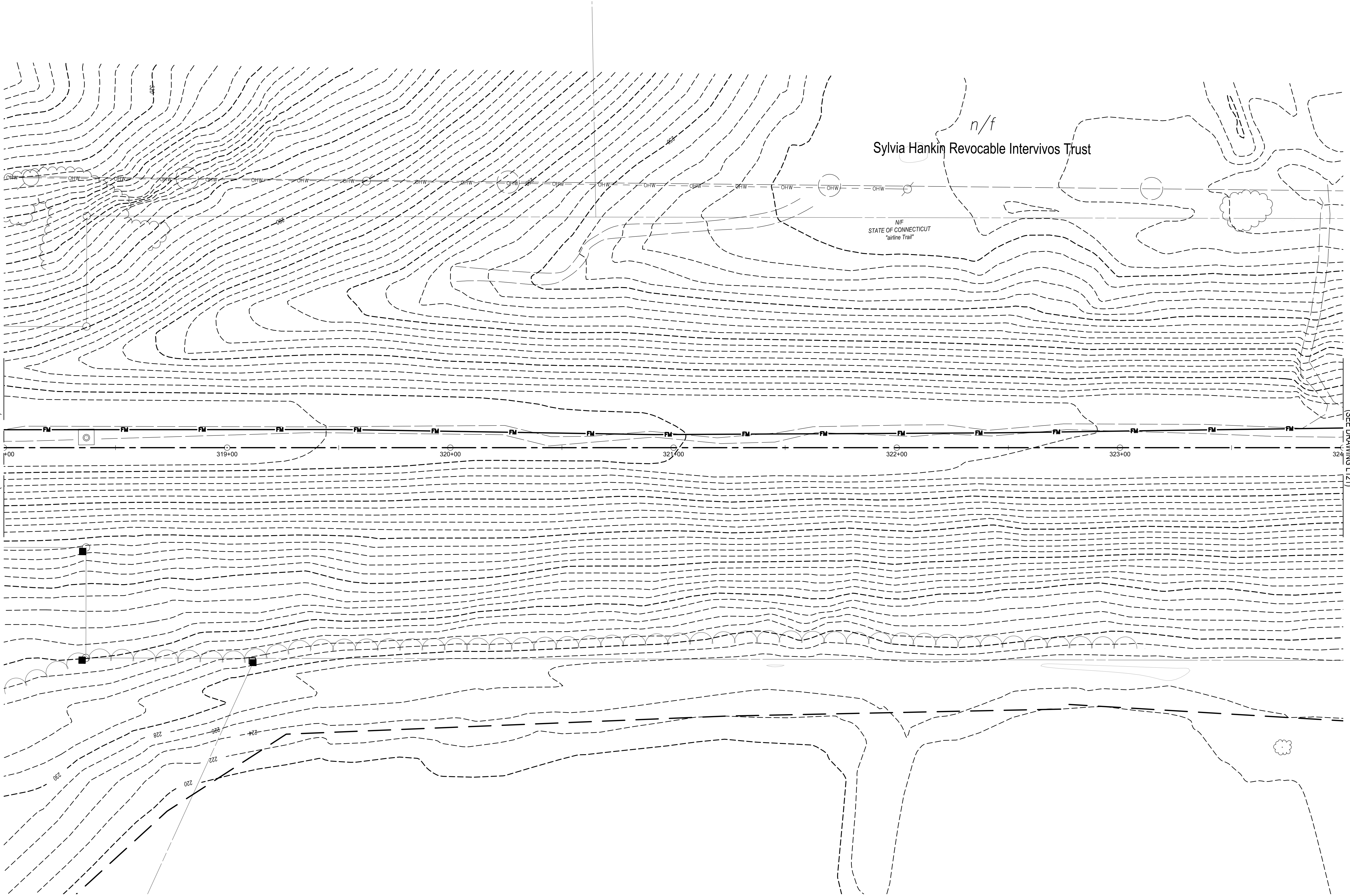
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MATCHLINE STA. 318+00  
(SEE DRAWING L119)

MATCHLINE STA. 324+00  
(SEE DRAWING L121)



Project: TAP PROJECT NO. 111-126



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W&S File No.: ---

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**EXISTING  
CONDITIONS  
PLANS AND  
SURVEY TIES**

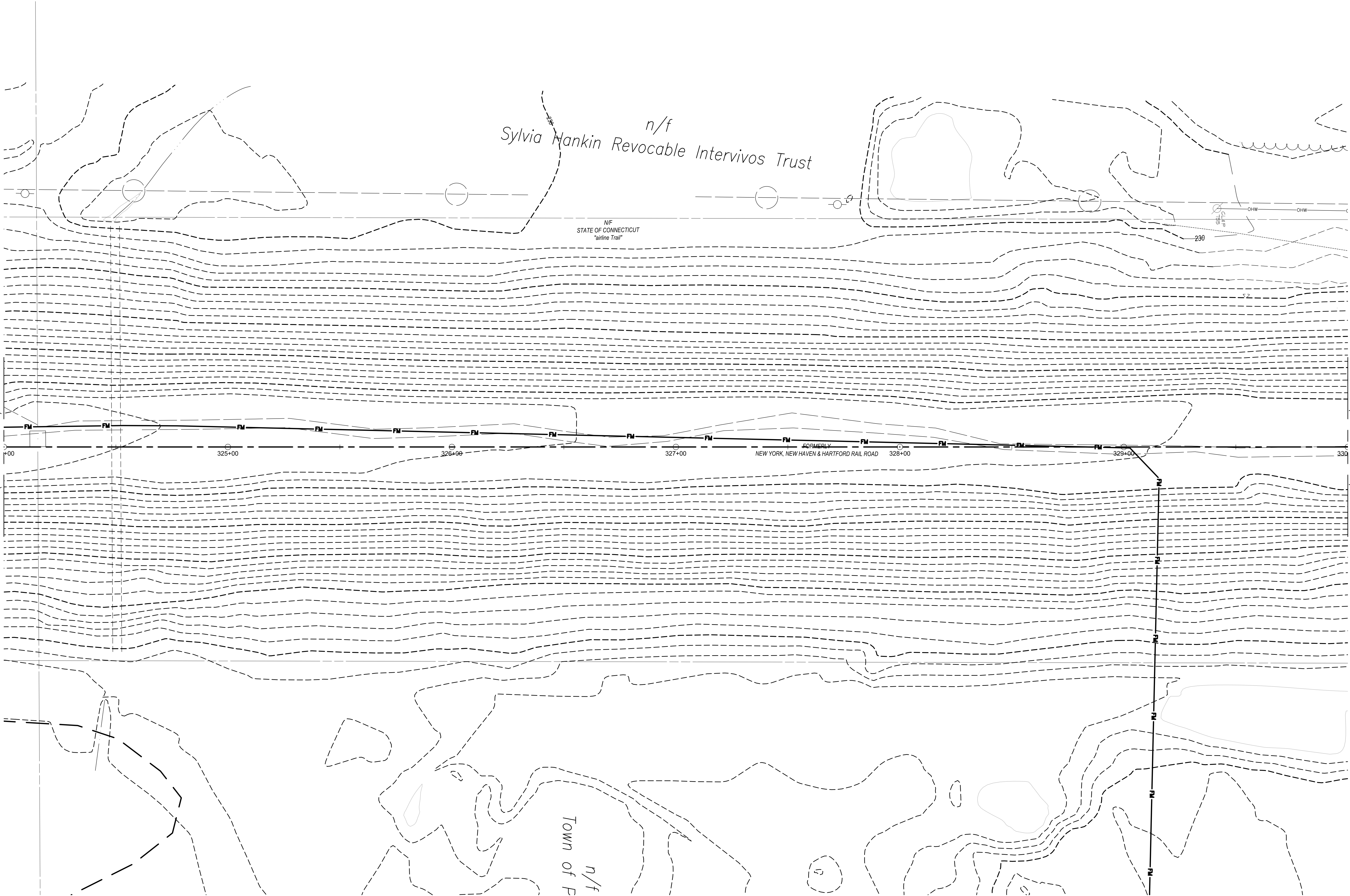
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MATCHLINE STA. 324+00  
(SEE DRAWING L120)



Project: TAP PROJECT NO. 111-126



AIR LINE TRAIL  
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**EXISTING  
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**L121**



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Reviewed By:	RC
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W&S File No.: ----

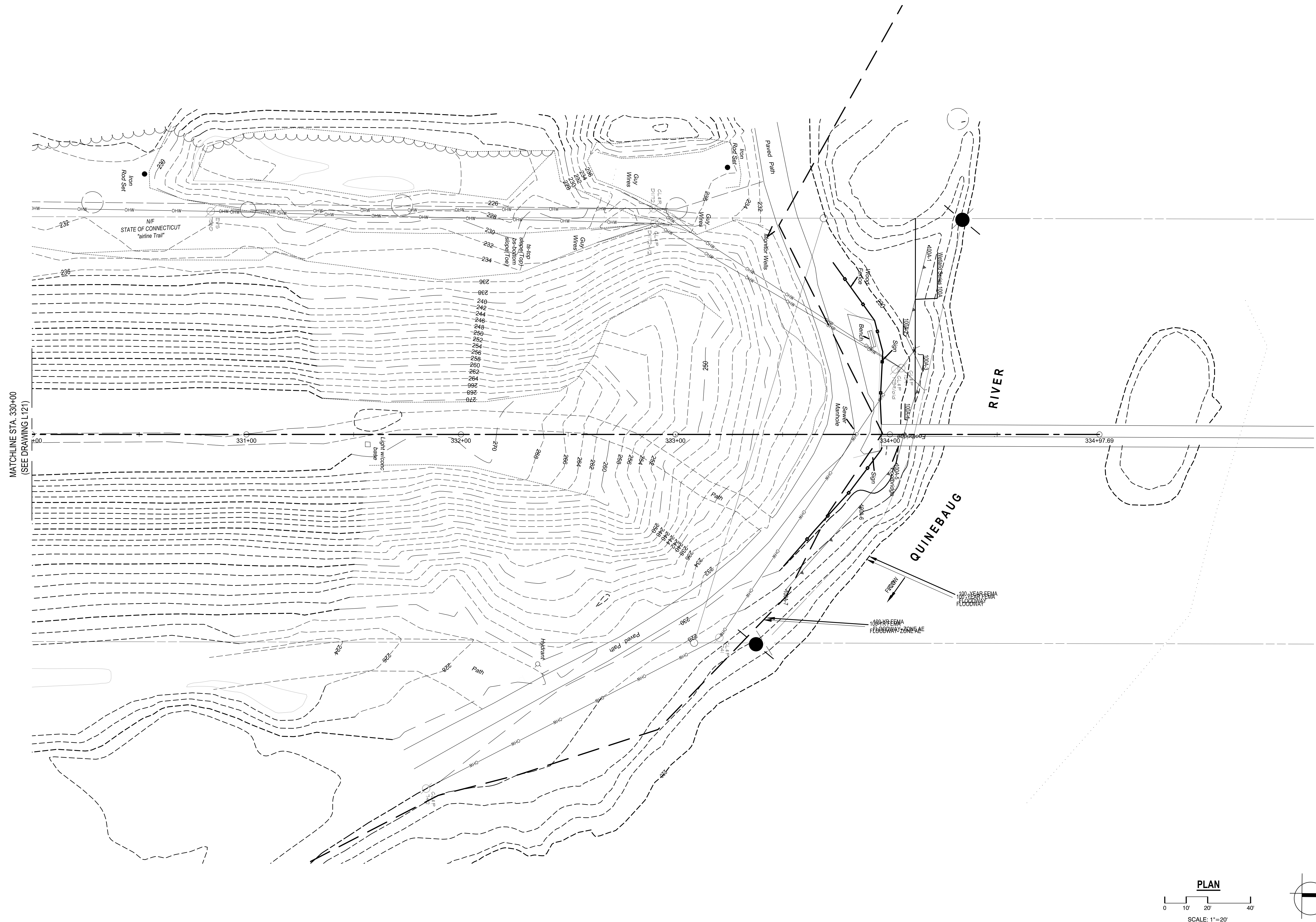
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CONDITIONS  
PLANS AND  
SURVEY TIES

Sheet Number:

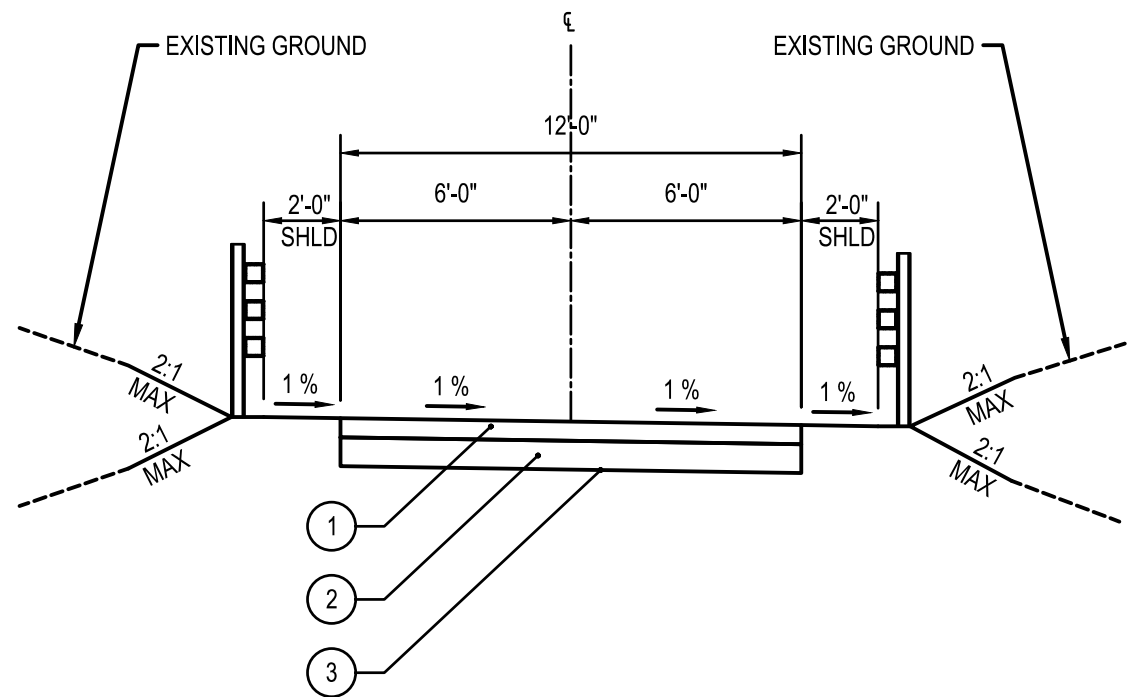
L122

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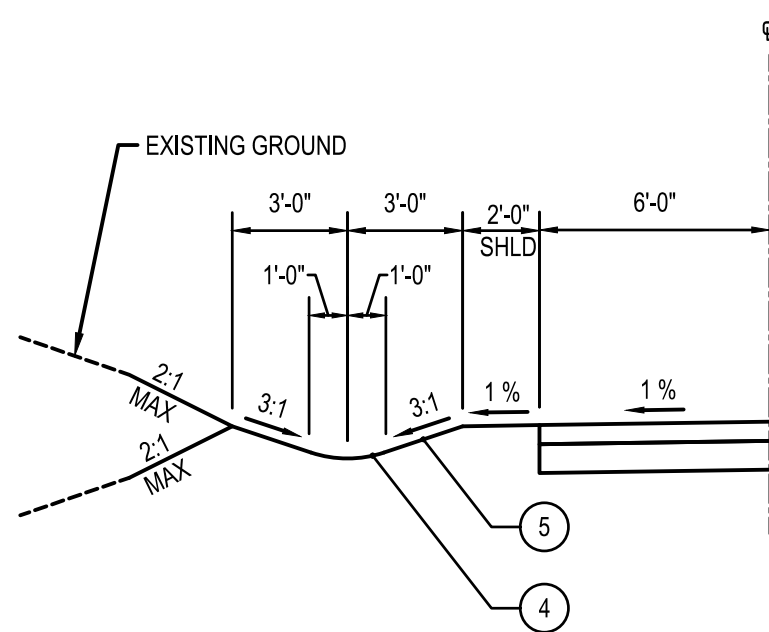




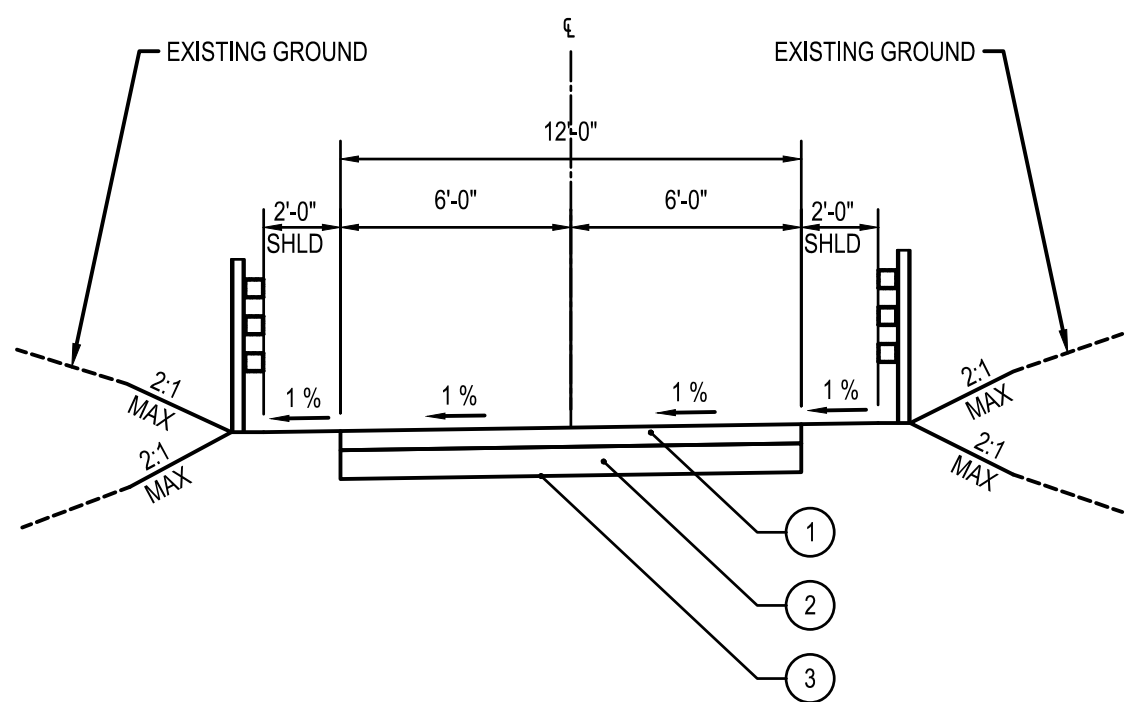
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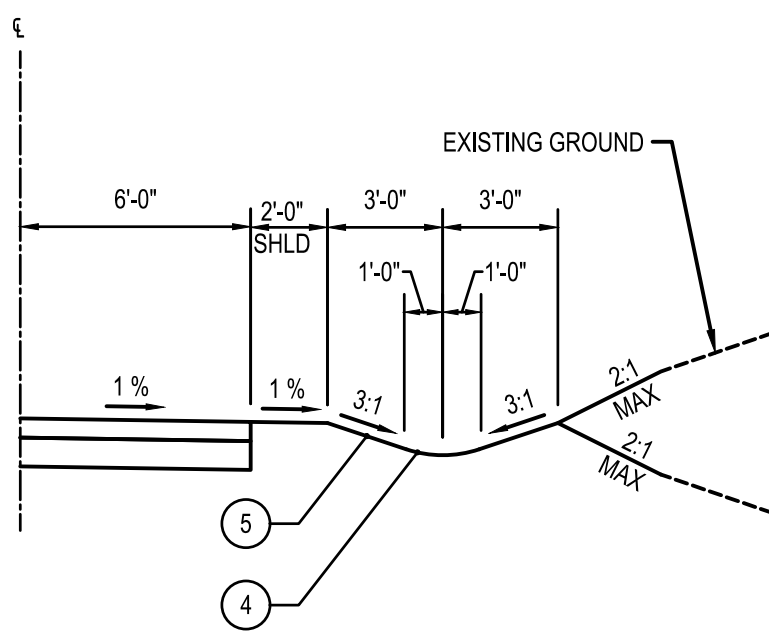
**TYPICAL TRAIL SECTION - BANK RIGHT**  
SCALE: 1"=5'-0"



**TYPICAL DRAINAGE SWALE - LEFT**  
SCALE: 1"=5'-0"



**TYPICAL TRAIL SECTION - BANK LEFT**  
SCALE: 1"=5'-0"



**TYPICAL DRAINAGE SWALE - RIGHT**  
SCALE: 1"=5'-0"

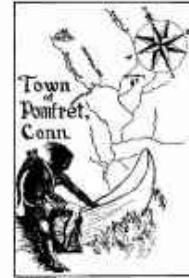
**LEGEND**

- 1 4" GRANULAR PAVEMENT
- 2 8" PROCESSED AGGREGATE BASE (SEE NOTE 3)
- 3 FORMATION OF SUBGRADE
- 4 4" TOPSOIL & TURF ESTABLISHMENT LAWN
- 5 EROSION CONTROL MATTING (SEE NOTE 4)

**NOTE:**

- 1. EROSION CONTROL MATTING TYPE D SHALL BE PLACED ON ALL SLOPES 3:1 OR STEEPER.
- 2. SEE MISCELLANEOUS DETAILS FOR UNDERDRAIN APPLICATION AND LOCATIONS.
- 3. IN AREAS OF POOR DRAINAGE OR UNSUITABLE SOIL CONDITIONS, 12" OF PROCESSED AGGREGATE BASE SHALL BE PROVIDED. FOR THE PURPOSES OF PRELIMINARY DESIGN, IT IS ASSUMED THIS WILL BE REQUIRED FOR 10% OF THE LENGTH OF THE IMPROVED TRAIL.
- 4. SEE TRAIL PLANS FOR EROSION CONTROL MATTING TYPE REQUIRED AT EACH DRAINAGE SWALE.

Project: TAP PROJECT NO. 111-126



AIR LINE TRAIL  
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Date: June 26, 2023  
Drawn By: GB  
Reviewed By: RC  
Approved By: MJJ

W&S Project No.: ENG22-1336  
W&S File No.: ---

Drawing Title:

**TRAIL TYPICAL  
SECTION**

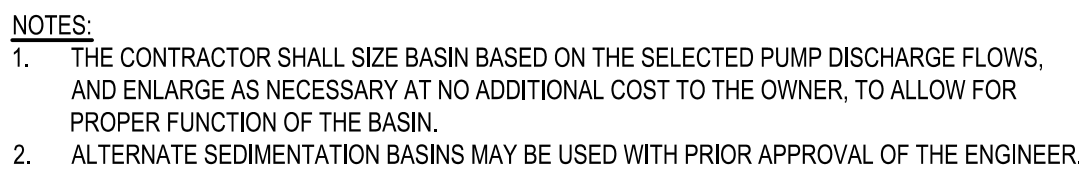
Sheet Number:

**L200**





N.T.S



N.T.S.



- NOTES:**
1. GEOTEXTILE BAG MATERIAL SHALL BE A NON-WOVEN MATERIAL.
  2. DO NOT OVER PRESSURIZE BAG OR USE BEYOND CAPACITY.
  3. LOCATE DISCHARGE SITE ON FLAT UPLAND AREAS AS FAR AWAY AS POSSIBLE FROM STREAMS, WETLANDS, AND OTHER RESOURCES AND POINTS OF CONCENTRATED FLOW.
  4. DOWNGRADIENT FROM RECEIVING AREA MUST BE WELL VEGETATED OR OTHERWISE STABLE FROM EROSION, E.G., FOREST FLOOR OR COARSE GRAVEL/STONE.



- NOTES:**
1. GEOTEXTILE BAG MATERIAL SHALL BE A NON-WOVEN MATERIAL.
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  4. DOWNGRADIENT FROM RECEIVING AREA MUST BE WELL VEGETATED OR OTHERWISE STABLE FROM EROSION, E.G., FOREST FLOOR OR COARSE GRAVEL/STONE.

GRASSED SWALES - ALL GRASSED SWALES SHALL BE STABILIZED WITH SOIL EROSION CONTROL MATS IMMEDIATELY AFTER GRADING. THE SWALES SHALL BE IMMEDIATELY SODDED OR SEEDED DEPENDING ON THE SEASON.



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Project: TAP PROJECT NO. 111-126



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Approved By: LAS

W&S Project No.: ENG22-1336  
W&S File No.: ----

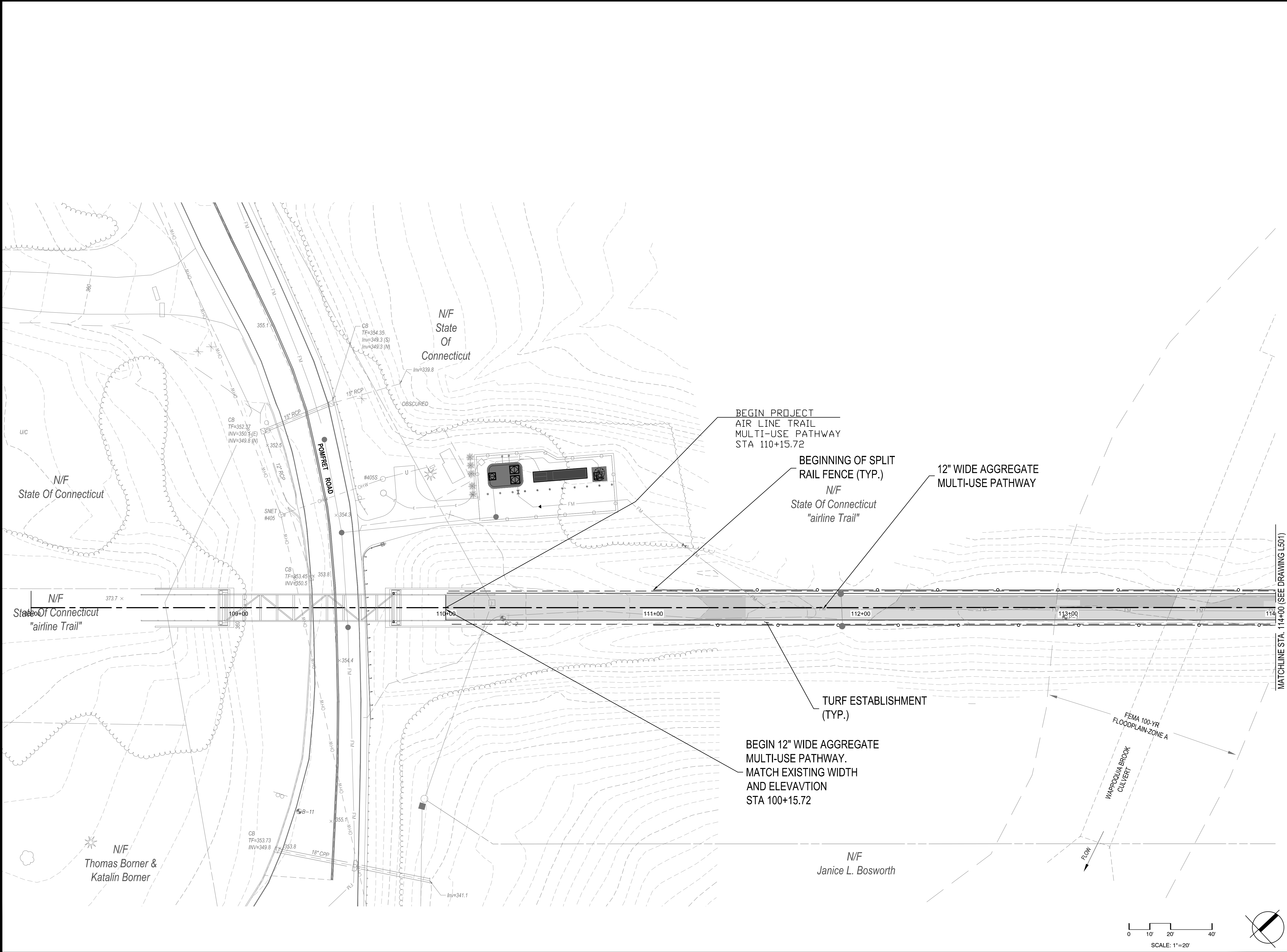
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MISCELLANEOUS  
DETAILS

Sheet Number:


L400





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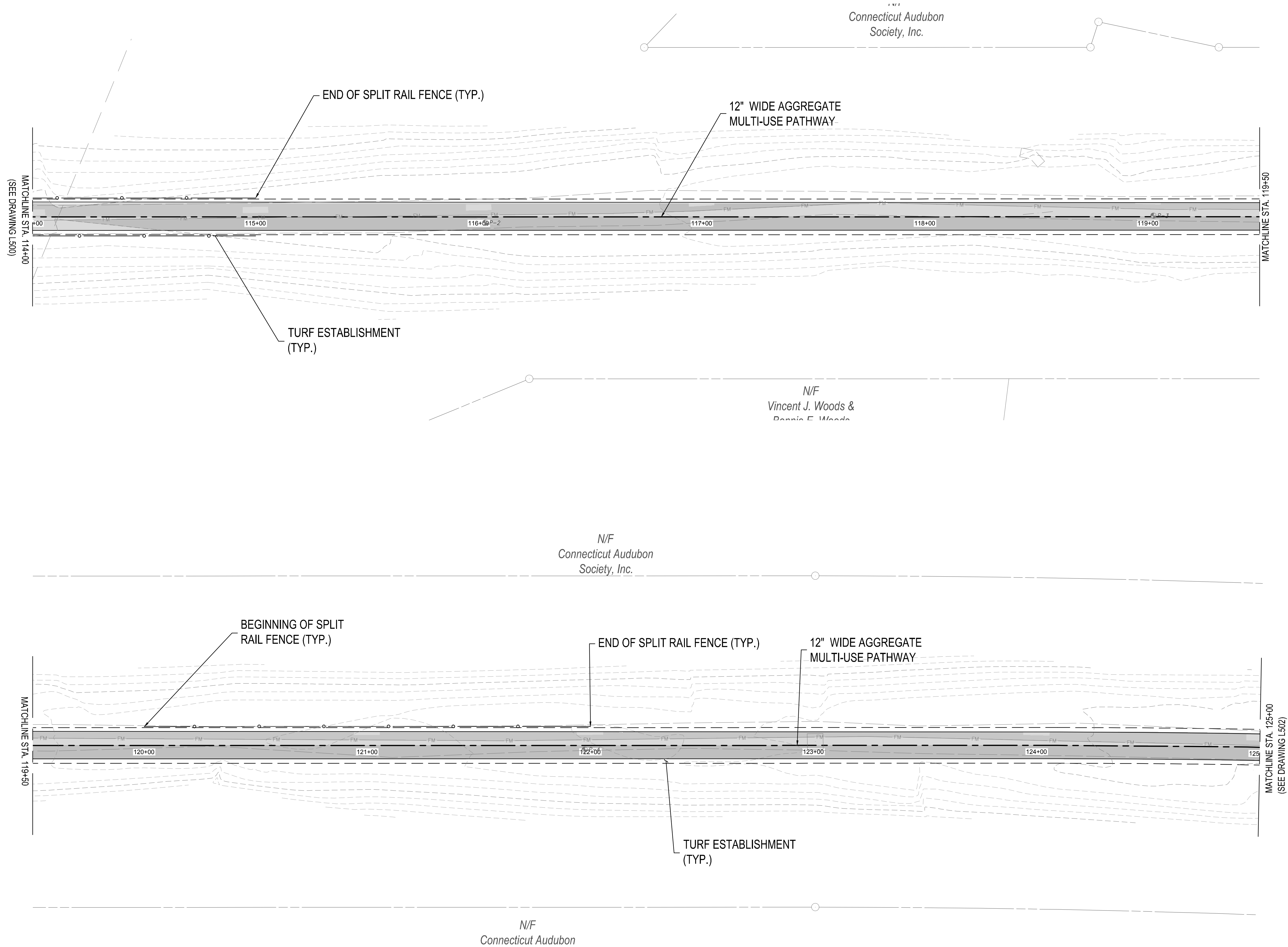
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Drawn By: AMN  
Reviewed By: RC  
Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ----

Drawing Title:  
**PLAN SHEETS**

Sheet Number:  
**L500**



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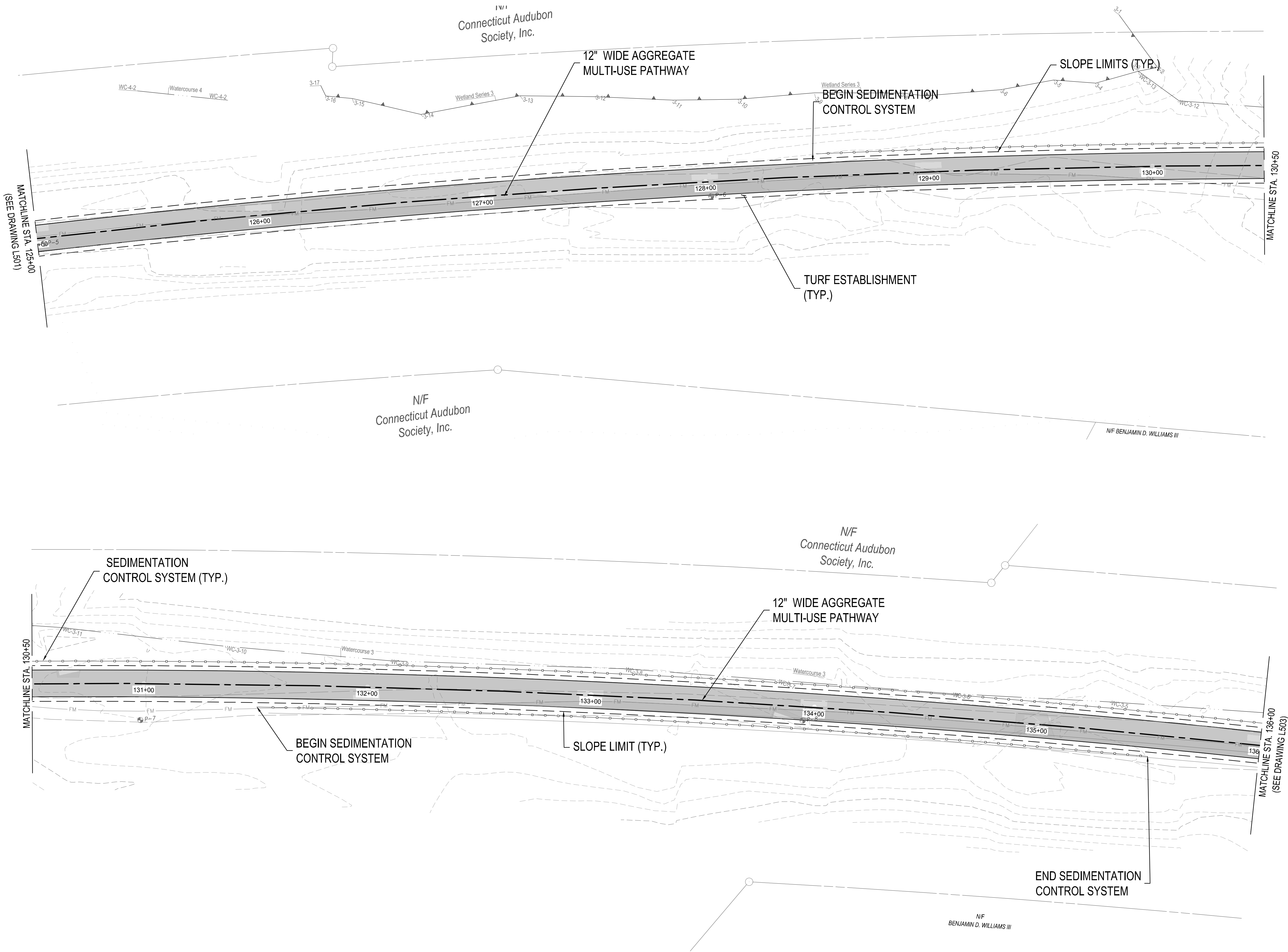
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Reviewed By: RC  
Approved By: MJJ  
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Sheet Number:  
**L501**



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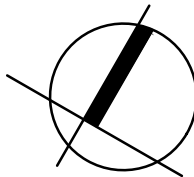
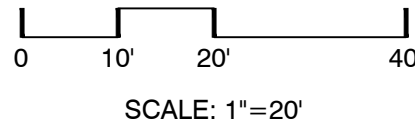
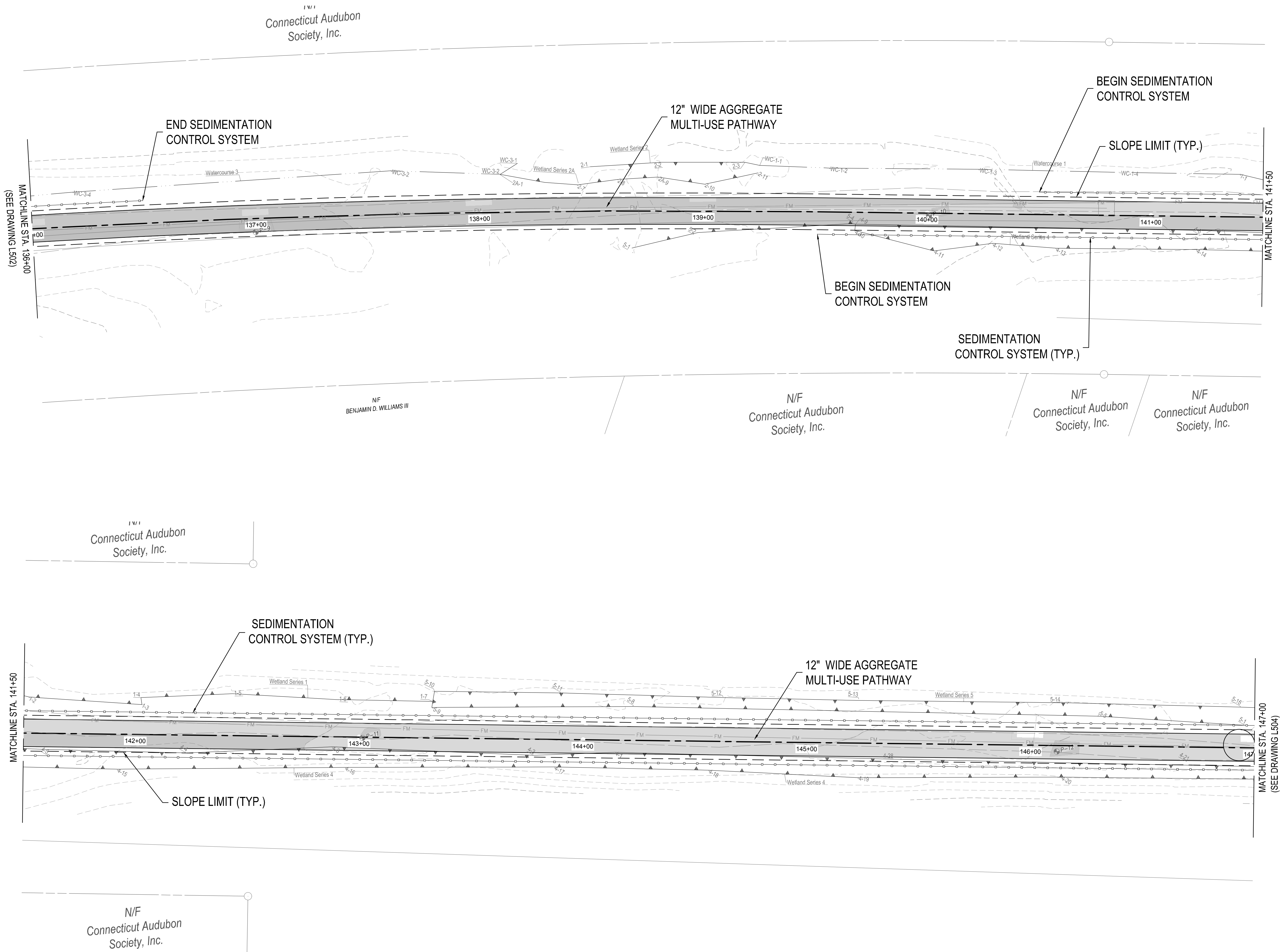
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L502

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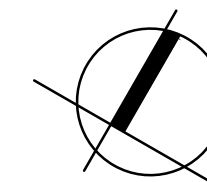
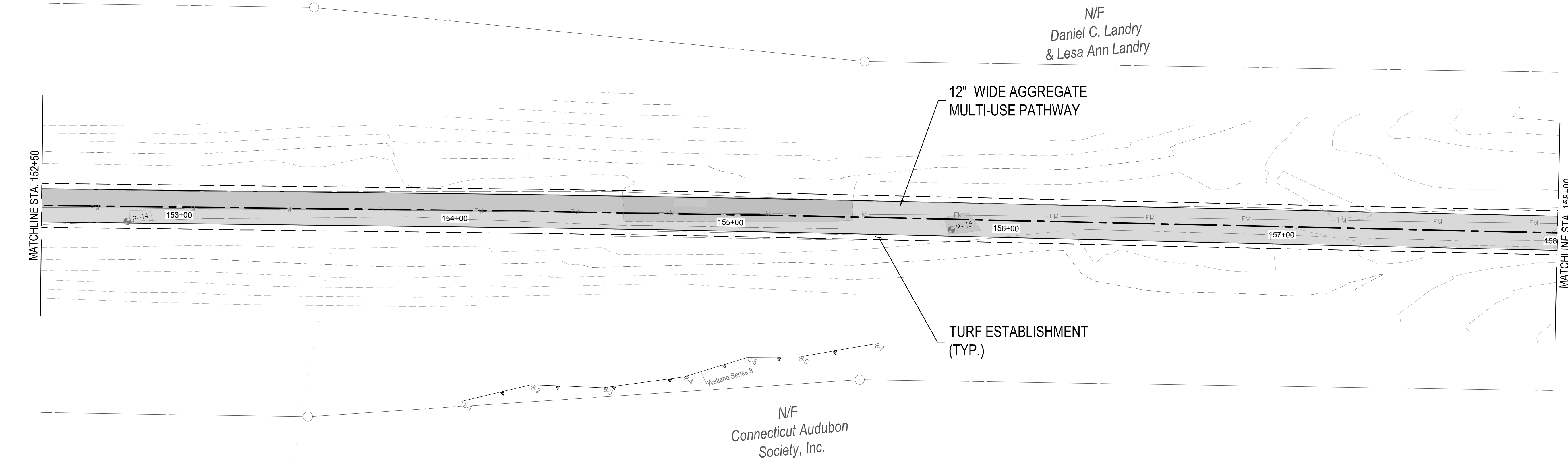
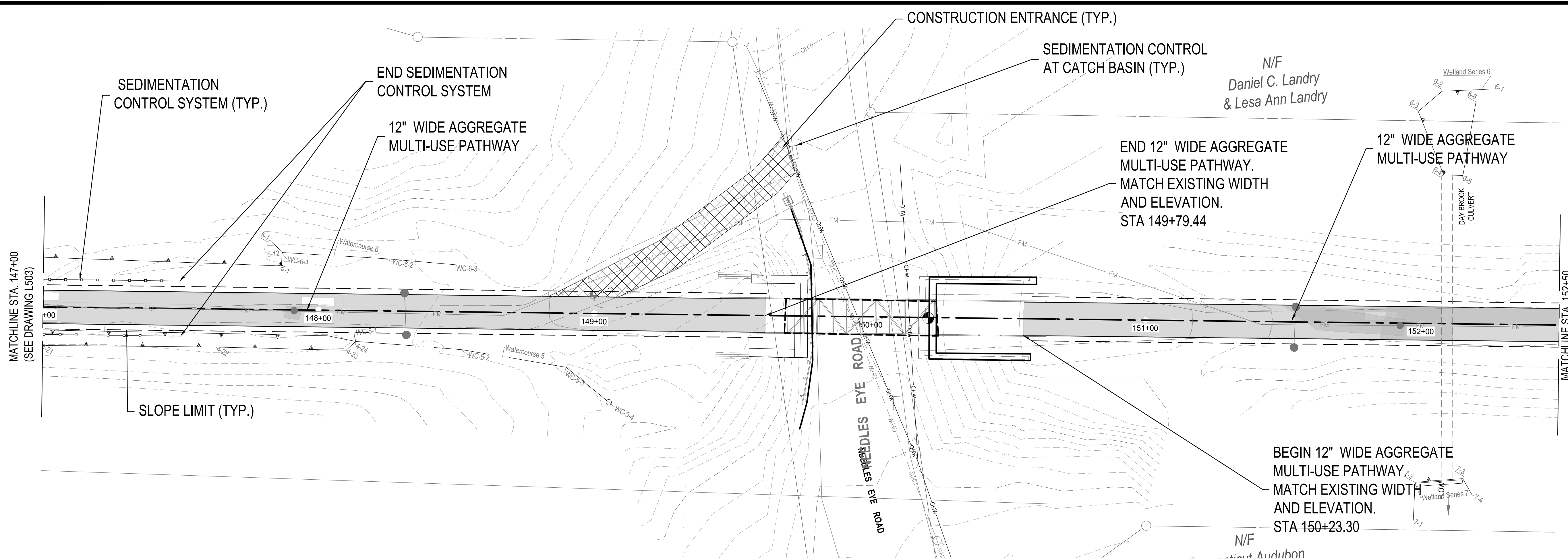
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**L503**



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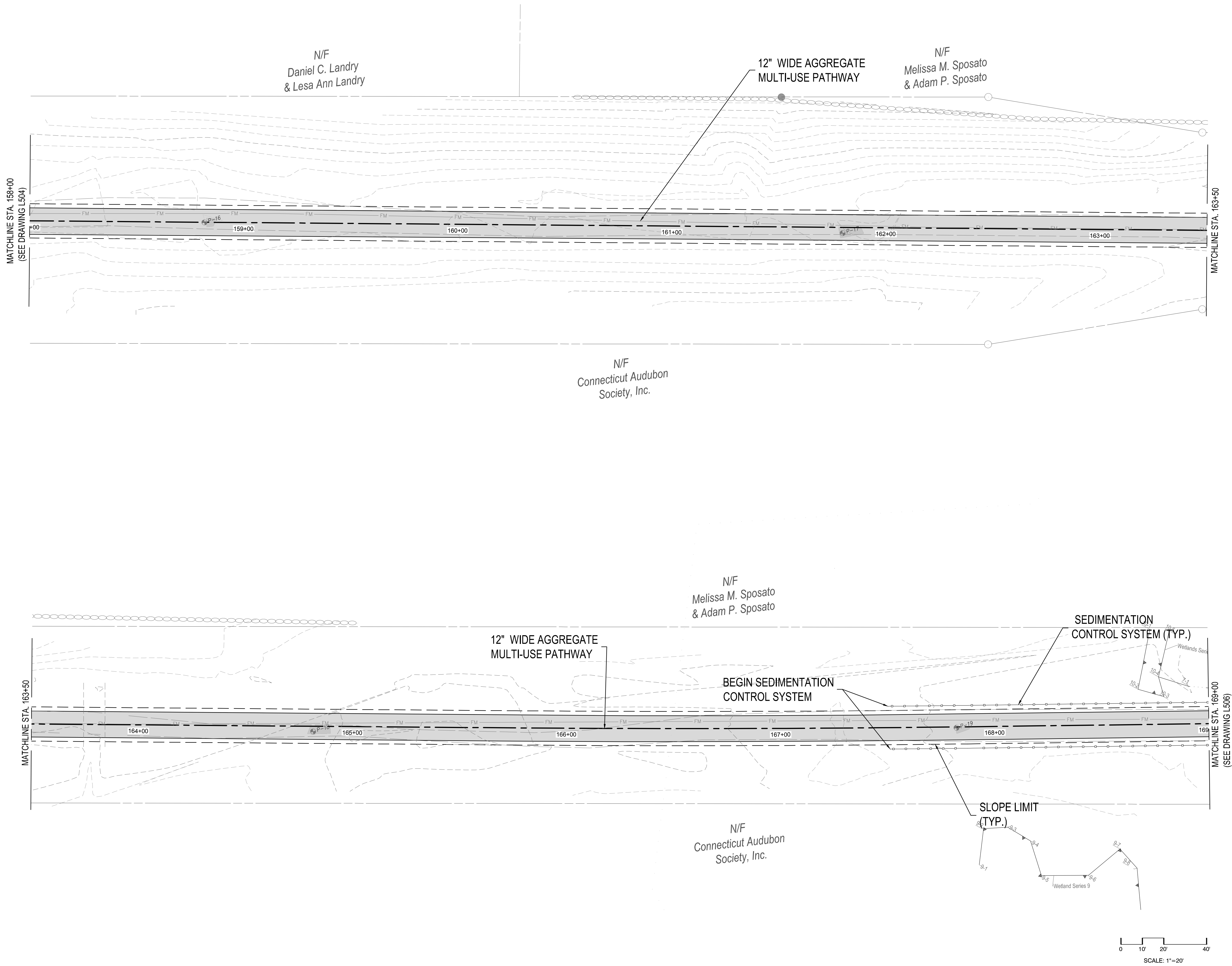
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Approved By: MJJ  
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**PLAN SHEETS**


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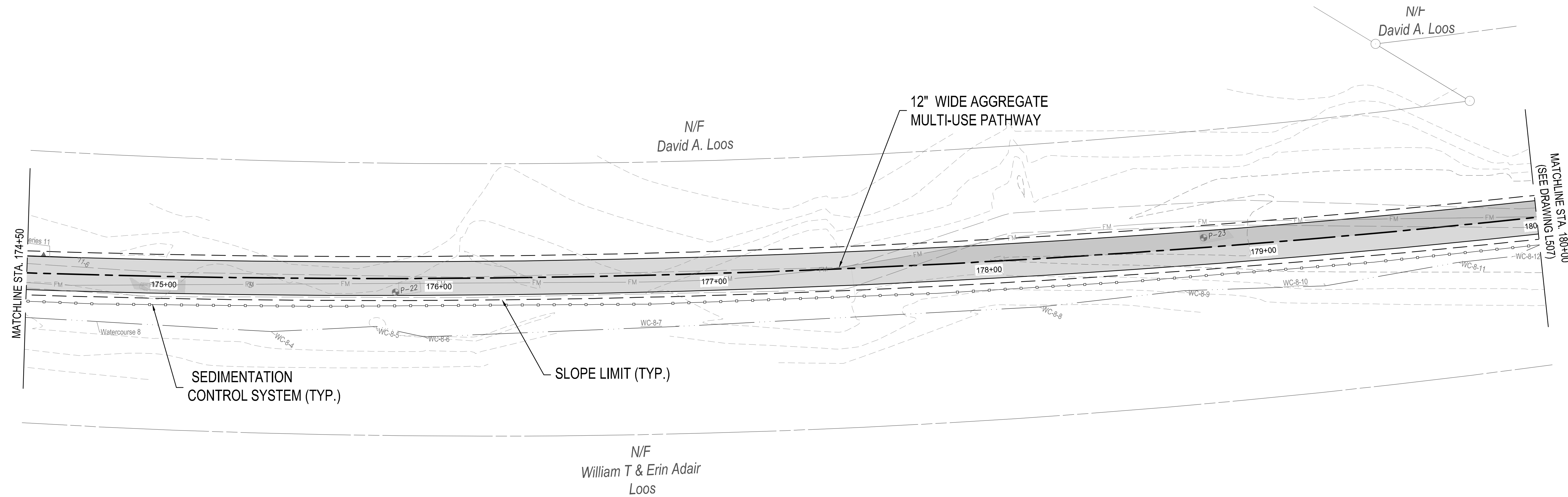
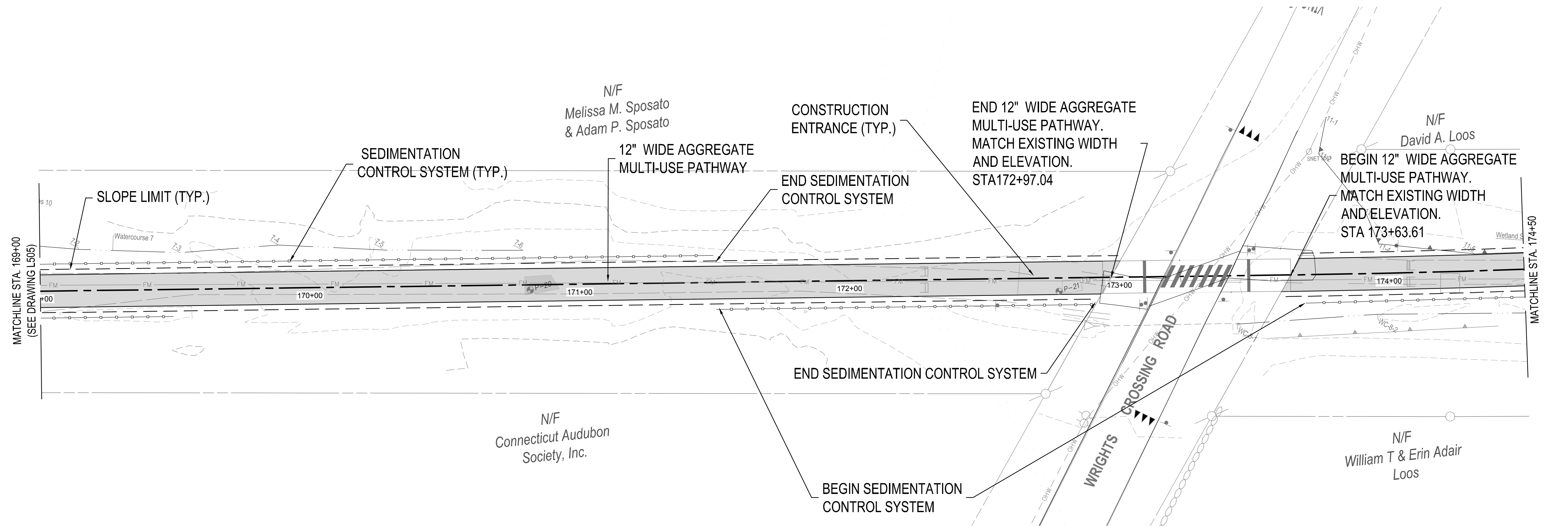
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Approved By: MJJ  
W&S Project No.: ENC22-1336  
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**PLAN SHEETS**


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**L505**



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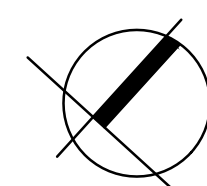
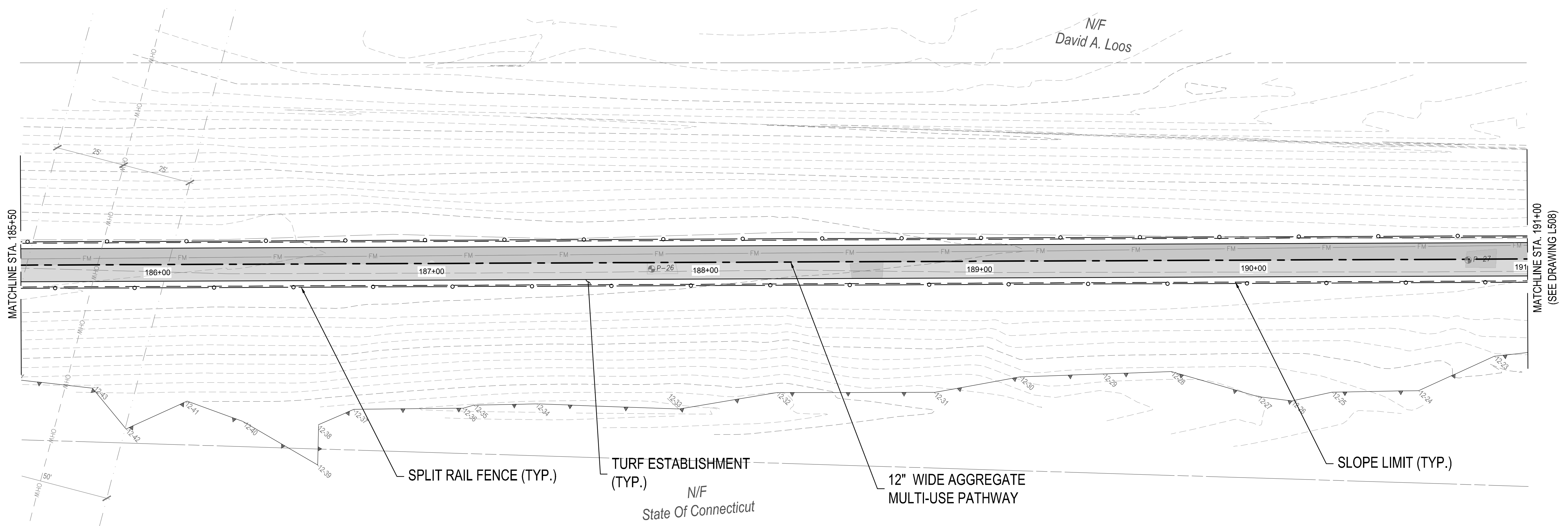
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Drawn By: AMN  
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**L506**



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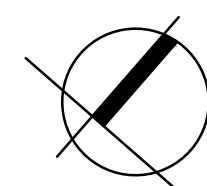
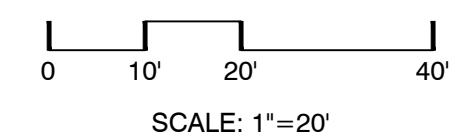
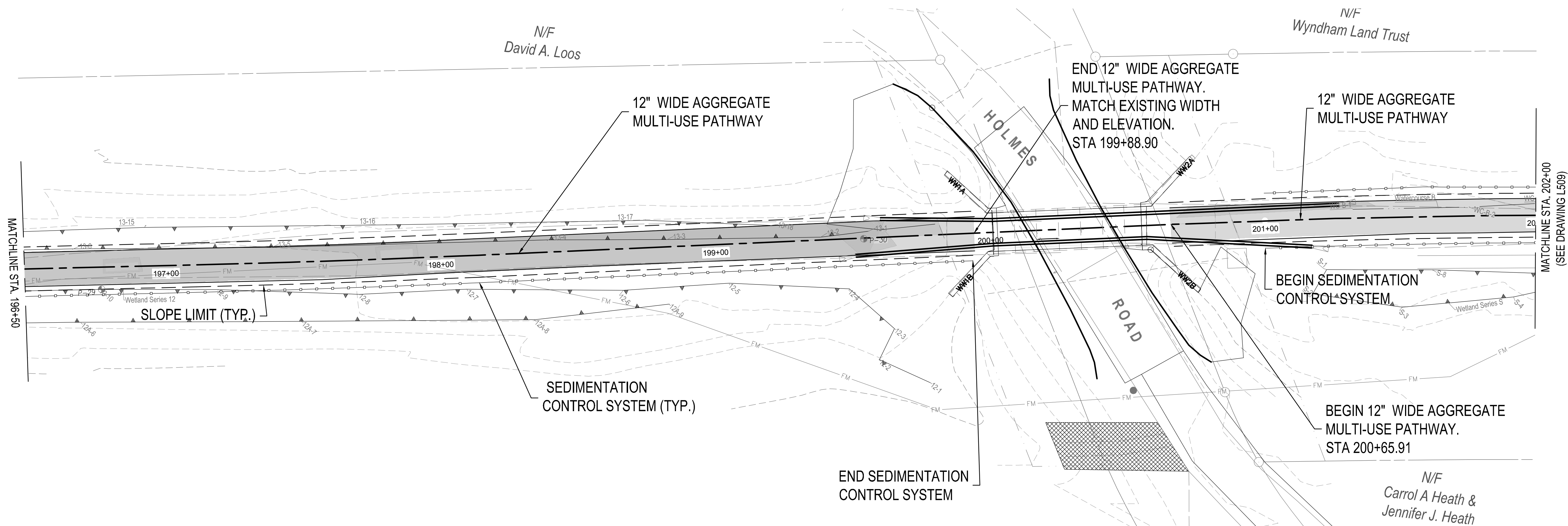
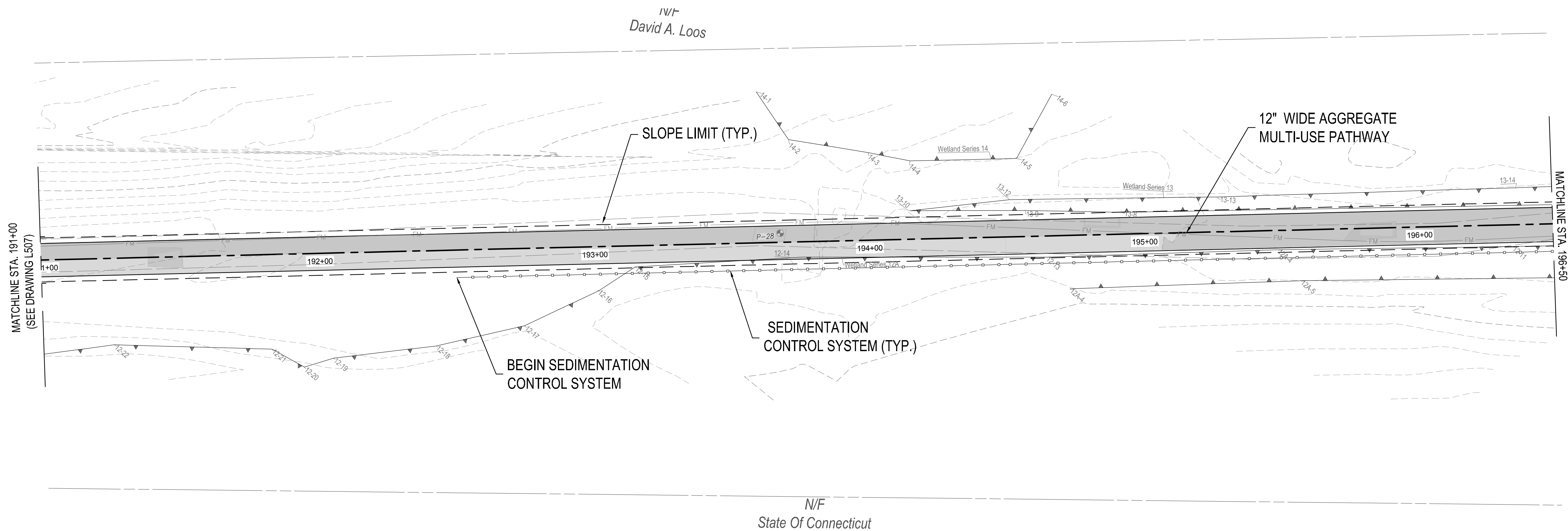
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Drawn By: AMN  
Reviewed By: RC  
Approved By: MJJ  
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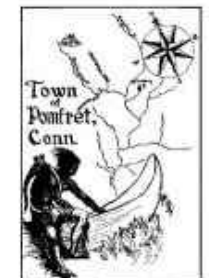
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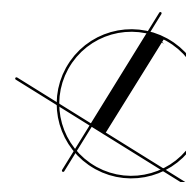
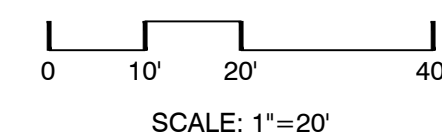
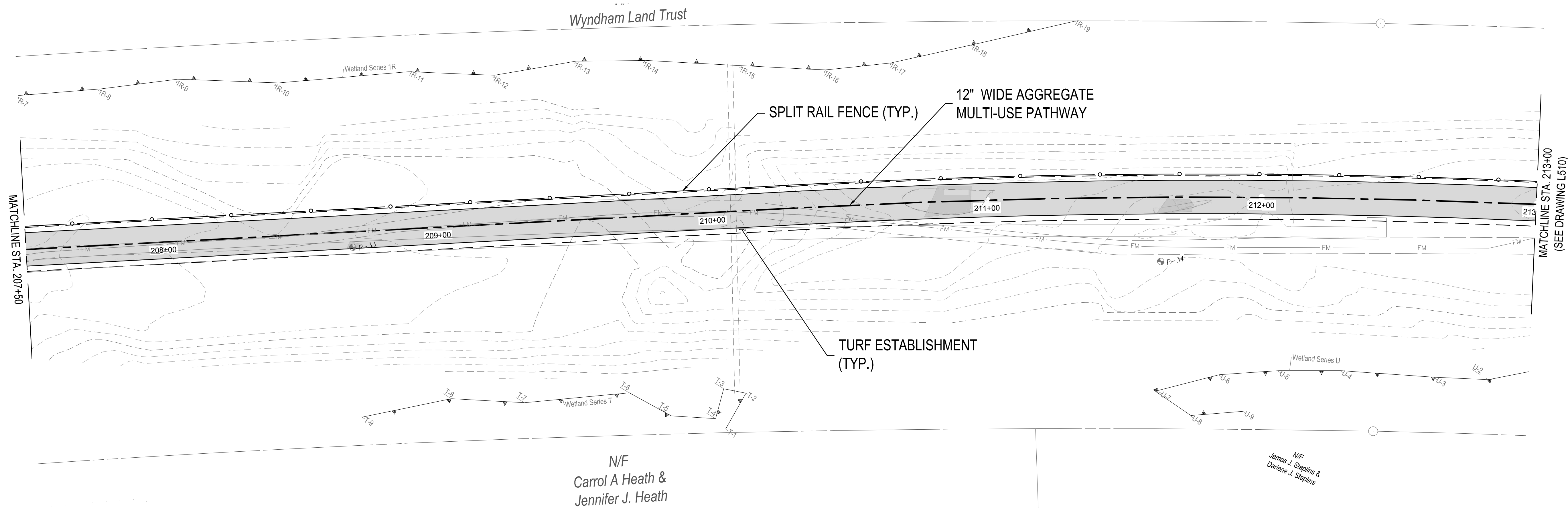
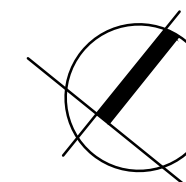
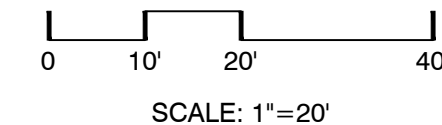
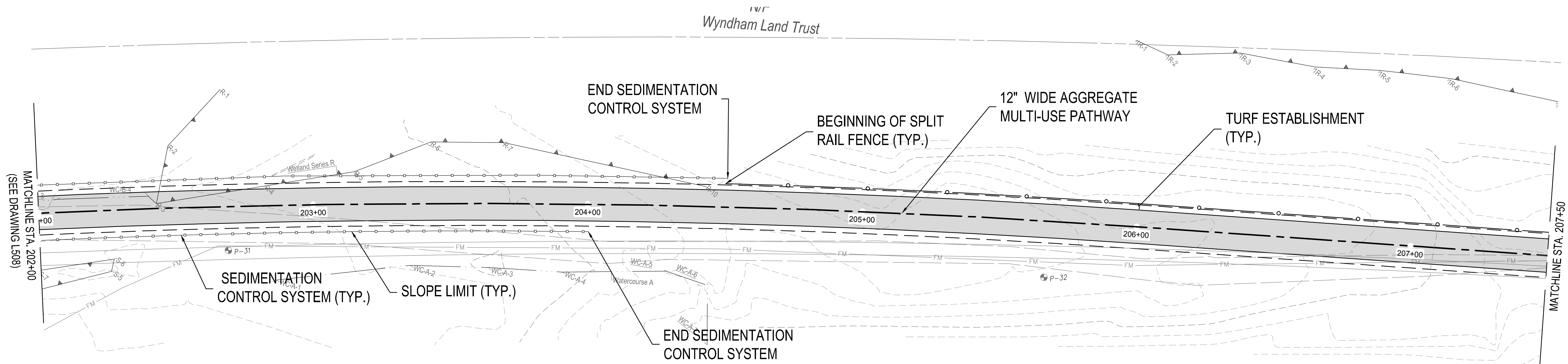
Date: August 28, 2023  
Drawn By: AMN  
Reviewed By: RC  
Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ----

Drawing Title:  
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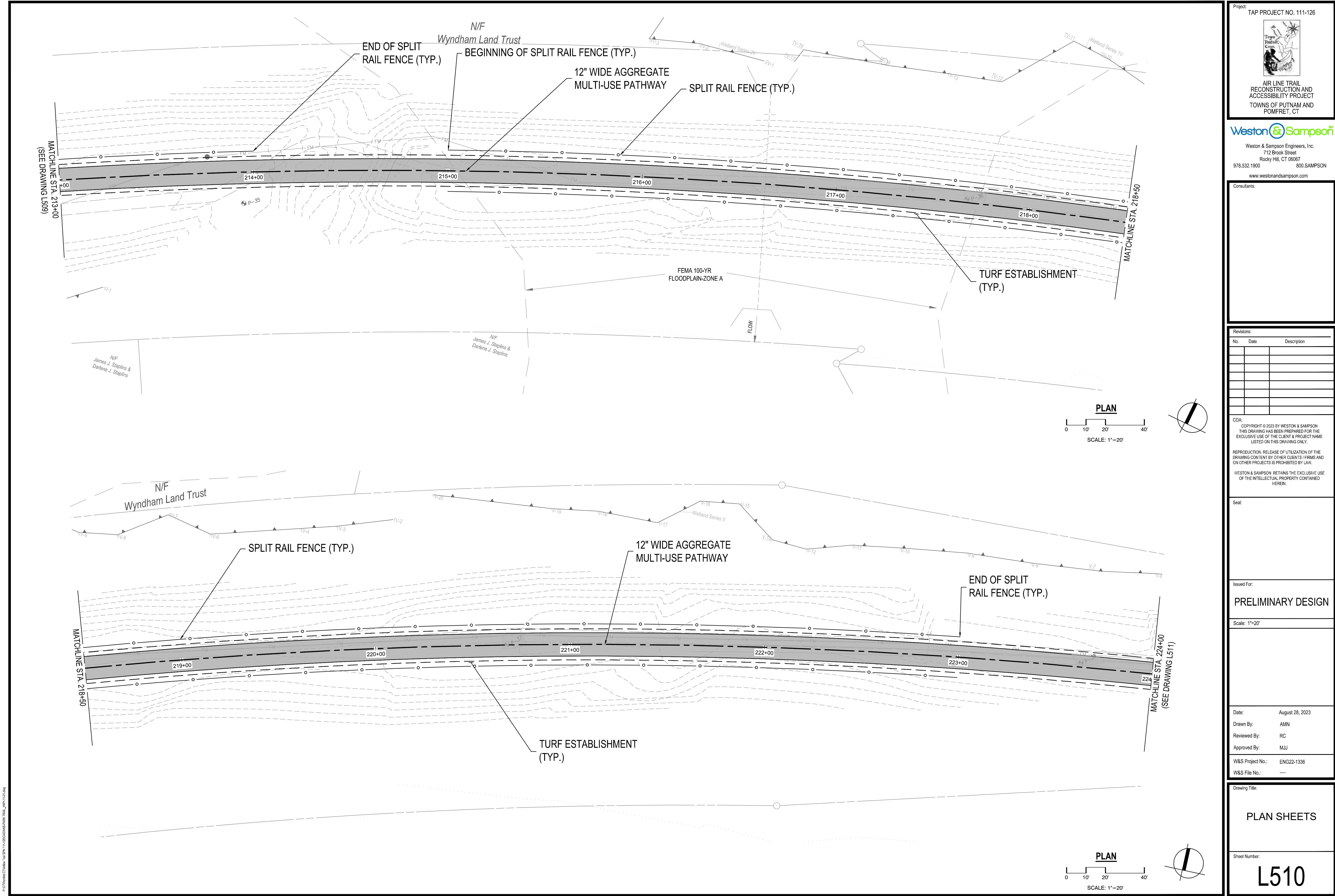
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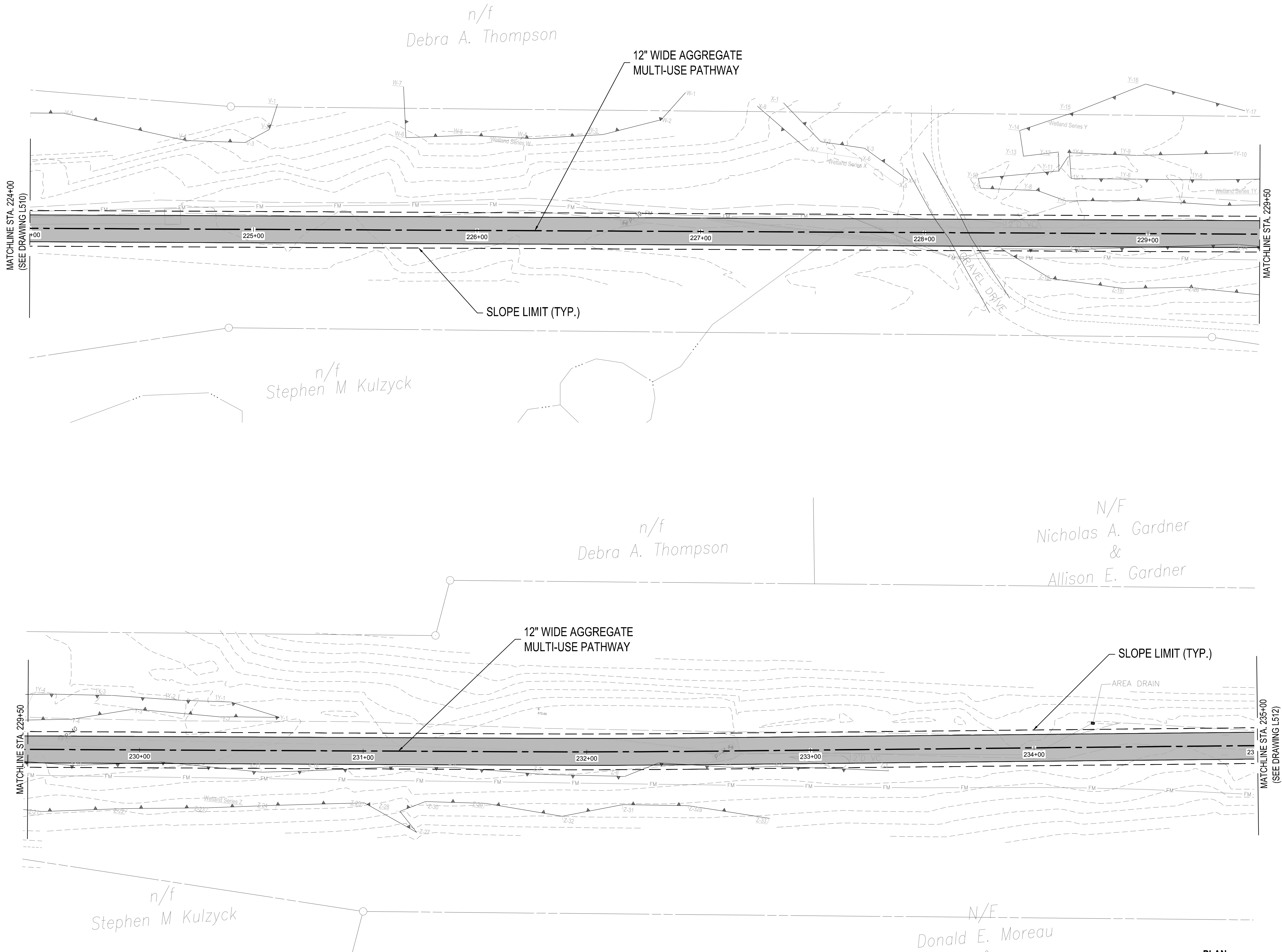
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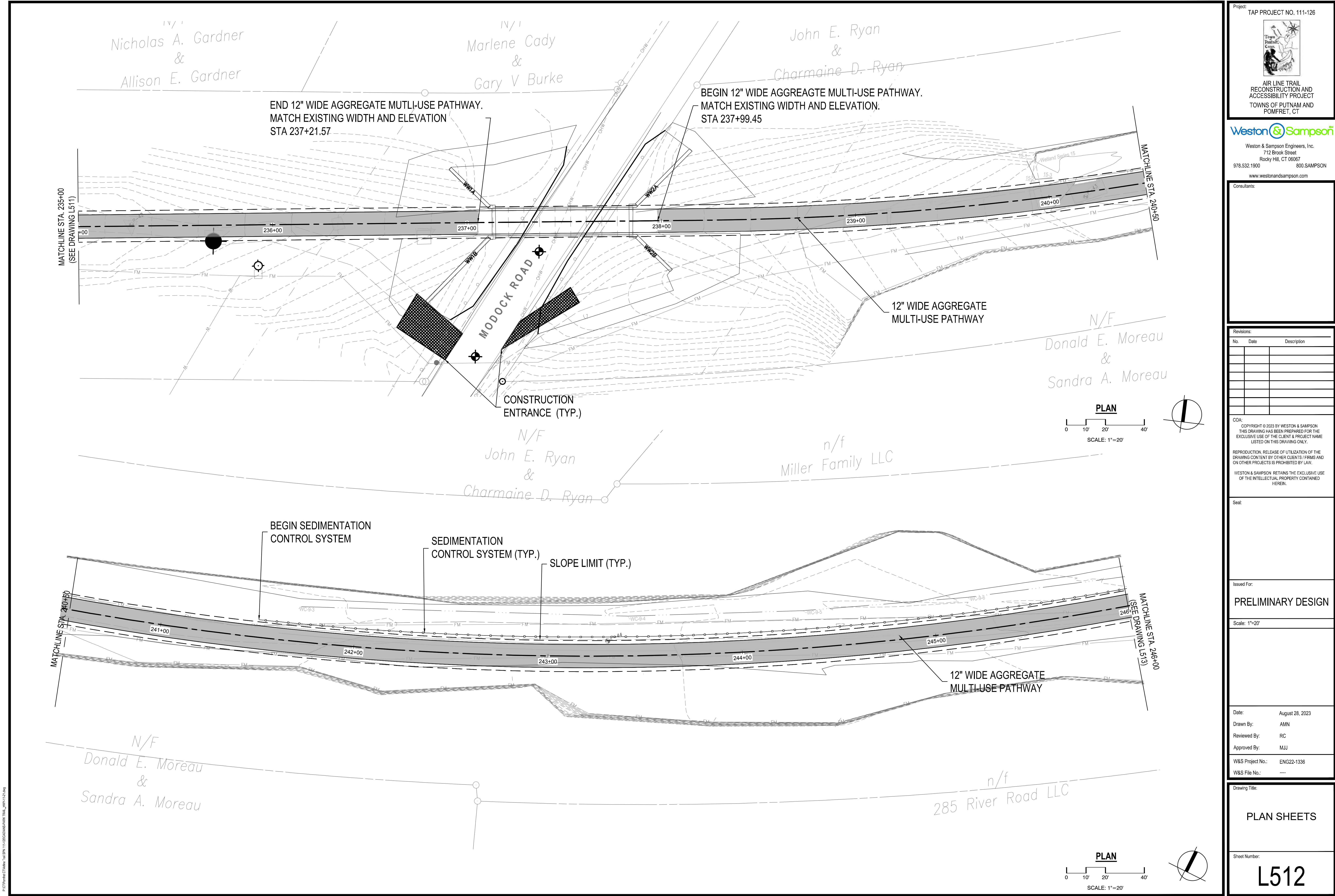
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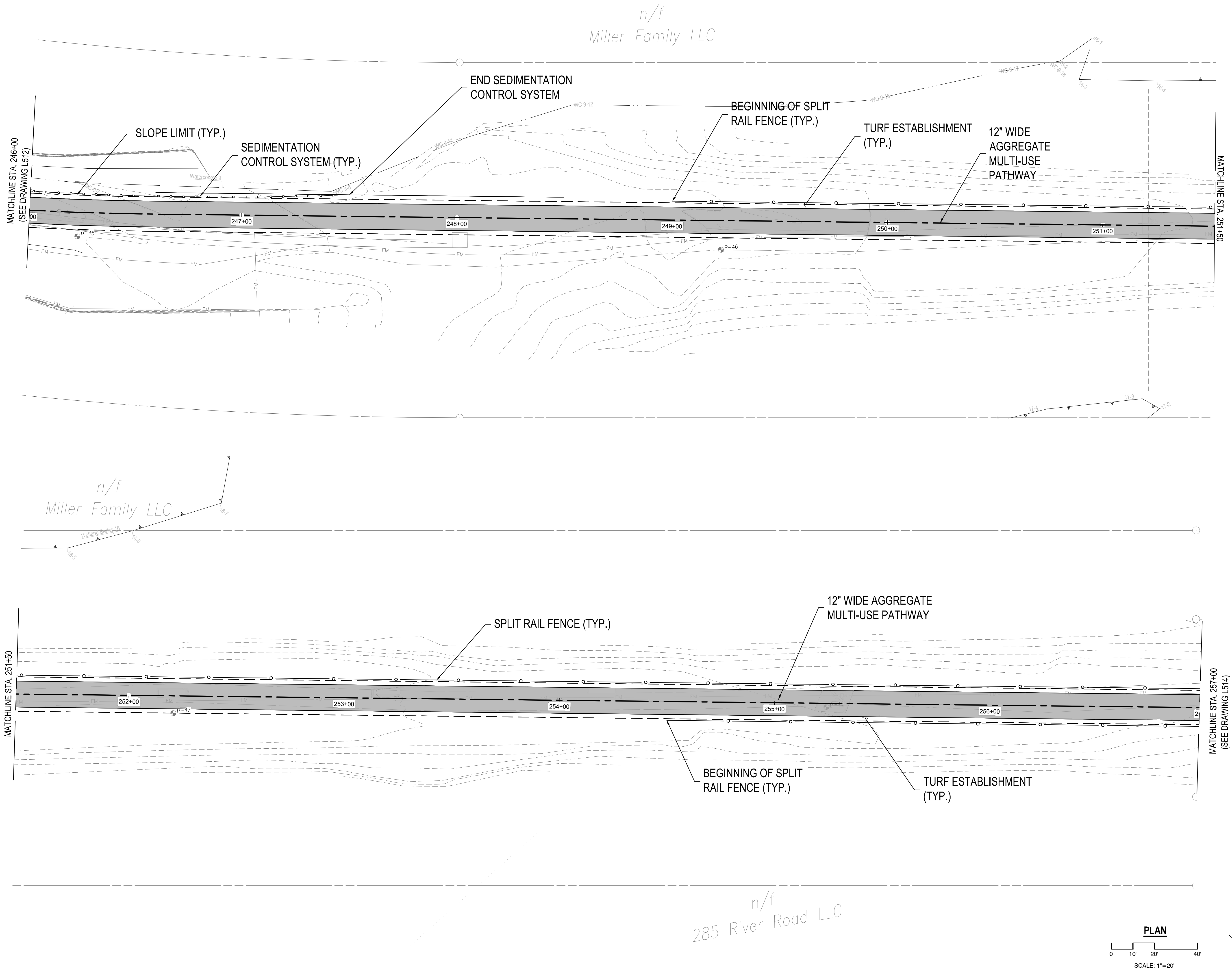
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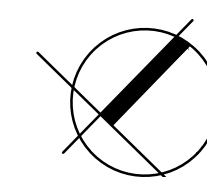
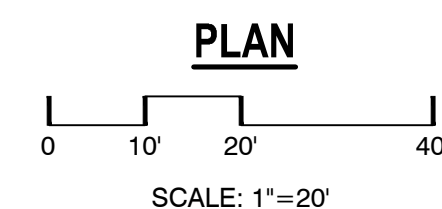
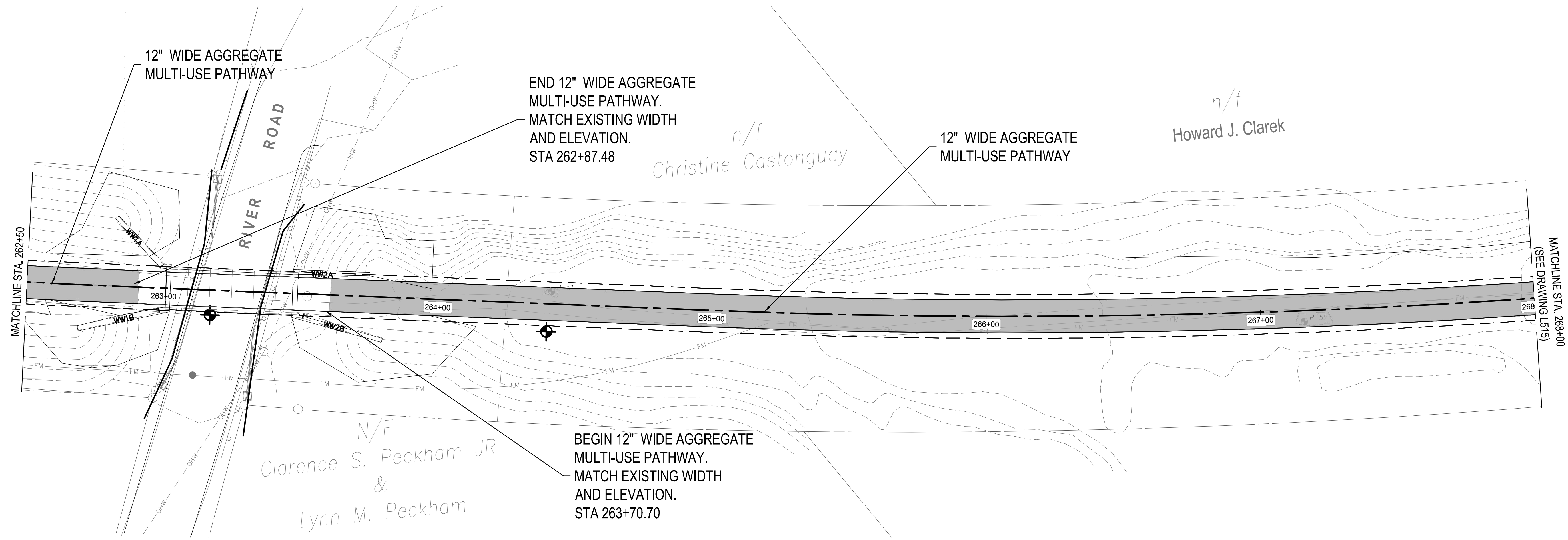
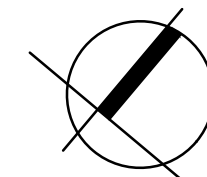
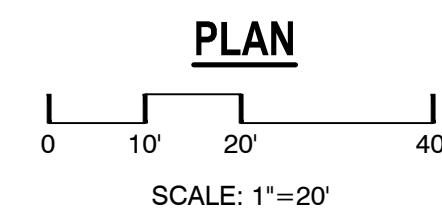
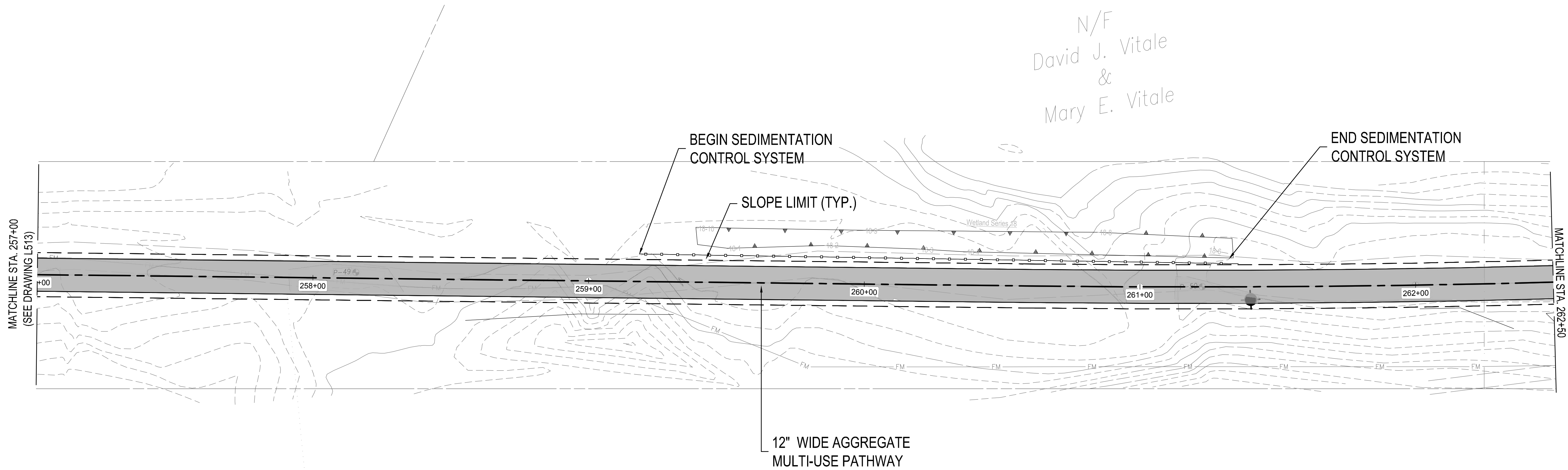
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Approved By: MJJ  
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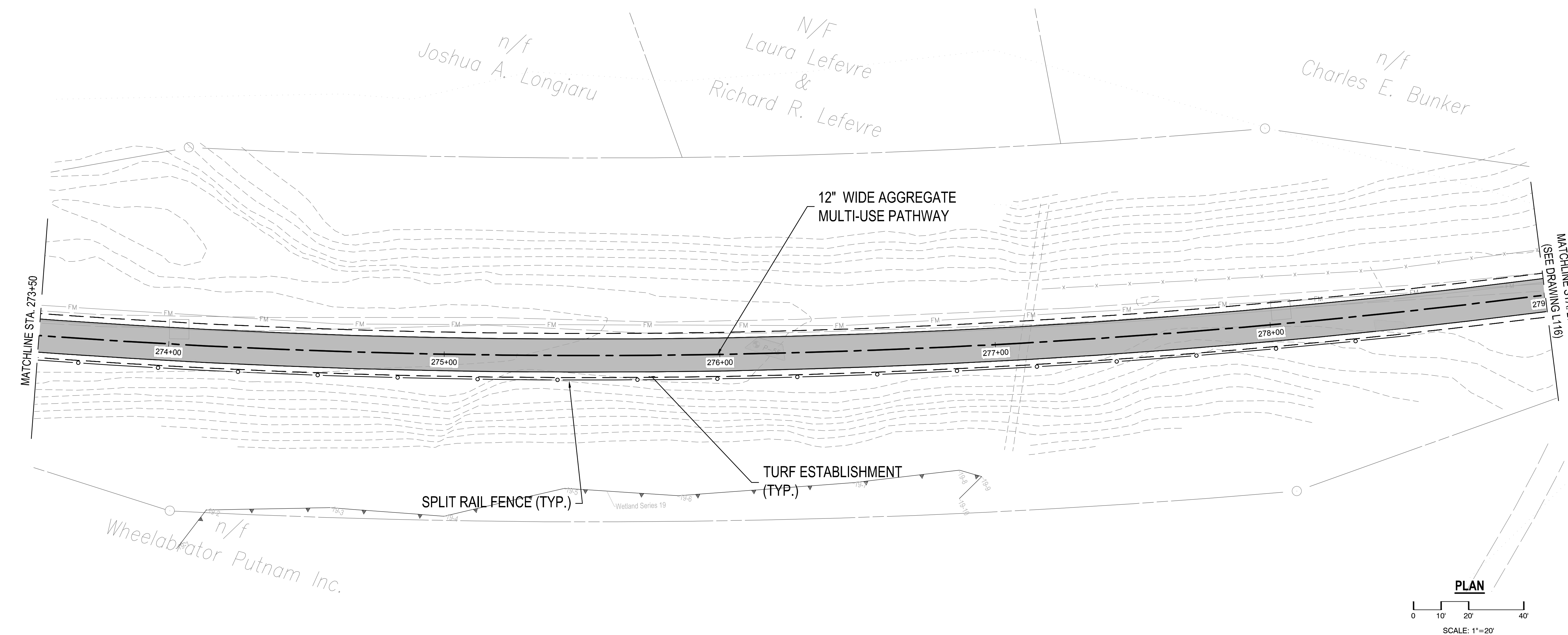
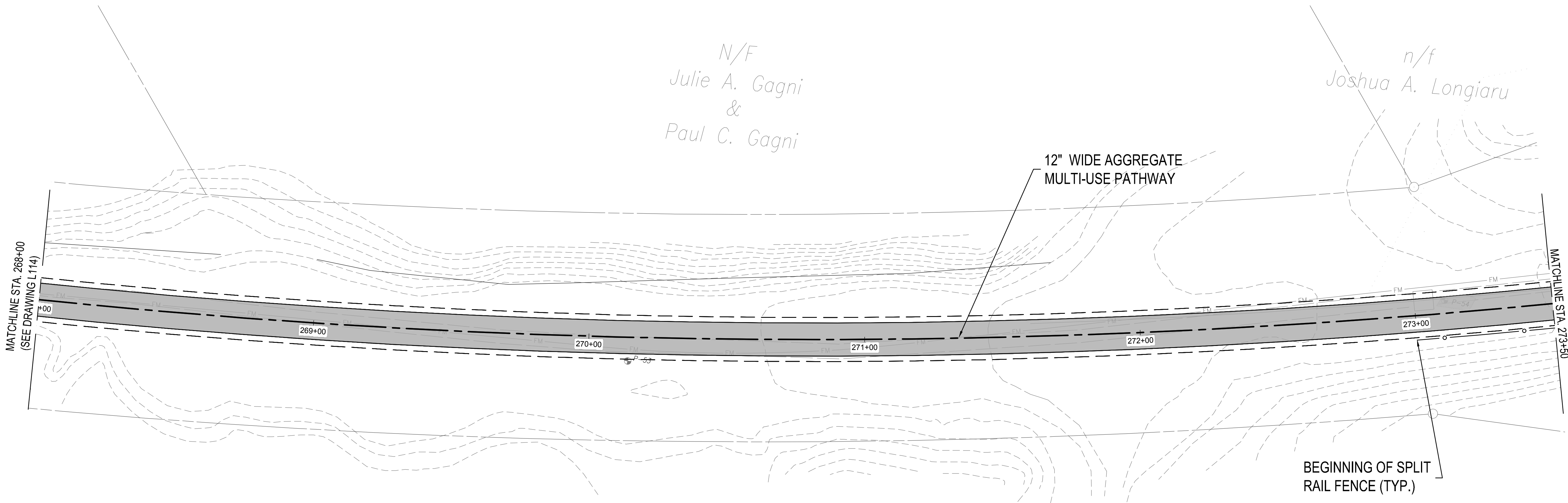
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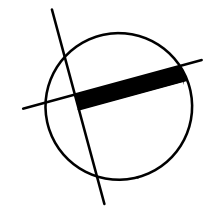
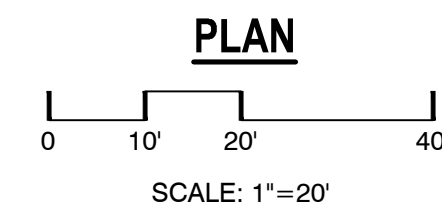
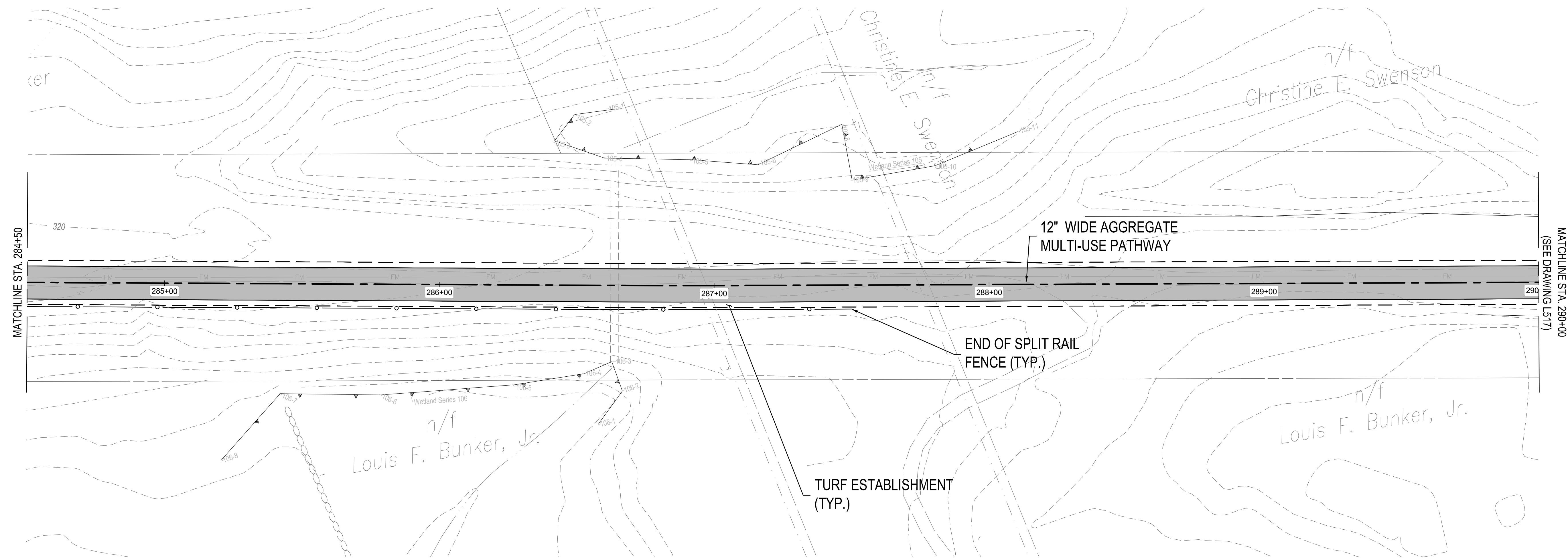
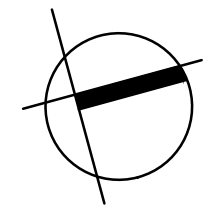
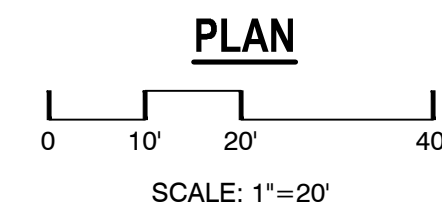
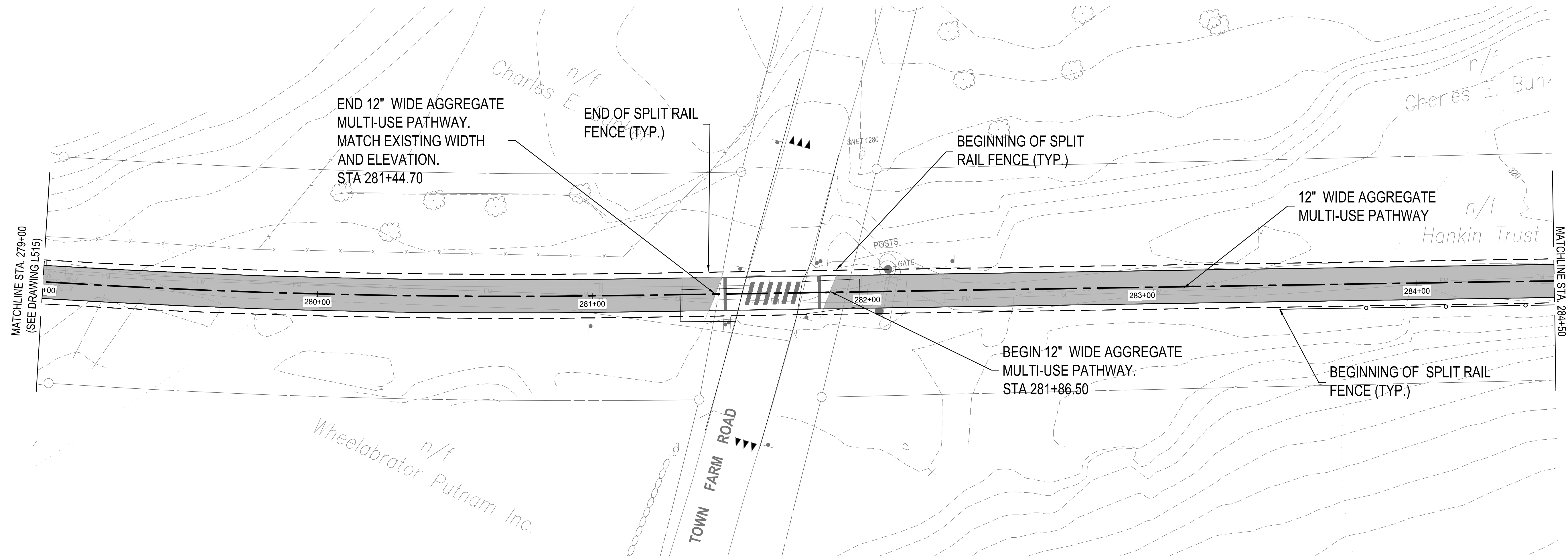
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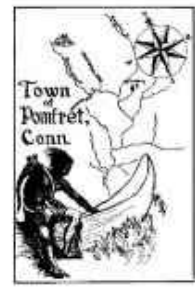
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Project: TAP PROJECT NO. 111-126



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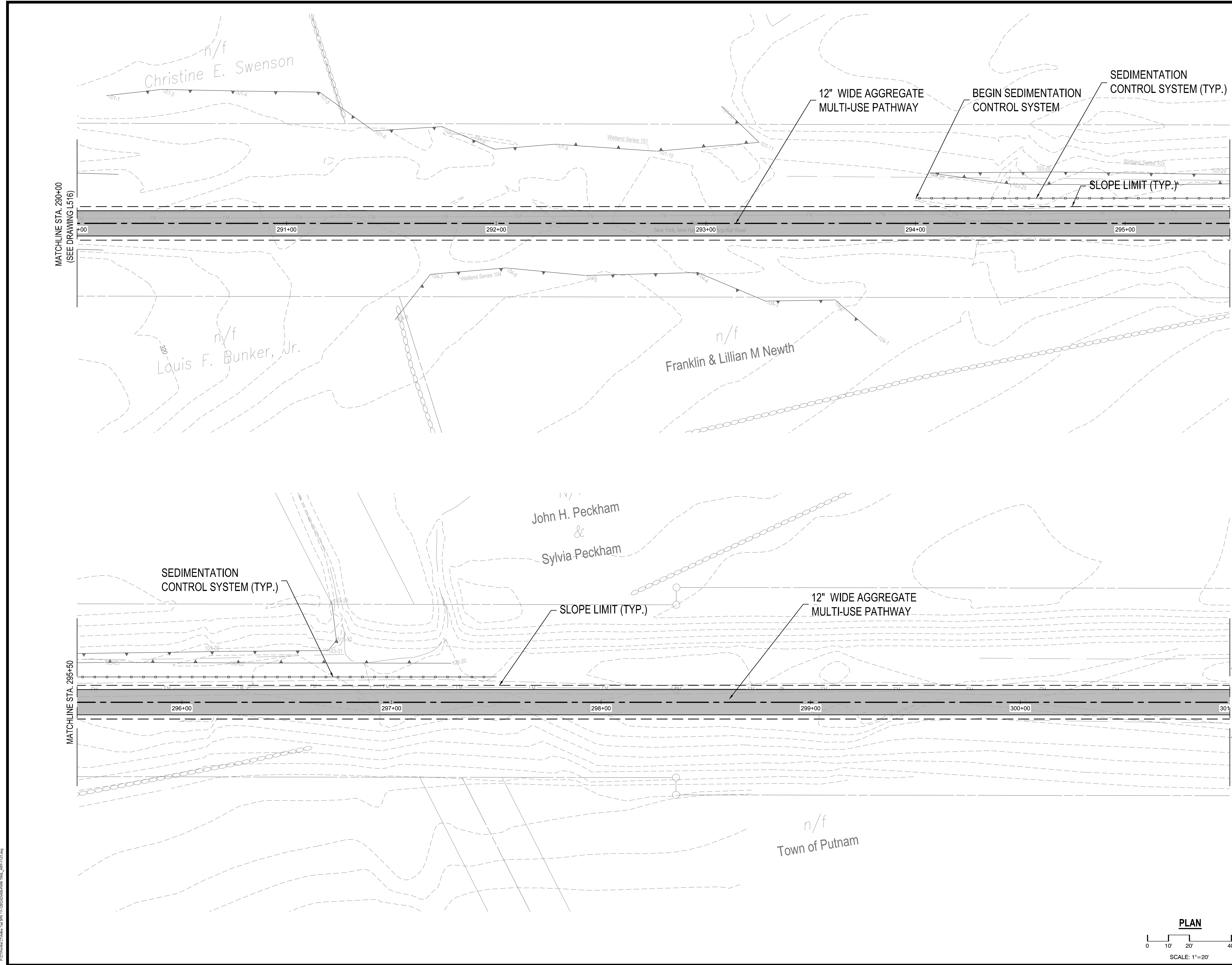
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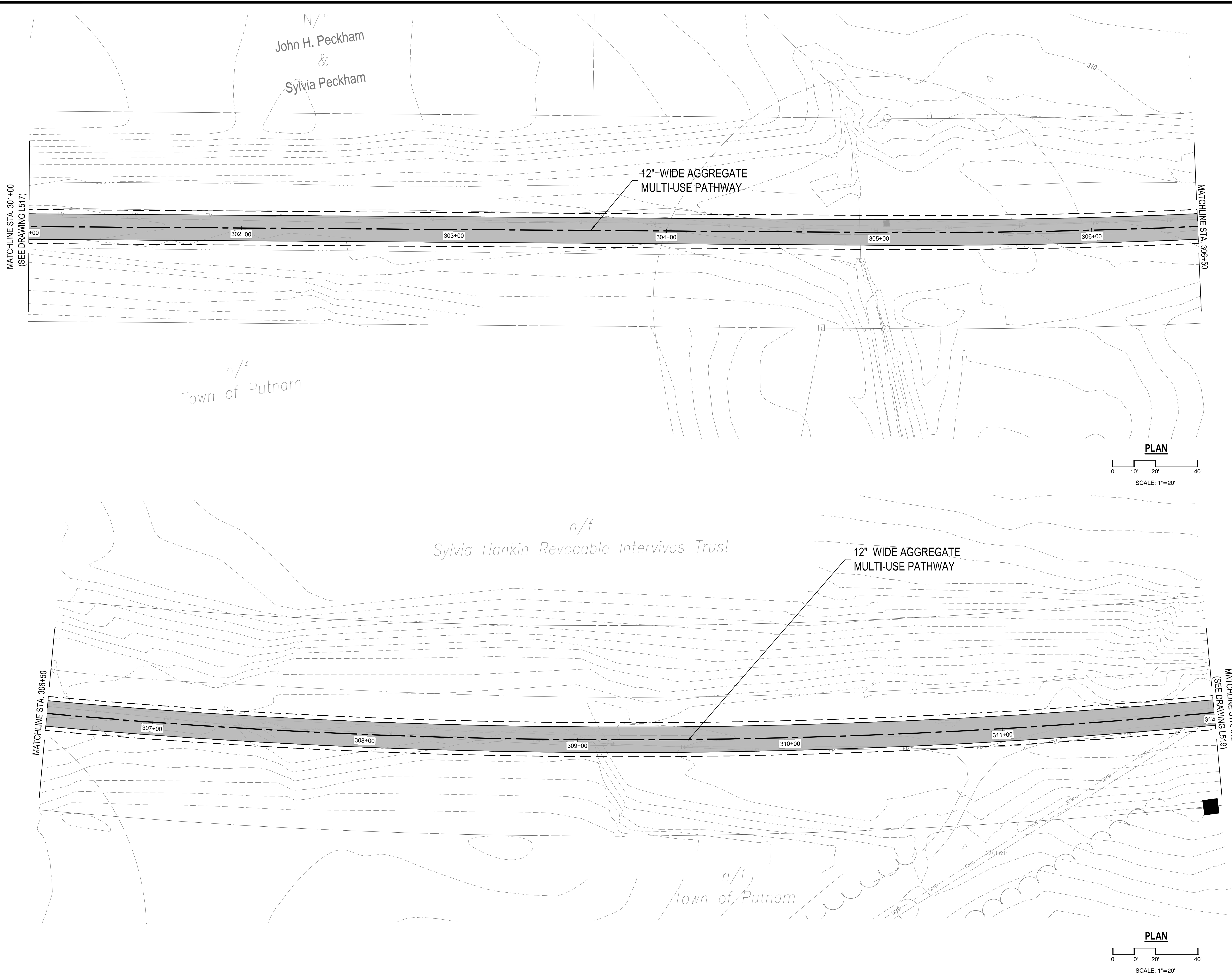
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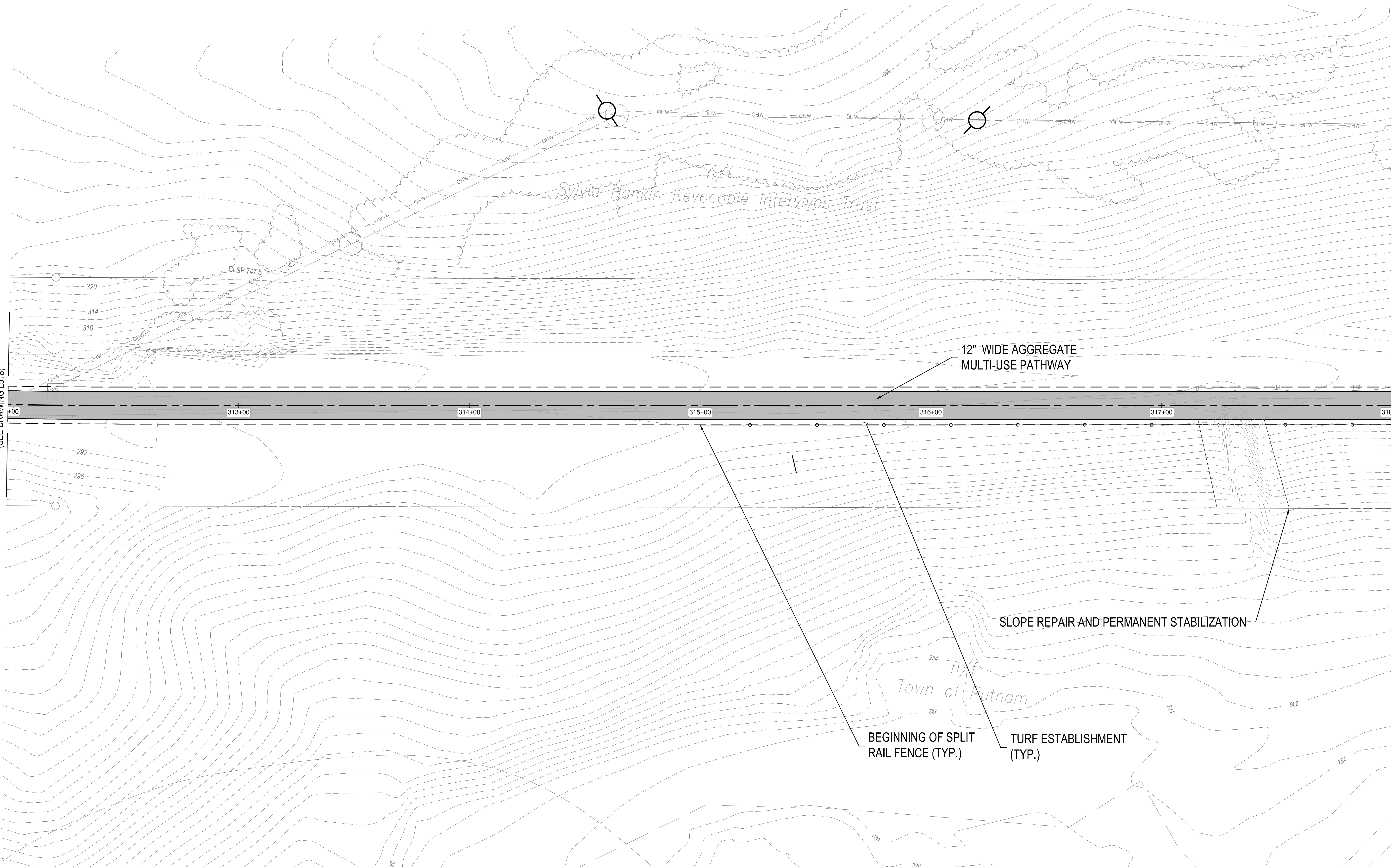
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(SEE DRAWING L518)

MATCHLINE STA. 318+00  
(SEE DRAWING L520)



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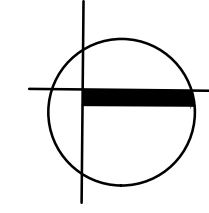
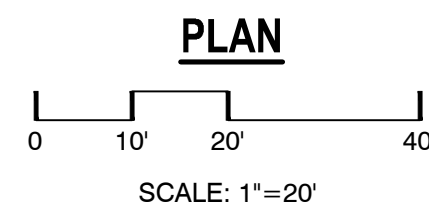
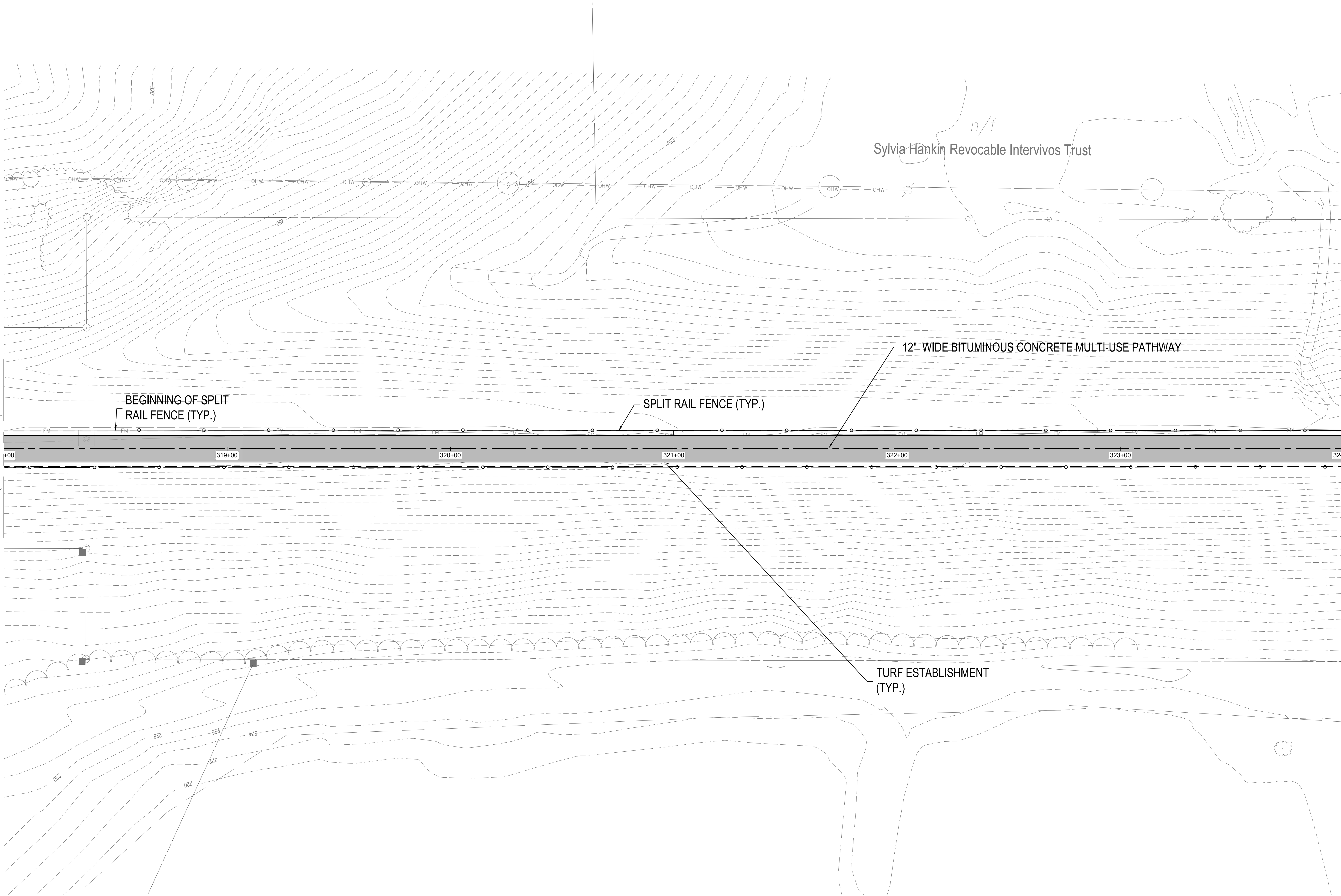
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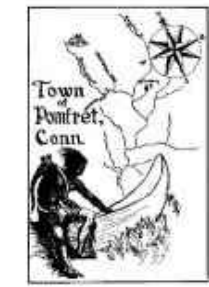
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MATCHLINE STA. 324+00  
(SEE DRAWING L521)



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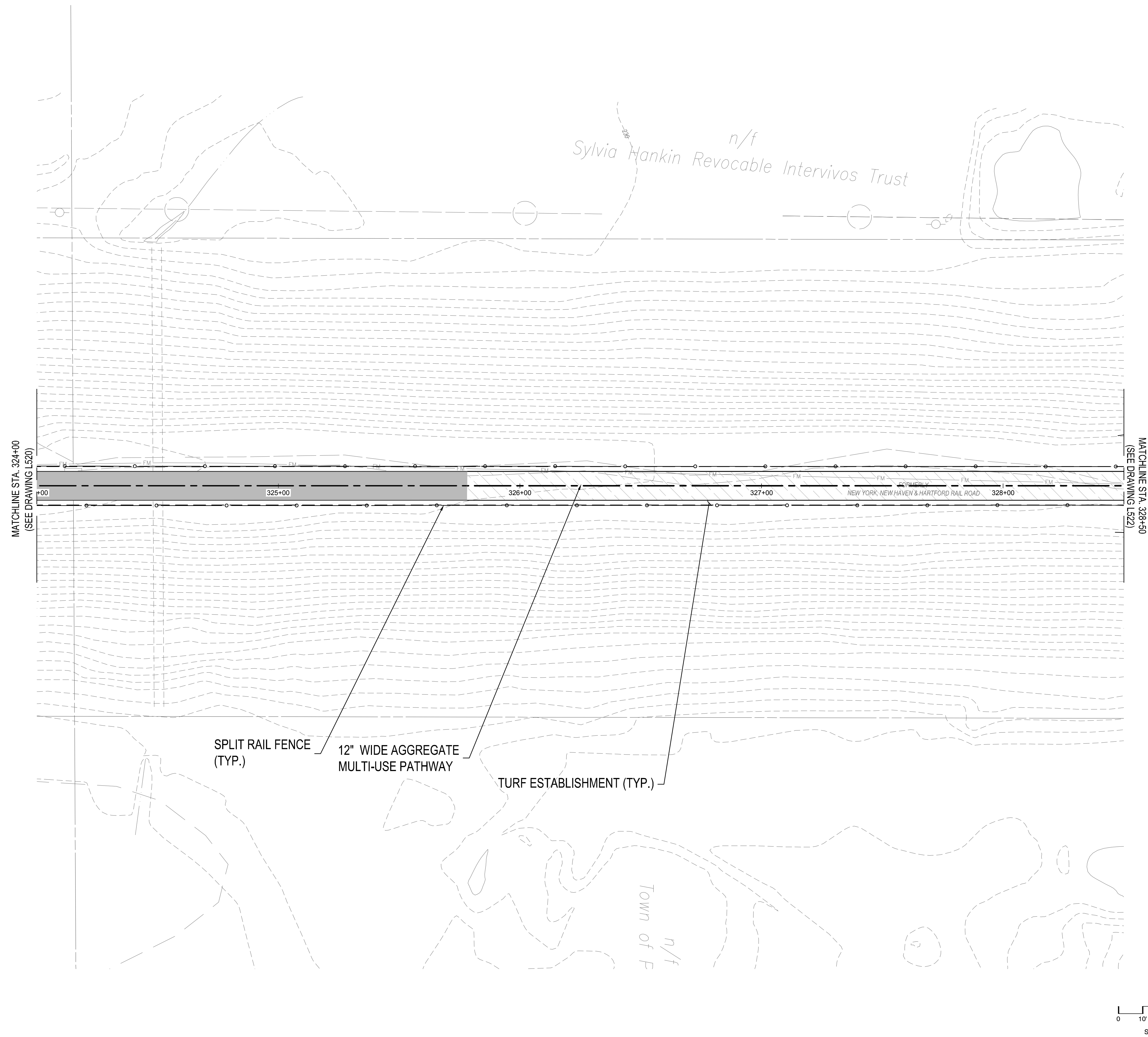
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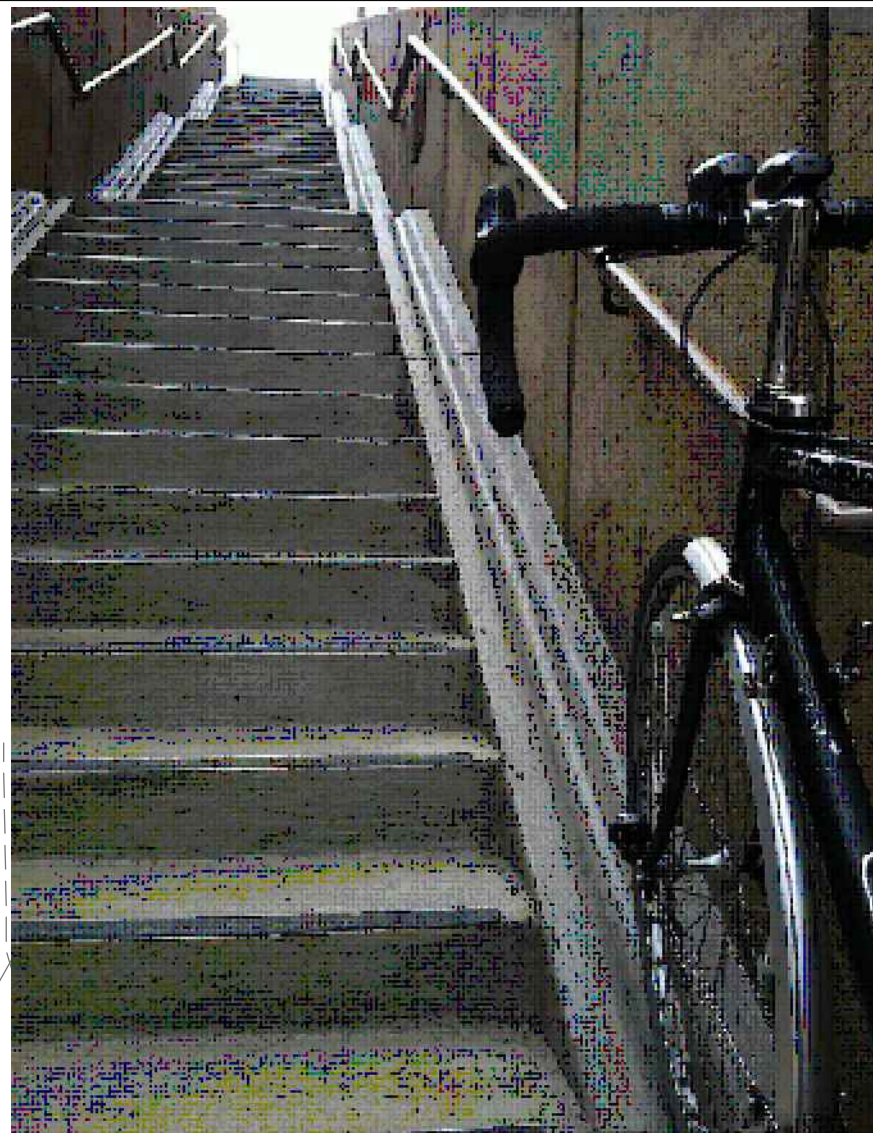
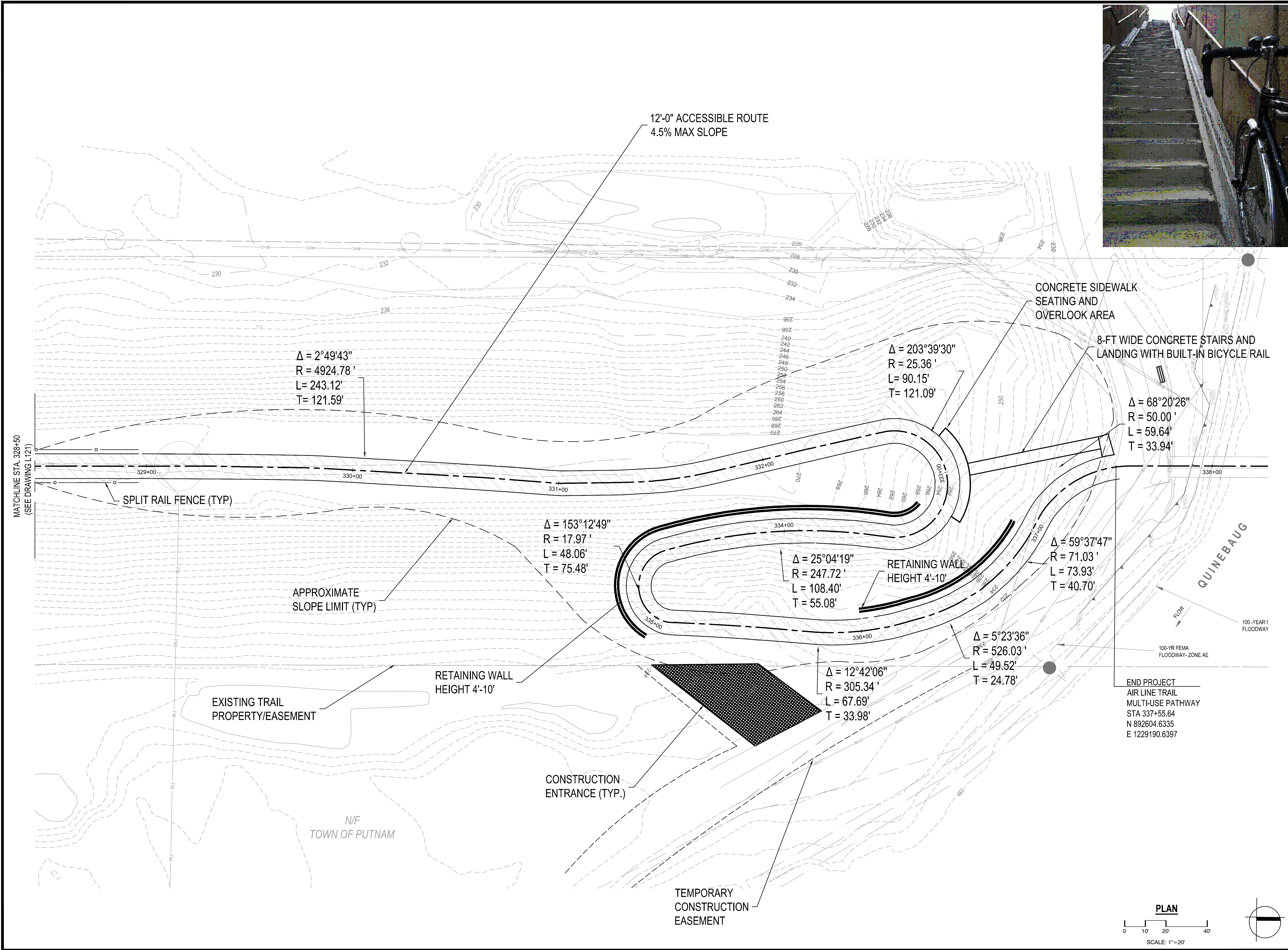
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Scale: 1"=20'

Date: August 28, 2023  
Drawn By: AMN  
Reviewed By: RC  
Approved By: MJJ

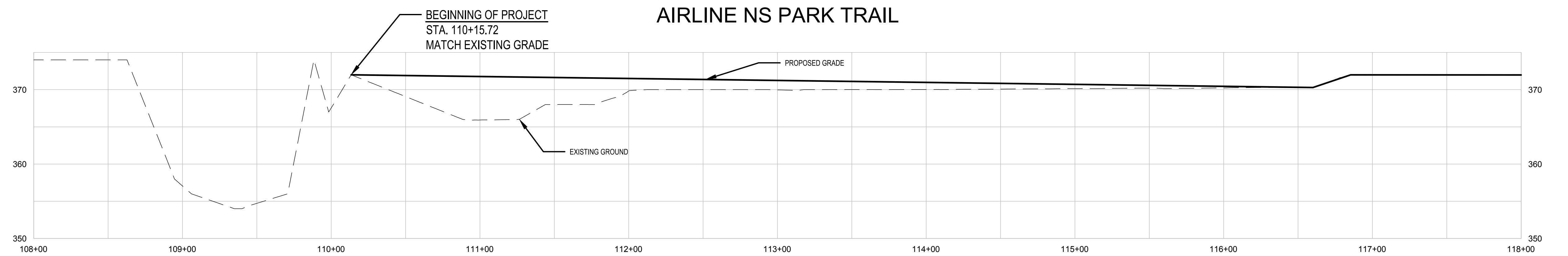
W&S Project No.: ENG22-1336  
W&S File No.: ----

Drawing Title:  
**PLAN SHEETS**

Sheet Number:  
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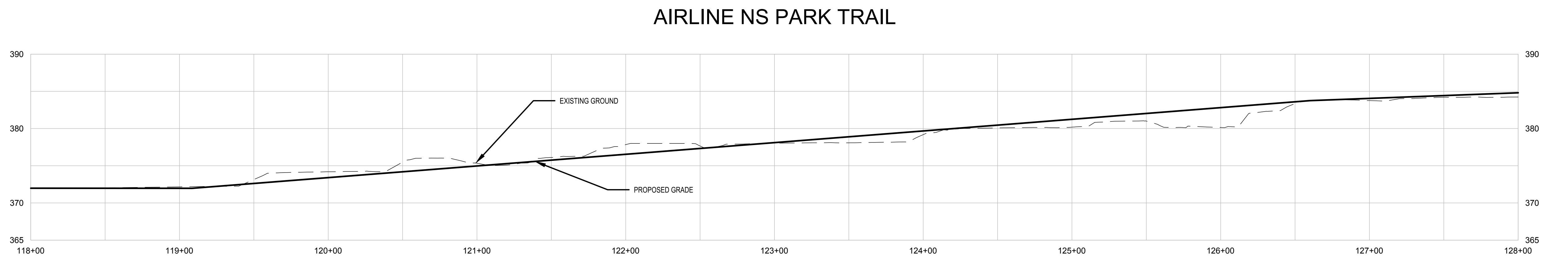


**PROFILE**

0 20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'



**PROFILE**

0 20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

Project:  
TAP PROJECT NO. 111-126

AIR LINE TRAIL

TOWNS OF PUTNAM AND  
POMFRET, CT

**Weston & Sampson**

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W&S File No.: ---

Drawing Title:

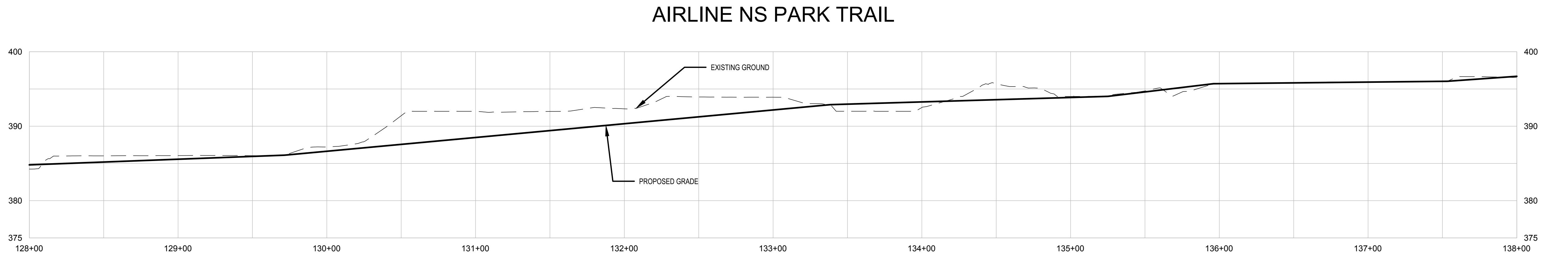
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STA. 108+00 TO 128+00**

Sheet Number:

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\\w011\land\100\Projects\22\Putnam, CT\Aerials - Final.dwg (11/28/2022) user: jrb@w.com

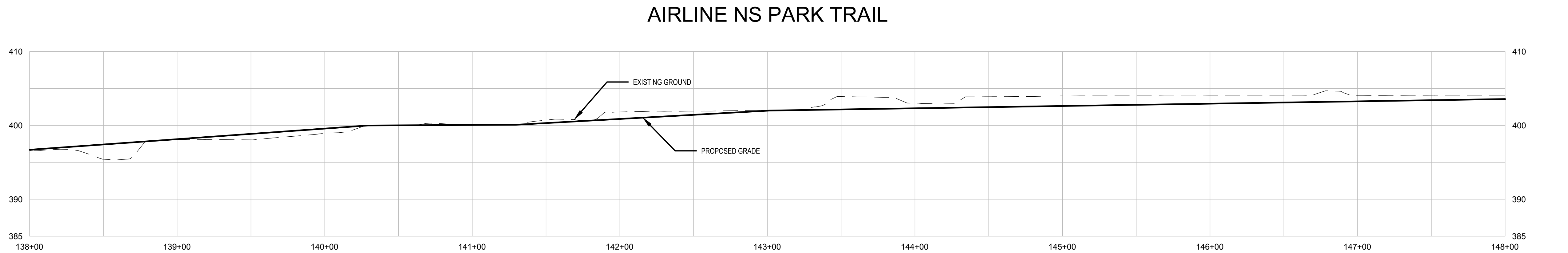


**PROFILE**

0 20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'



**PROFILE**

0 20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

Project:  
TAP PROJECT NO. 111-126

AIR LINE TRAIL

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Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ---

Drawing Title:

**EXISTING PROFILE  
STA. 128+00 TO 148+00**

Sheet Number:

**L602**



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Drawn By: AMN

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W&S File No.: ----

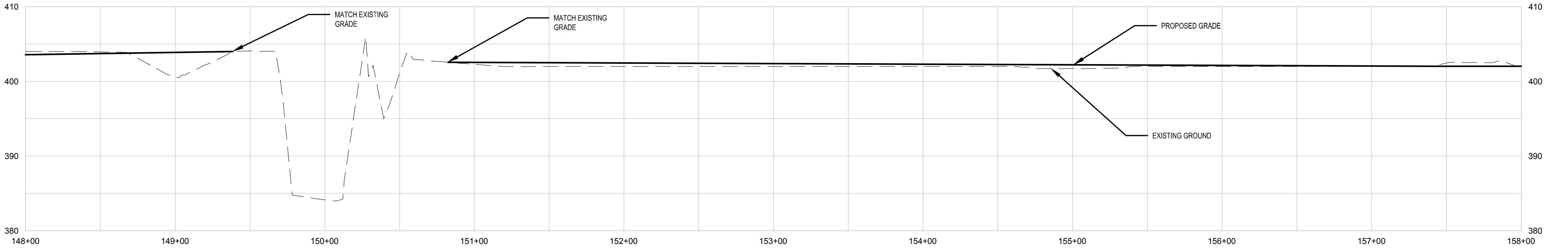
Drawing Title:

EXISTING PROFILE  
STA. 148+00 TO 168+00

Sheet Number:

L603

AIRLINE NS PARK TRAIL



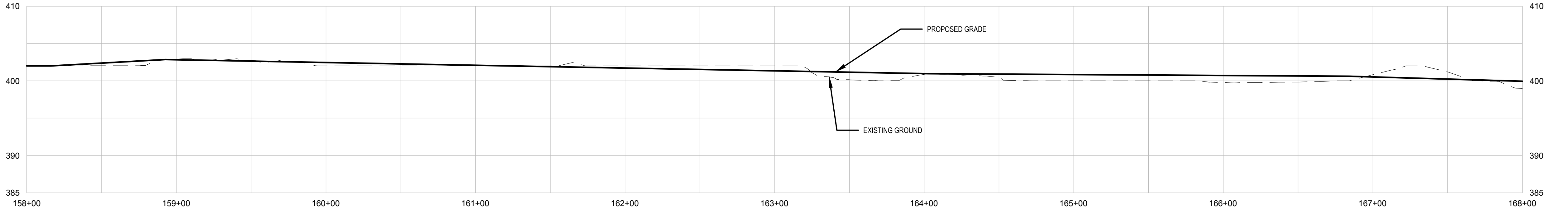
**PROFILE**

0 20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

AIRLINE NS PARK TRAIL



**PROFILE**

0 20' 40' 80'

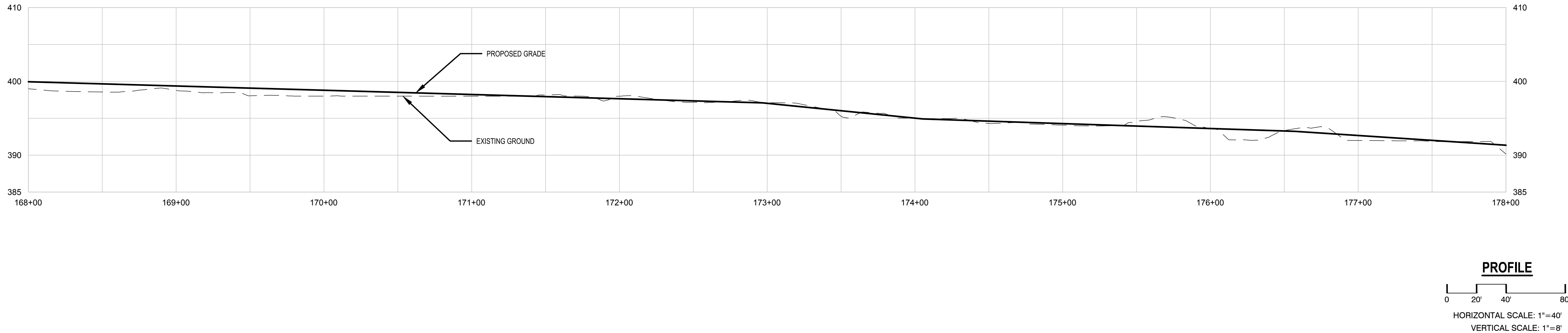
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VERTICAL SCALE: 1"=8'

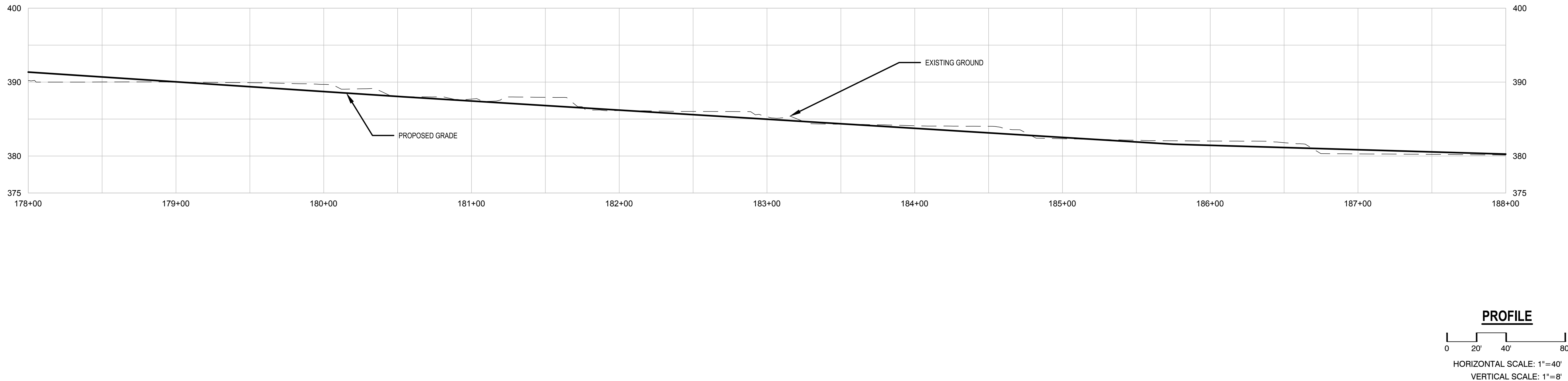


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AIRLINE NS PARK TRAIL



AIRLINE NS PARK TRAIL



Project:  
TAP PROJECT NO. 111-126

AIR LINE TRAIL

TOWNS OF PUTNAM AND  
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Drawn By: AMN  
Reviewed By: RC  
Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ----

Drawing Title:

**EXISTING PROFILE  
STA. 168+00 TO 188+00**

Sheet Number:

**L604**



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W&S File No.: ----

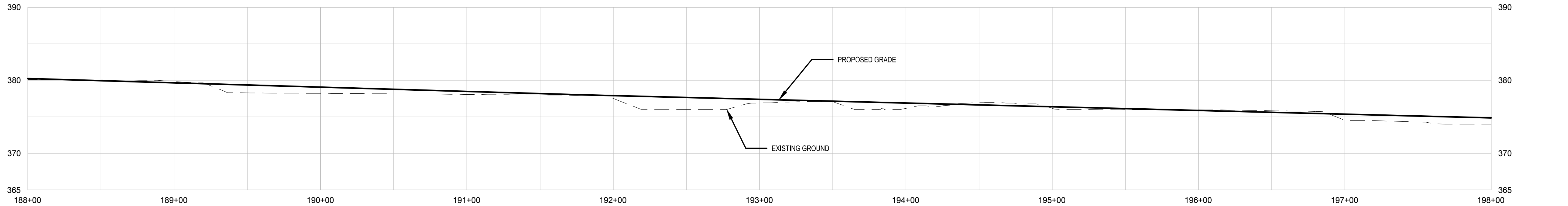
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EXISTING PROFILE  
STA. 188+00 TO 208+00

Sheet Number:

L605

AIRLINE NS PARK TRAIL



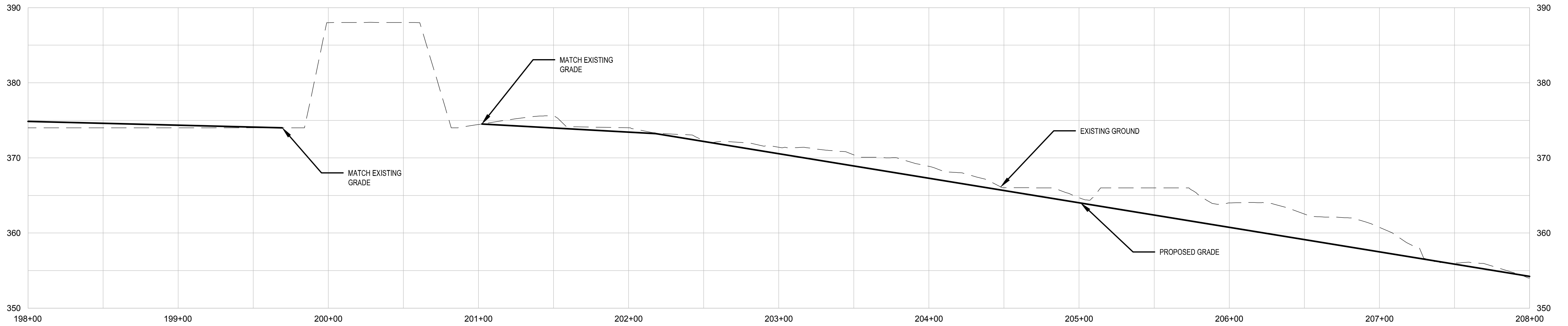
PROFILE

0 20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

AIRLINE NS PARK TRAIL



PROFILE

0 20' 40' 80'

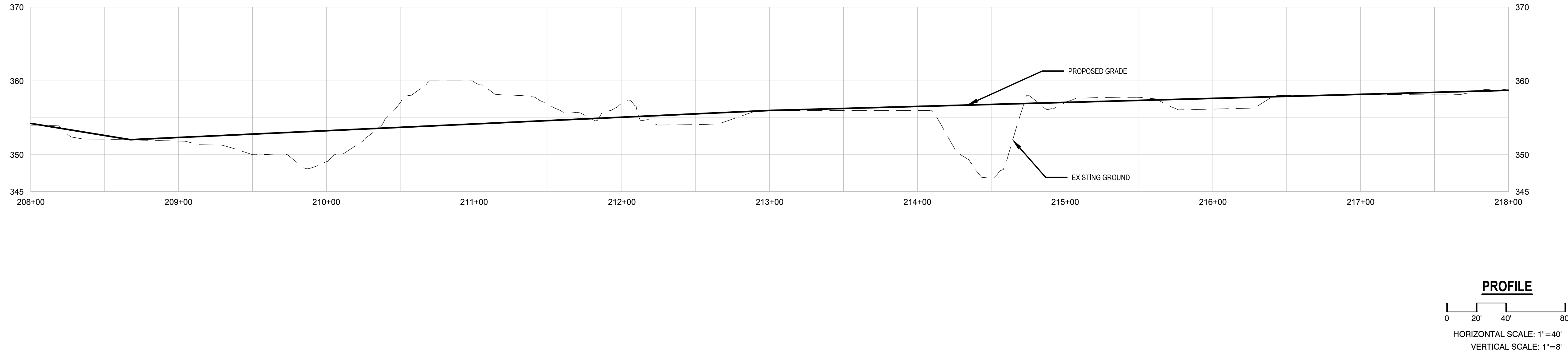
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VERTICAL SCALE: 1"=8'

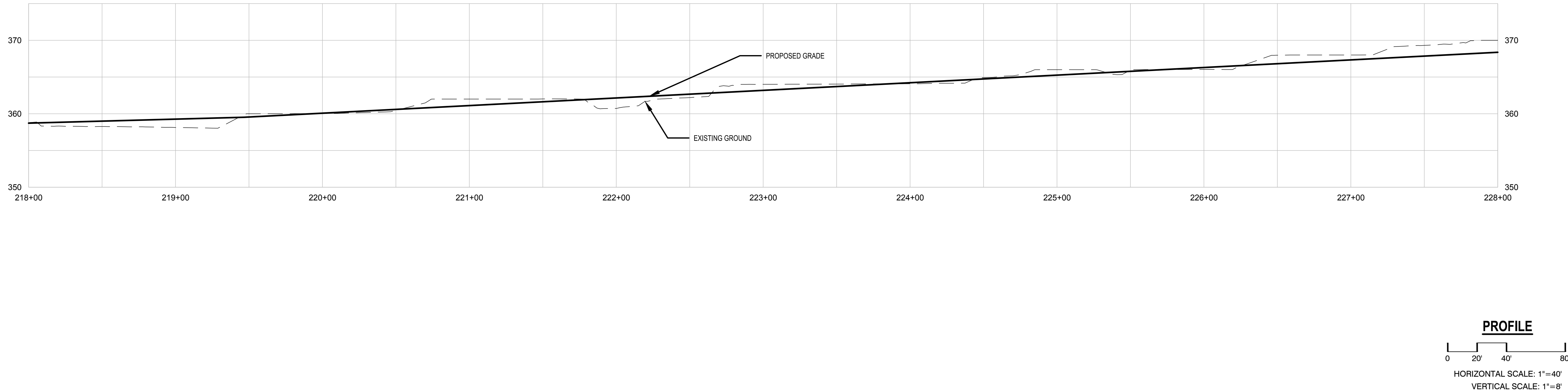


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AIRLINE NS PARK TRAIL



AIRLINE NS PARK TRAIL



Project:  
TAP PROJECT NO. 111-126



AIR LINE TRAIL

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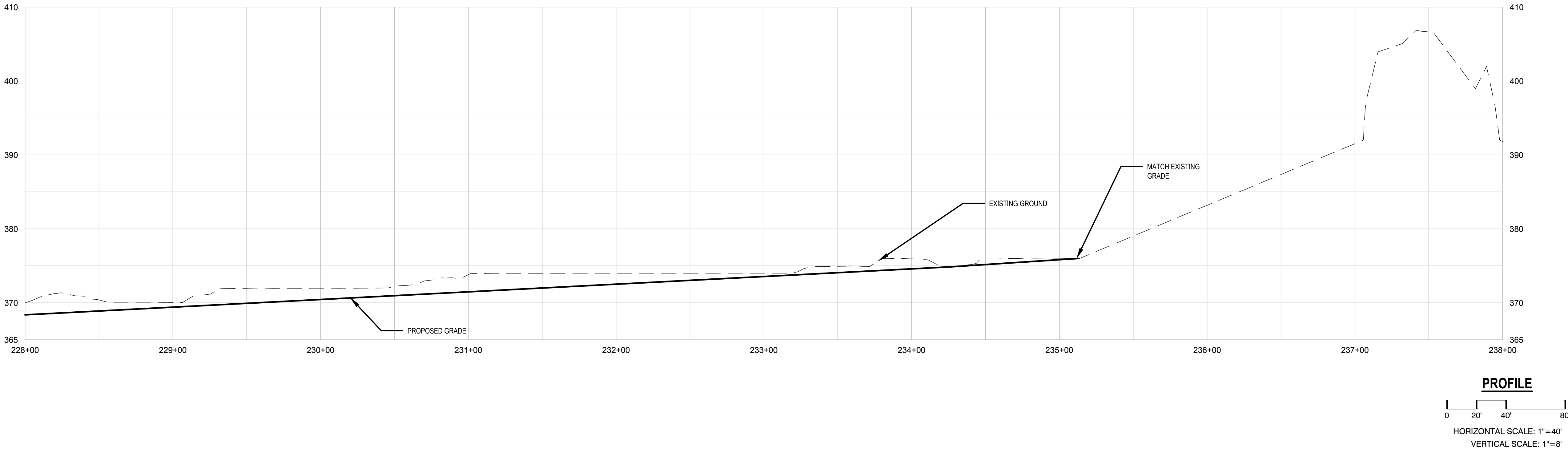
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Drawn By: AMN  
Reviewed By: RC  
Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ---

Drawing Title:  
**EXISTING PROFILE  
STA. 208+00 TO 228+00**  
  
Sheet Number:  
**L606**

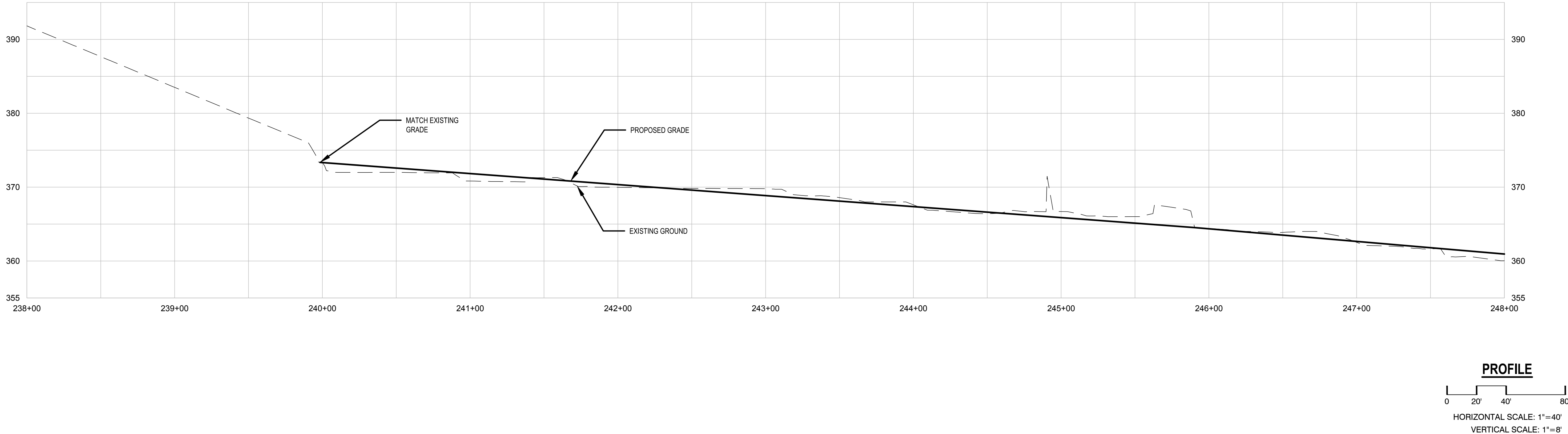


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AIRLINE NS PARK TRAIL



AIRLINE NS PARK TRAIL



Project:  
TAP PROJECT NO. 111-126



AIR LINE TRAIL

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Reviewed By: RC  
Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ---

Drawing Title:

**EXISTING PROFILE  
STA. 128+00 TO 148+00**

Sheet Number:

**L607**



## AIR LINE TRAIL

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Approved By: MJJ

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W&amp;S File No.:

Drawing Title:

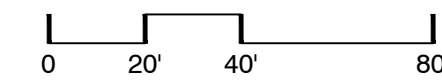
EXISTING PROFILE  
STA. 248+00 TO 268+00

Sheet Number:

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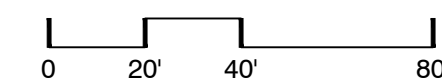
## PROFILE



HORIZONTAL SCALE: 1"=40'  
VERTICAL SCALE: 1"=8'

The profile view shows the vertical alignment of the proposed road. The horizontal axis represents stationing from 258+00 to 268+00. The vertical axis represents elevation in feet, ranging from 335 to 360. The 'PROPOSED GRADE' is shown as a solid black line, which starts at an elevation of approximately 350 feet at station 258+00 and gradually descends to about 340 feet at station 268+00. The 'EXISTING GROUND' is shown as a dashed line, which follows the proposed grade closely but shows significant dips and rises, particularly between stations 258+00 and 261+00. The 'MATCH EXISTING GRADE' is indicated at two points: one at station 263+00 where the proposed grade meets an existing grade that rises sharply to about 360 feet, and another at station 264+00 where the proposed grade meets an existing grade that remains relatively flat around 340 feet.

## PROFILE



HORIZONTAL SCALE: 1"=40'  
VERTICAL SCALE: 1"=8'

\\base03\local\NSE\Projects\CT\Portret CT\Jm\Trill SPN 111-126\CAD\Test Profile.dwg



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Approved By:	MJJ

W&S Project No.:	ENG22-1336
W&S File No.:	----

Drawing Title:

EXISTING PROFILE  
STA. 268+00 TO 288+00

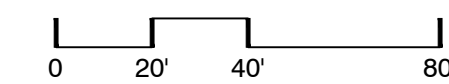
Sheet Number:

L609

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Profile view showing the proposed grade (solid line) and existing ground (dashed line) along the alignment. The vertical axis represents elevation (320 to 340), and the horizontal axis represents stationing (268+00 to 278+00). The proposed grade is generally higher than the existing ground, with a significant drop at station 274+00.

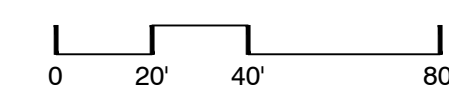
## PROFILE



HORIZONTAL SCALE: 1"=40'  
VERTICAL SCALE: 1"=8'

Profile view showing the proposed grade and existing ground. The vertical axis represents elevation (310 to 330) and the horizontal axis represents stationing (278+00 to 288+00). The proposed grade is shown as a solid line, and the existing ground is shown as a dashed line. The proposed grade starts at approximately 327.5 at station 278+00 and decreases to about 315.5 at station 288+00. The existing ground is generally higher than the proposed grade, especially between stations 278+00 and 283+00.

## PROFILE

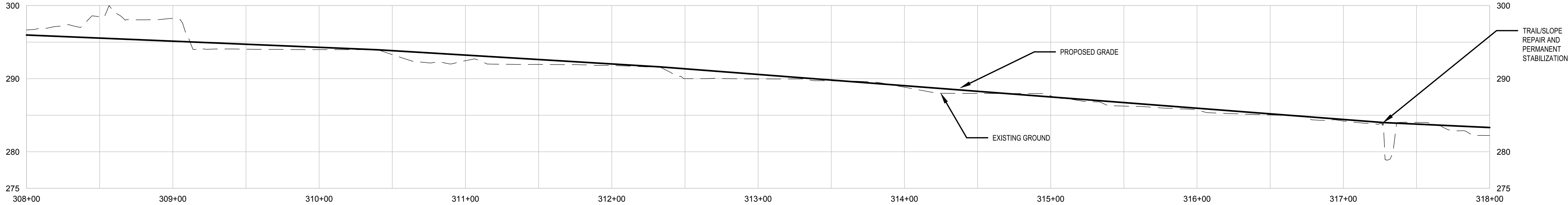


HORIZONTAL SCALE: 1"=40'  
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AIRLINE NS PARK TRAIL



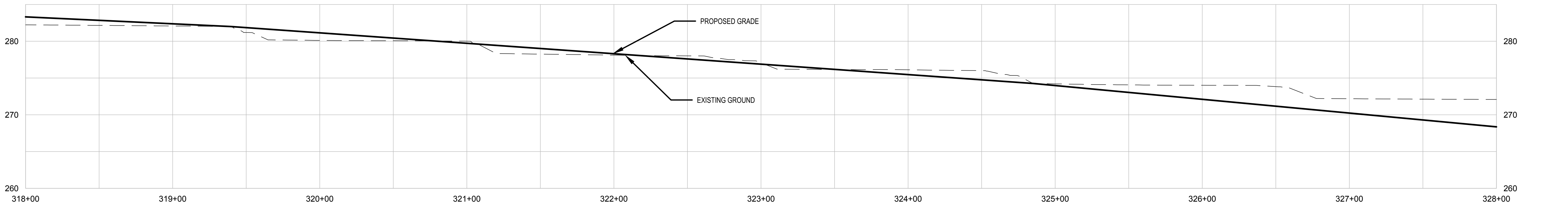
**PROFILE**

0 20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

AIRLINE NS PARK TRAIL




**PROFILE**

0 20' 40' 80'

HORIZONTAL SCALE: 1"=40'

VERTICAL SCALE: 1"=8'

Project:  
TAP PROJECT NO. 111-126



AIR LINE TRAIL

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W&S Project No.: ENG22-1336  
W&S File No.: ---

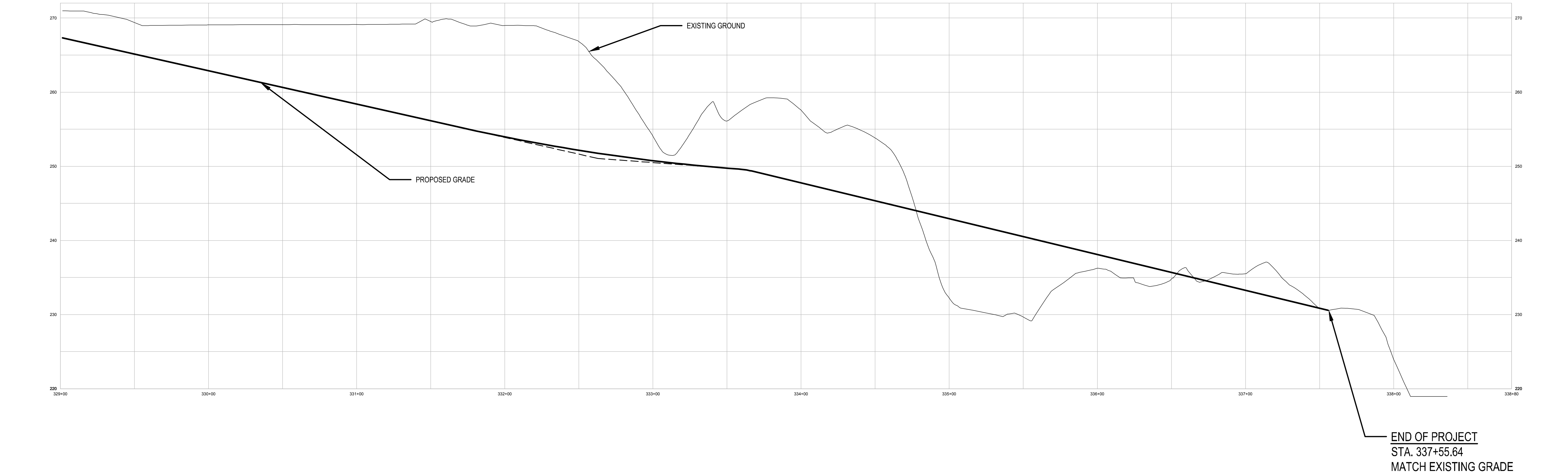
Drawing Title:

**EXISTING PROFILE  
STA. 308+00 TO 328+00**

Sheet Number:

**L610**





Project:  
TAP PROJECT NO. 111-126

AIR LINE TRAIL

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W&S Project No.:	ENC22-1336
W&S File No.:	----

Drawing Title:  
**EXISTING PROFILE  
STA. 328+00 TO 334+25**

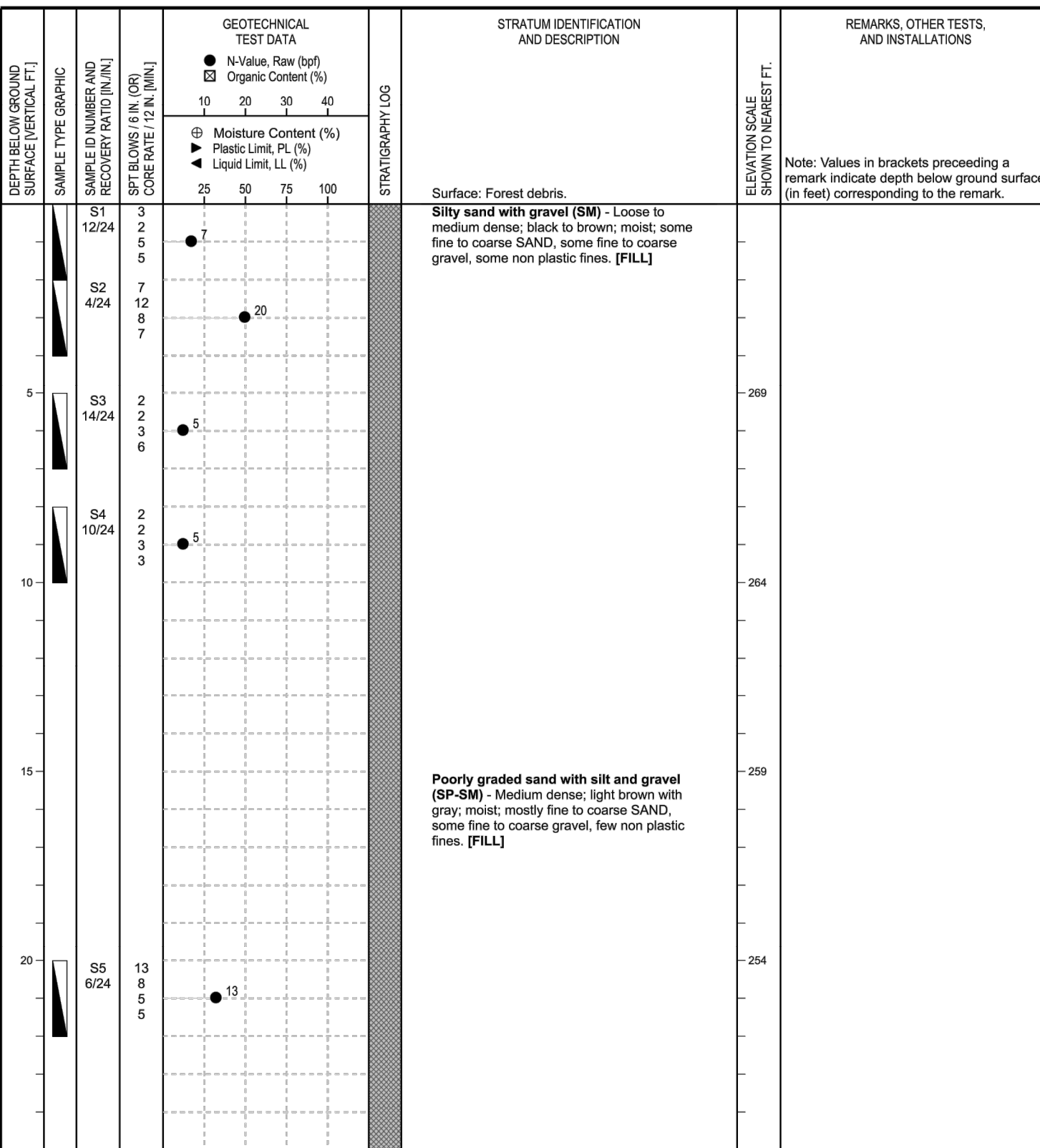
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WSE Project: ENG22-1336

Page 1 of 2

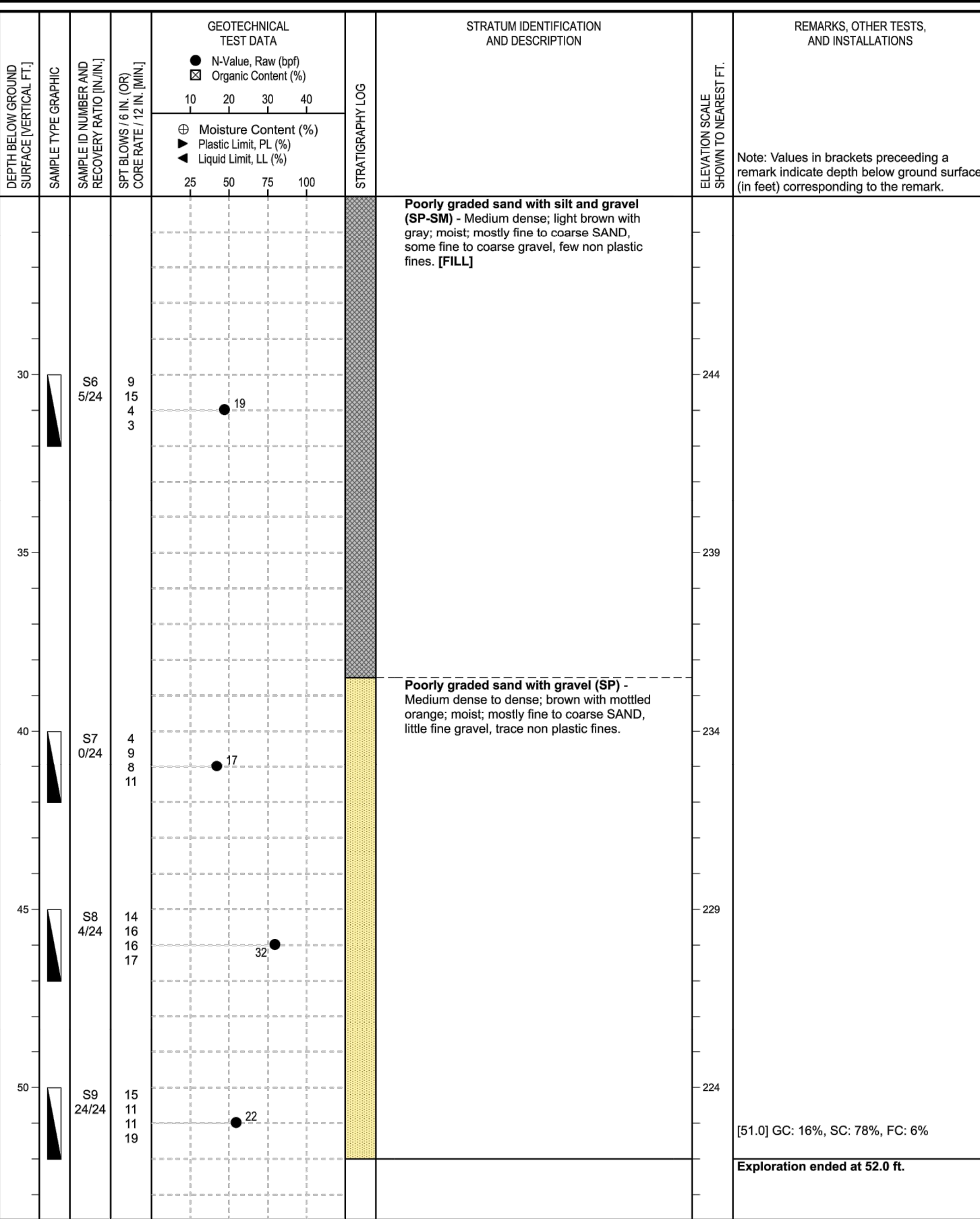
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FOREMAN: Jeff Nitsch ADVANCE METHOD: Hollow-Stem Auger Drilling DATE FINISH: January 27, 2023  
LOGGED BY: Geno Mirabella AUGER DIAMETER: 4-1/4" ID (Stem), 7-5/8" OD (Flights) GROUND EL: 274.0 ± (NAVD88)  
CHECKED BY: S. Spink, P.E. SUPPORT CASING: N/A FINAL DEPTH: 52.0 ft.  
EQUIPMENT: Dietrich D-50, ATV Mounted CORING METHOD: N/A GRID COORDS: N/A  
SPT HAMMER: Automatic (140-lb.) BACKFILL MATERIAL: Drill Cuttings GRID SYSTEM: Not Available



Refer to the attached index sheets for important information about this log including general notes, legends, and guidance on description methods and procedures.

WSE Project: ENG22-1336

Page 2 of 2

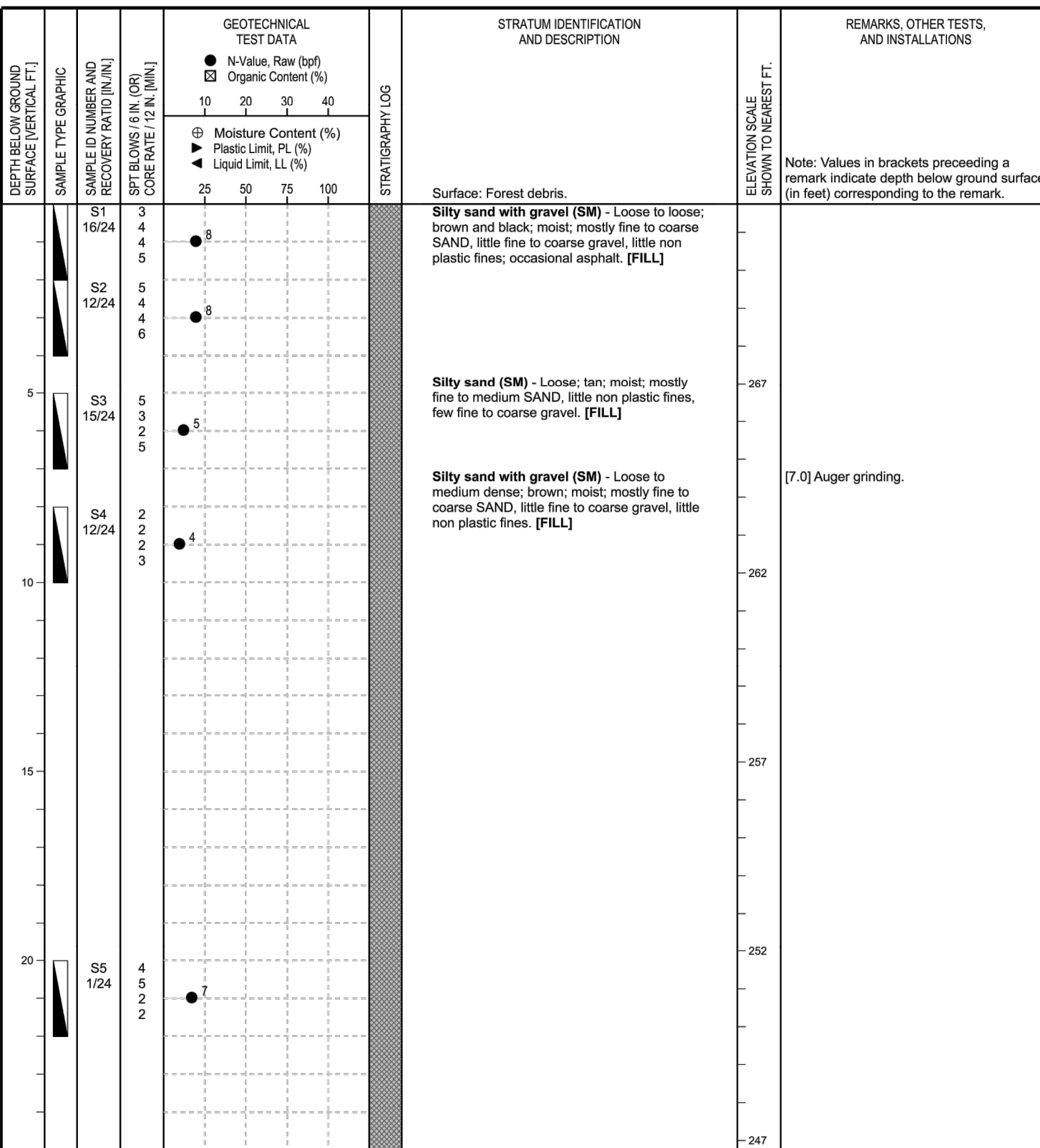


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WSE Project: ENG22-1336

Page 1 of 2

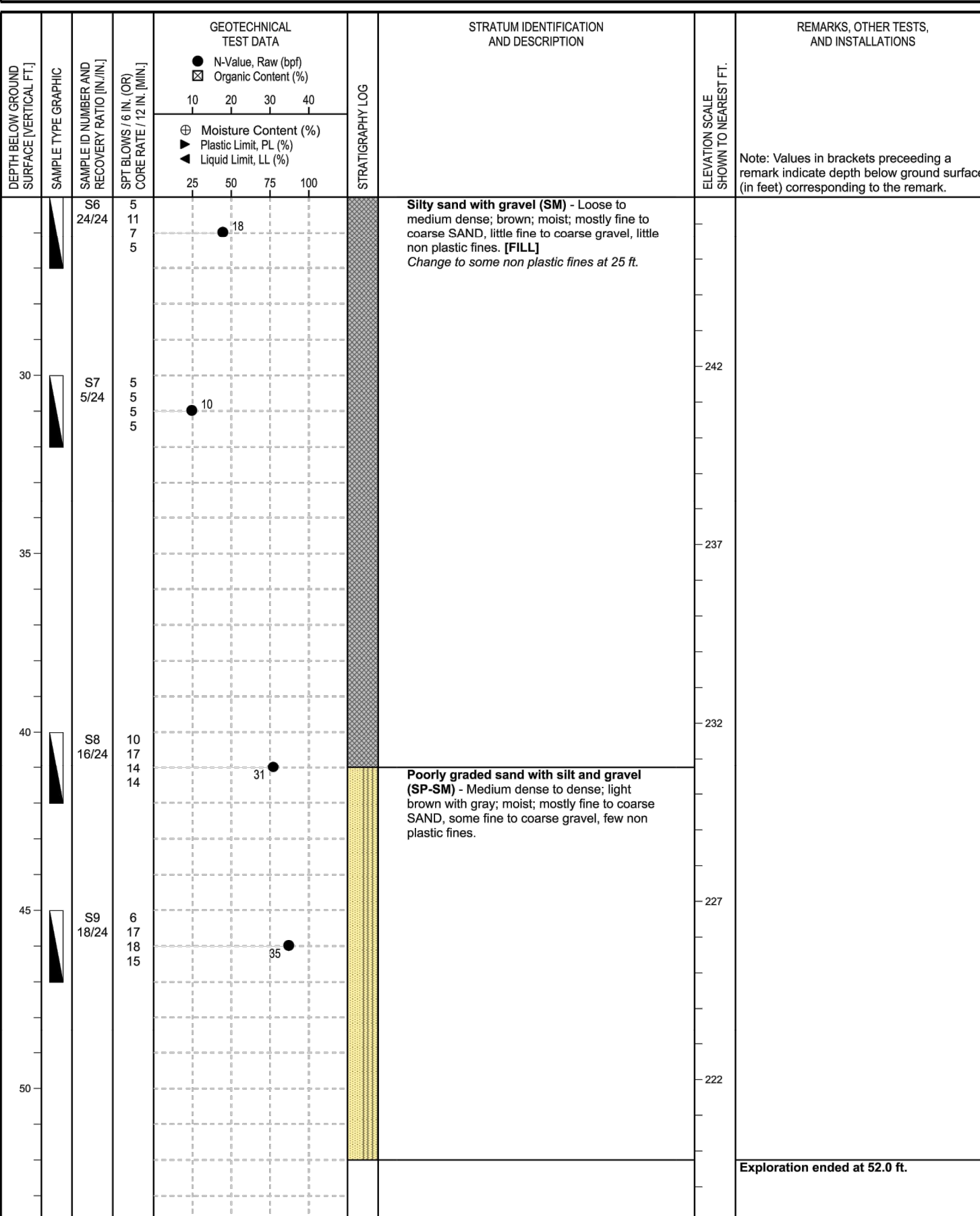
CONTRACTOR: Seaboard Drilling, Inc. BORING LOCATION: See Site Exploration Map DATE START: January 27, 2023  
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CHECKED BY: S. Spink, P.E. SUPPORT CASING: N/A FINAL DEPTH: 52.0 ft.  
EQUIPMENT: Dietrich D-50, ATV Mounted CORING METHOD: N/A GRID COORDS: N/A  
SPT HAMMER: Automatic (140-lb.) BACKFILL MATERIAL: Drill Cuttings GRID SYSTEM: Not Available



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WSE Project: ENG22-1336

Page 2 of 2

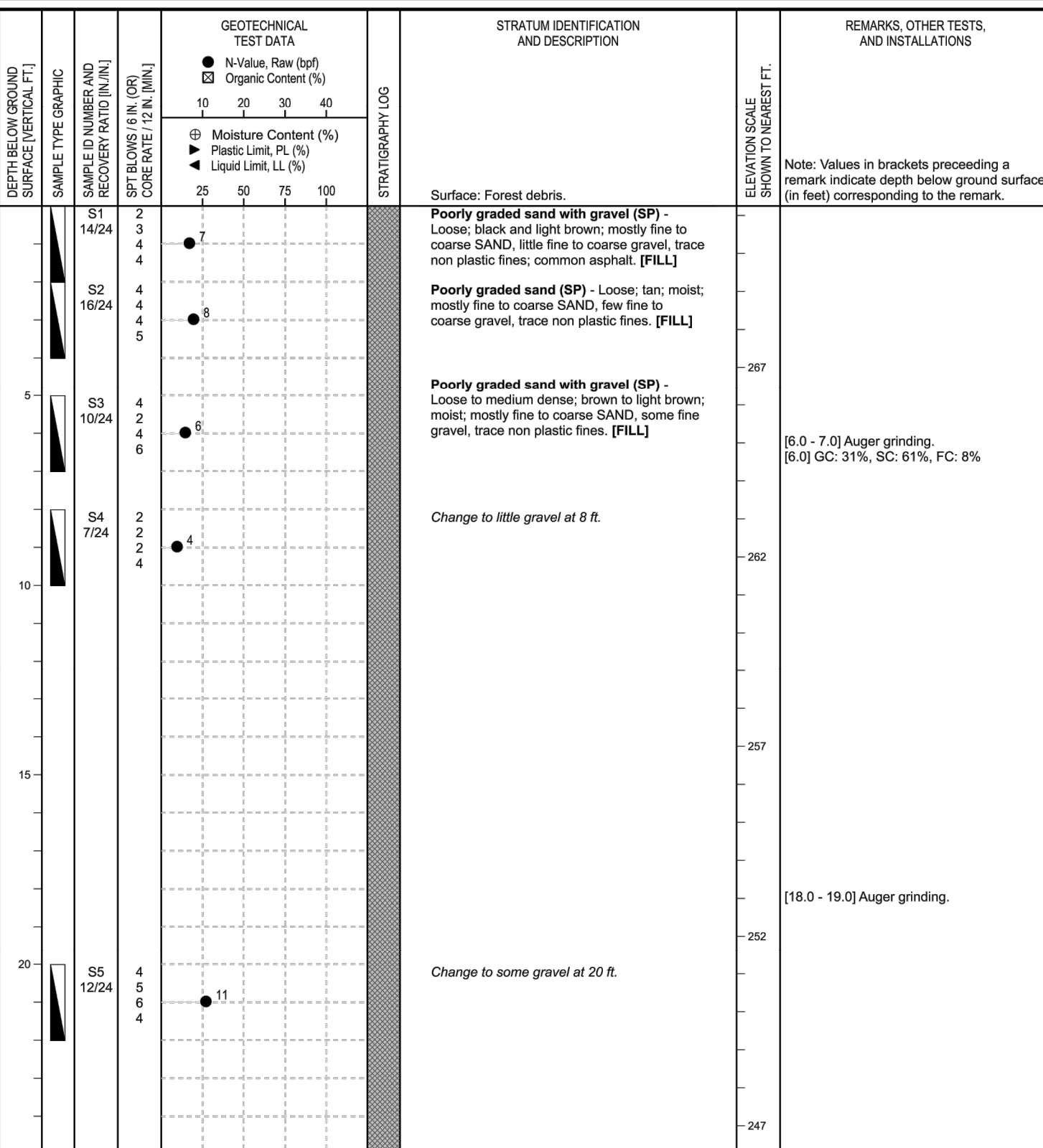


Refer to the attached index sheets for important information about this log including general notes, legends, and guidance on description methods and procedures.

WSE Project: ENG22-1336

Page 1 of 2

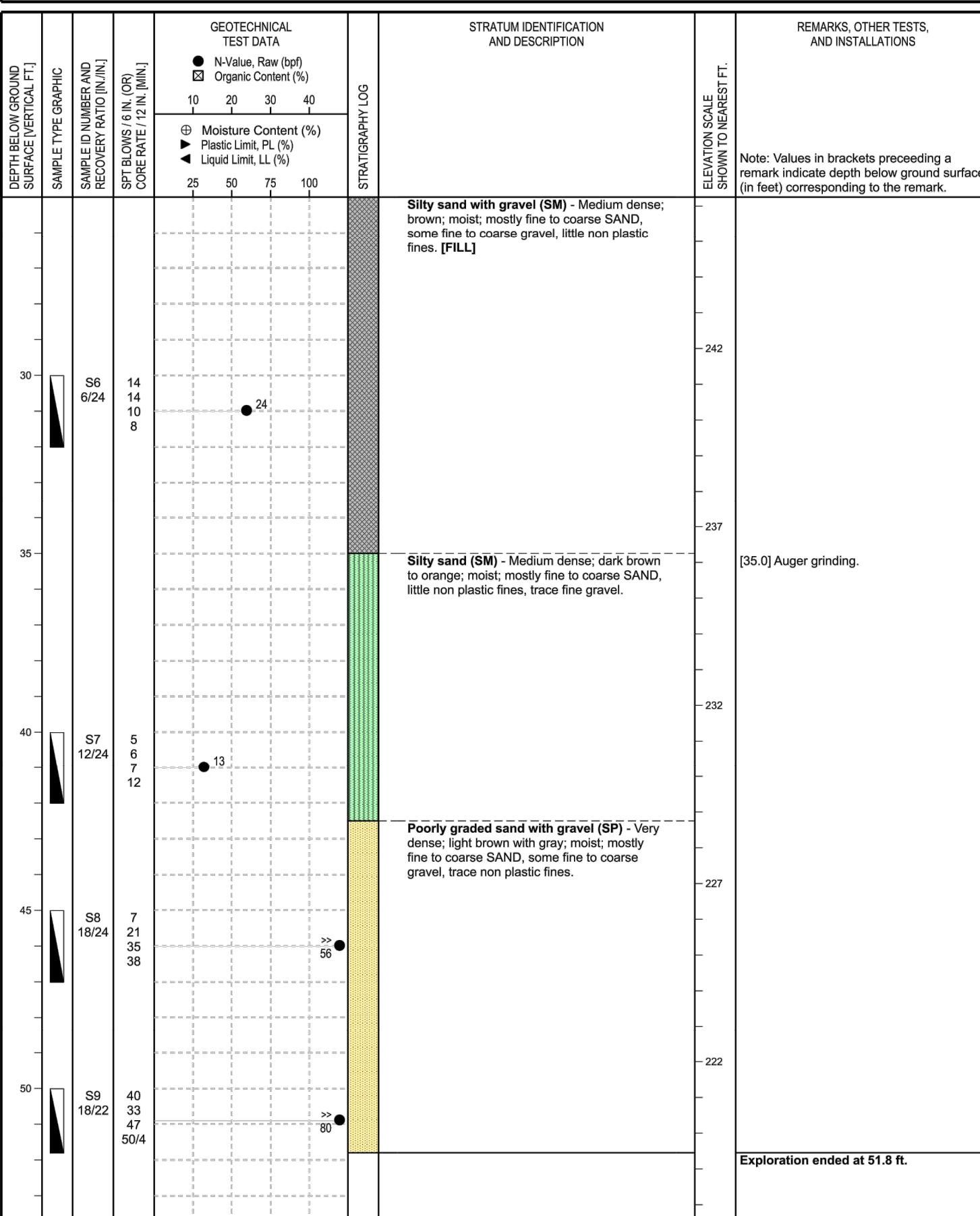
CONTRACTOR: Seaboard Drilling, Inc. BORING LOCATION: See Site Exploration Map DATE START: January 28, 2023  
FOREMAN: Jeff Nitsch ADVANCE METHOD: Hollow-Stem Auger Drilling DATE FINISH: January 28, 2023  
LOGGED BY: Geno Mirabella AUGER DIAMETER: 4-1/4" ID (Stem), 7-5/8" OD (Flights) GROUND EL: 271.2 ± (NAVD88)  
CHECKED BY: S. Spink, P.E. SUPPORT CASING: N/A FINAL DEPTH: 51.8 ft.  
EQUIPMENT: Dietrich D-50, ATV Mounted CORING METHOD: N/A GRID COORDS: N/A  
SPT HAMMER: Automatic (140-lb.) BACKFILL MATERIAL: Drill Cuttings GRID SYSTEM: Not Available



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WSE Project: ENG22-1336

Page 2 of 2



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ACCESSIBILITY PROJECT  
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Scale: 1"=20'

Date: APRIL 28, 2023

Drawn By: GB

Reviewed By: RC

Approved By: MJJ

W&amp;S Project No.: ENG22-1336

W&amp;S File No.: ---

Drawing Title:

BORING LOGS

Sheet Number:

L700



Weston & Sampson

WSE Project: ENG22-1336

Airline State Park Trail

Pomfret, Putnam, CT

BORING ID: B-104

Page 1 of 2

CONTRACTOR: Seaboard Drilling, Inc.

BORING LOCATION: See Site Exploration Map

DATE START: January 25, 2023

FOREMAN: Jeff Nitsch

ADVANCE METHOD: Hollow-Stem Auger Drilling

DATE FINISH: January 25, 2023

LOGGED BY: Geno Mirabella

AUGER DIAMETER: 4-1/4" ID (Stem), 7-5/8" OD (Flights)

GROUND EL: 229.0 ± (NAVD88)

CHECKED BY: S. Spink, P.E.

SUPPORT CASING: N/A

FINAL DEPTH: 27.0 ft.

EQUIPMENT: Diedrich D-56, ATV Mounted

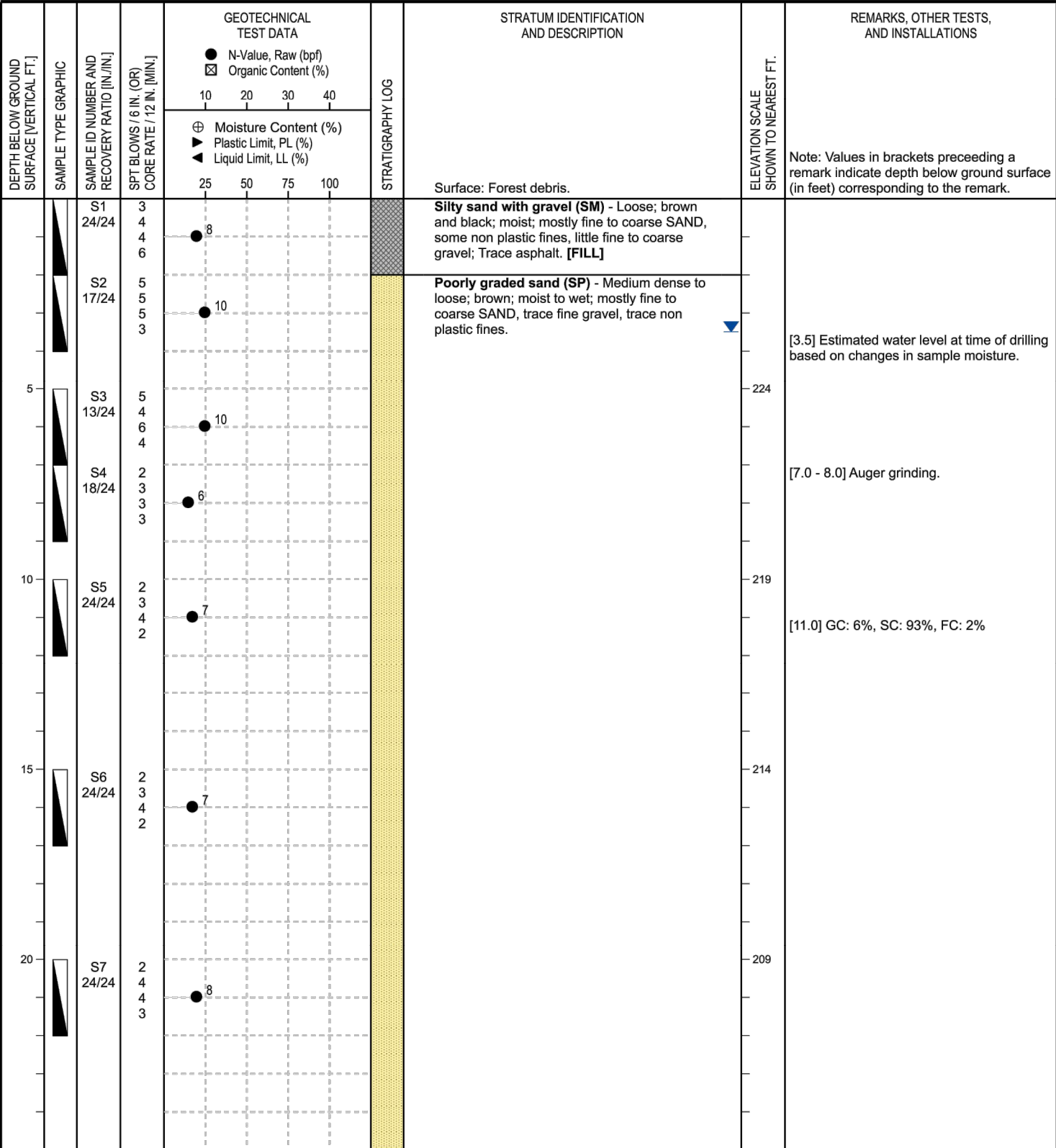
CORING METHOD: N/A

GRID COORDS: N/A

SPT HAMMER: Automatic (140-lb.)

BACKFILL MATERIAL: Drill Cuttings

GRID SYSTEM: Not Available



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Weston & Sampson

WSE Project: ENG22-1336

Airline State Park Trail

Pomfret, Putnam, CT

BORING ID: B-104

Page 2 of 2

CONTRACTOR: Seaboard Drilling, Inc.

BORING LOCATION: See Site Exploration Map

DATE START: January 25, 2023

FOREMAN: Jeff Nitsch

ADVANCE METHOD: Hollow-Stem Auger Drilling

DATE FINISH: January 25, 2023

LOGGED BY: Geno Mirabella

AUGER DIAMETER: 4-1/4" ID (Stem), 7-5/8" OD (Flights)

GROUND EL: 229.0 ± (NAVD88)

CHECKED BY: S. Spink, P.E.

SUPPORT CASING: N/A

FINAL DEPTH: 27.0 ft.

EQUIPMENT: Diedrich D-56, ATV Mounted

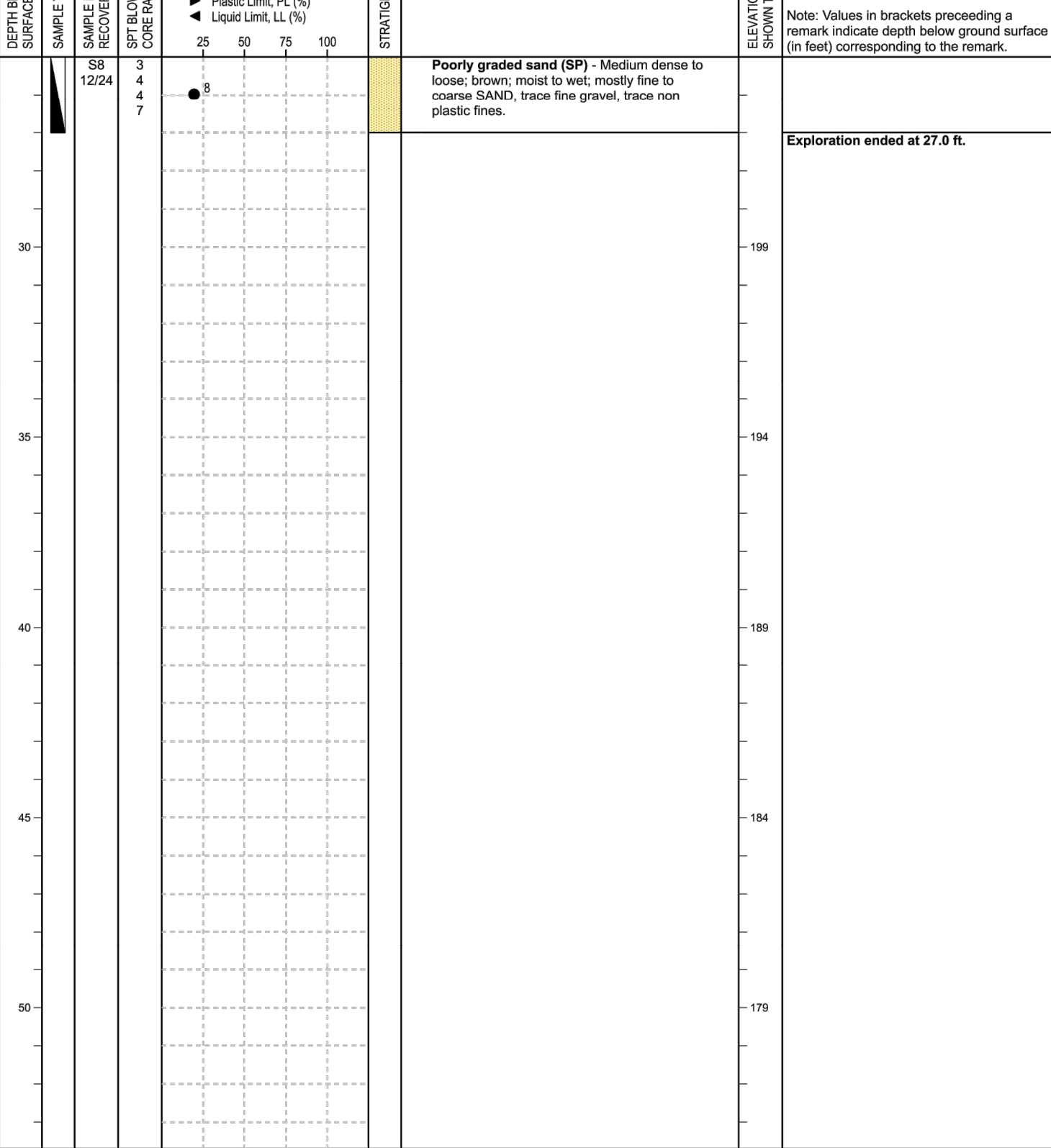
CORING METHOD: N/A

GRID COORDS: N/A

SPT HAMMER: Automatic (140-lb.)

BACKFILL MATERIAL: Drill Cuttings

GRID SYSTEM: Not Available



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WSE Project: ENG22-1336

Airline State Park Trail

Pomfret, Putnam, CT

BORING ID: B-105

Page 1 of 2

CONTRACTOR: Seaboard Drilling, Inc.

BORING LOCATION: See Site Exploration Map

DATE START: January 25, 2023

FOREMAN: Jeff Nitsch

ADVANCE METHOD: Hollow-Stem Auger Drilling

DATE FINISH: January 25, 2023

LOGGED BY: Geno Mirabella

AUGER DIAMETER: 4-1/4" ID (Stem), 7-5/8" OD (Flights)

GROUND EL: 228.0 ± (NAVD88)

CHECKED BY: S. Spink, P.E.

SUPPORT CASING: N/A

FINAL DEPTH: 28.0 ft. (Refusal)

EQUIPMENT: Diedrich D-56, ATV Mounted

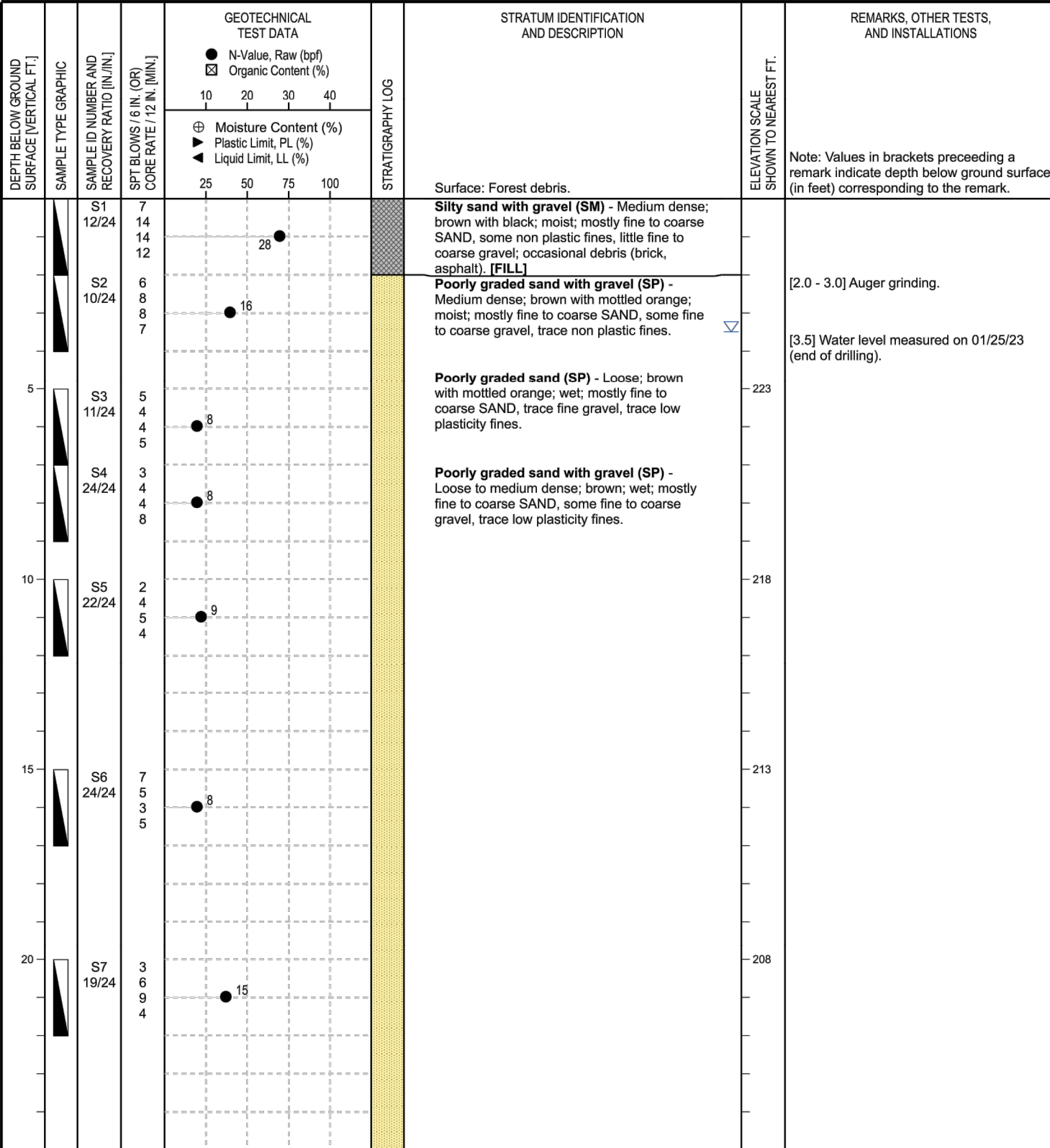
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GRID COORDS: N/A

SPT HAMMER: Automatic (140-lb.)

BACKFILL MATERIAL: Drill Cuttings

GRID SYSTEM: Not Available



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WSE Project: ENG22-1336

Airline State Park Trail

Pomfret, Putnam, CT

BORING ID: B-105

Page 2 of 2

CONTRACTOR: Seaboard Drilling, Inc.

BORING LOCATION: See Site Exploration Map

DATE START: January 25, 2023

FOREMAN: Jeff Nitsch

ADVANCE METHOD: Hollow-Stem Auger Drilling

DATE FINISH: January 25, 2023

LOGGED BY: Geno Mirabella

AUGER DIAMETER: 4-1/4" ID (Stem), 7-5/8" OD (Flights)

GROUND EL: 229.0 ± (NAVD88)

CHECKED BY: S. Spink, P.E.

SUPPORT CASING: N/A

FINAL DEPTH: 22.0 ft.

EQUIPMENT: Diedrich D-56, ATV Mounted

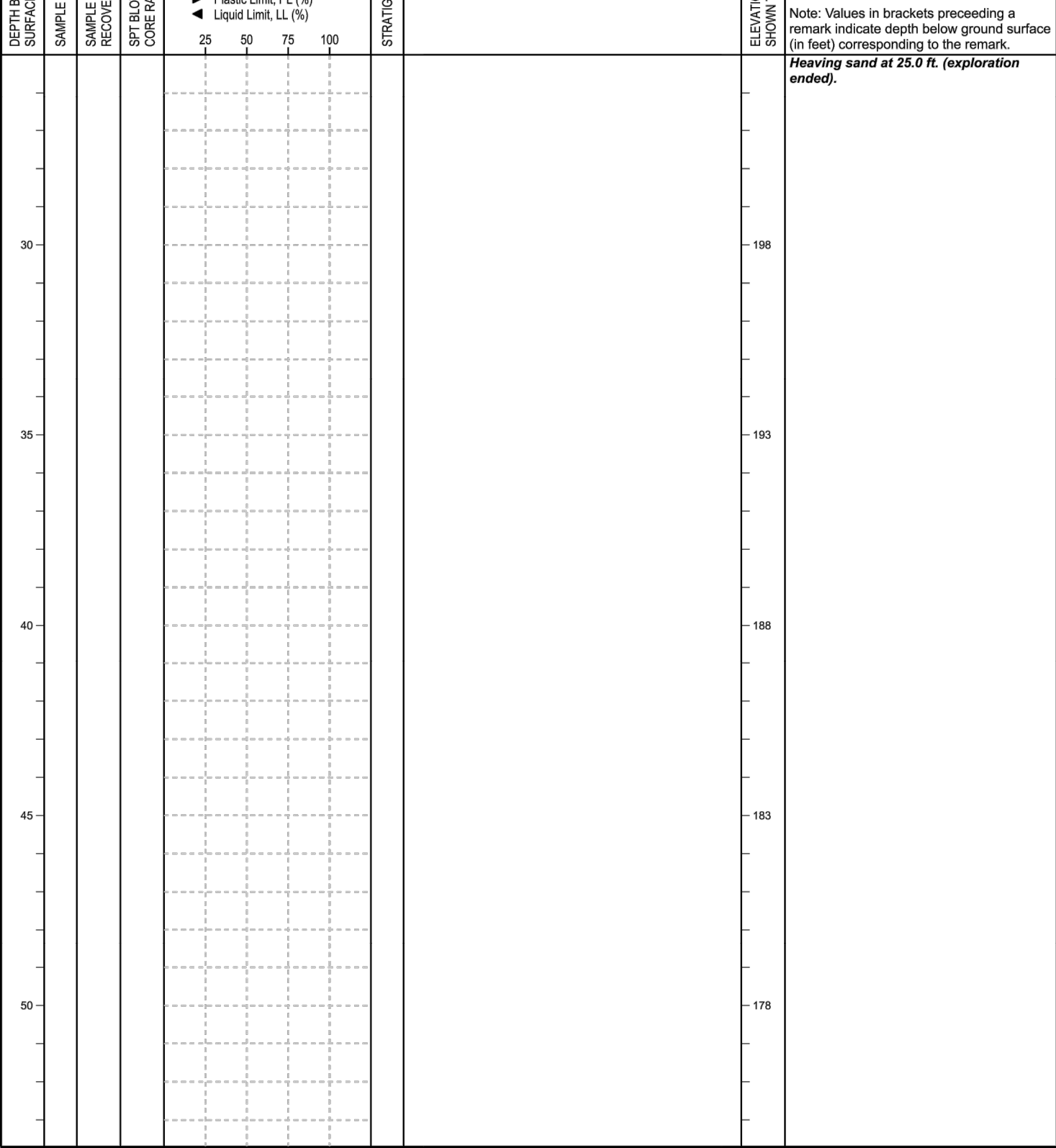
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GRID COORDS: N/A

SPT HAMMER: Automatic (140-lb.)

BACKFILL MATERIAL: Drill Cuttings

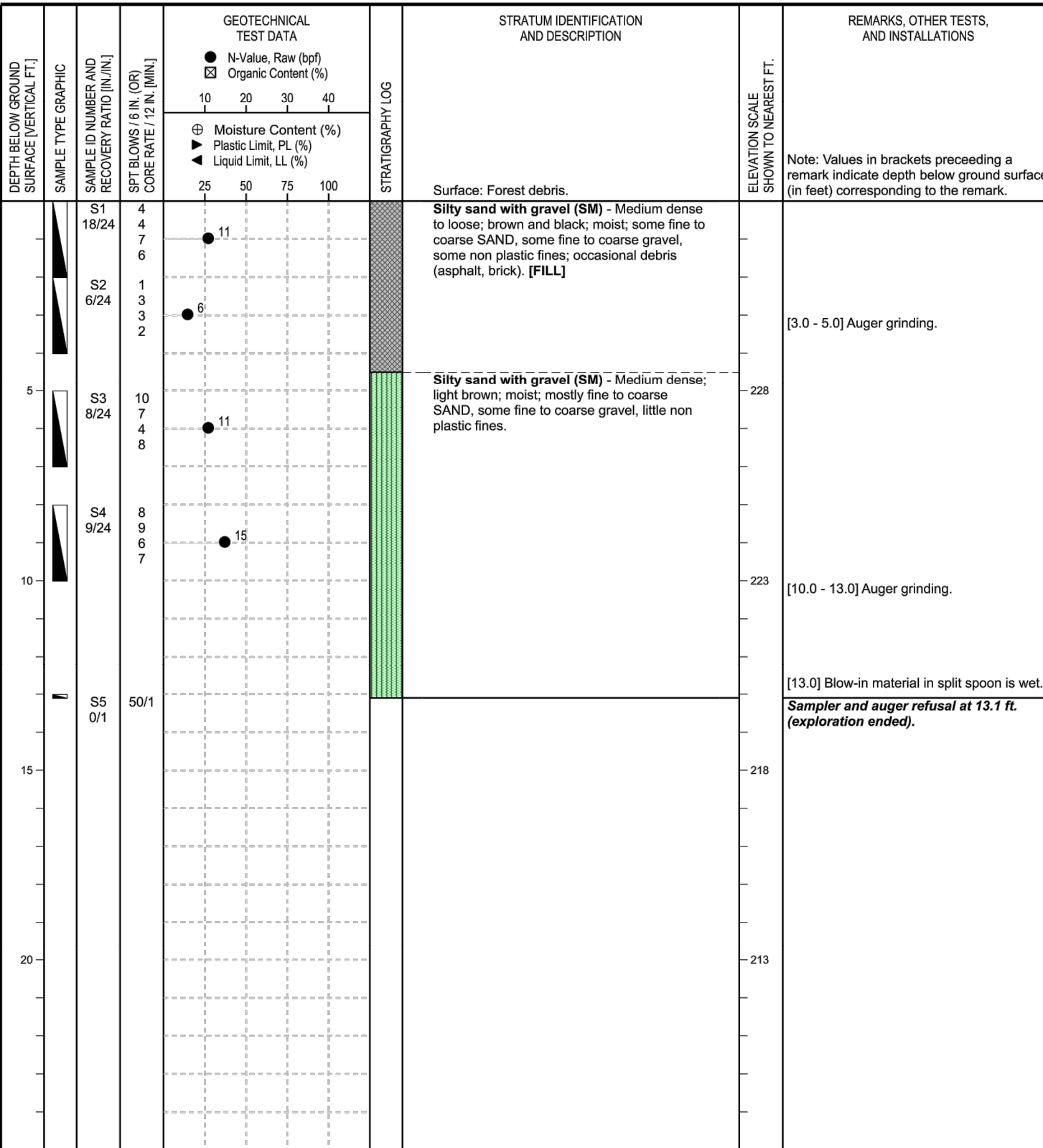
GRID SYSTEM: Not Available





WSE Project: ENG22-1336

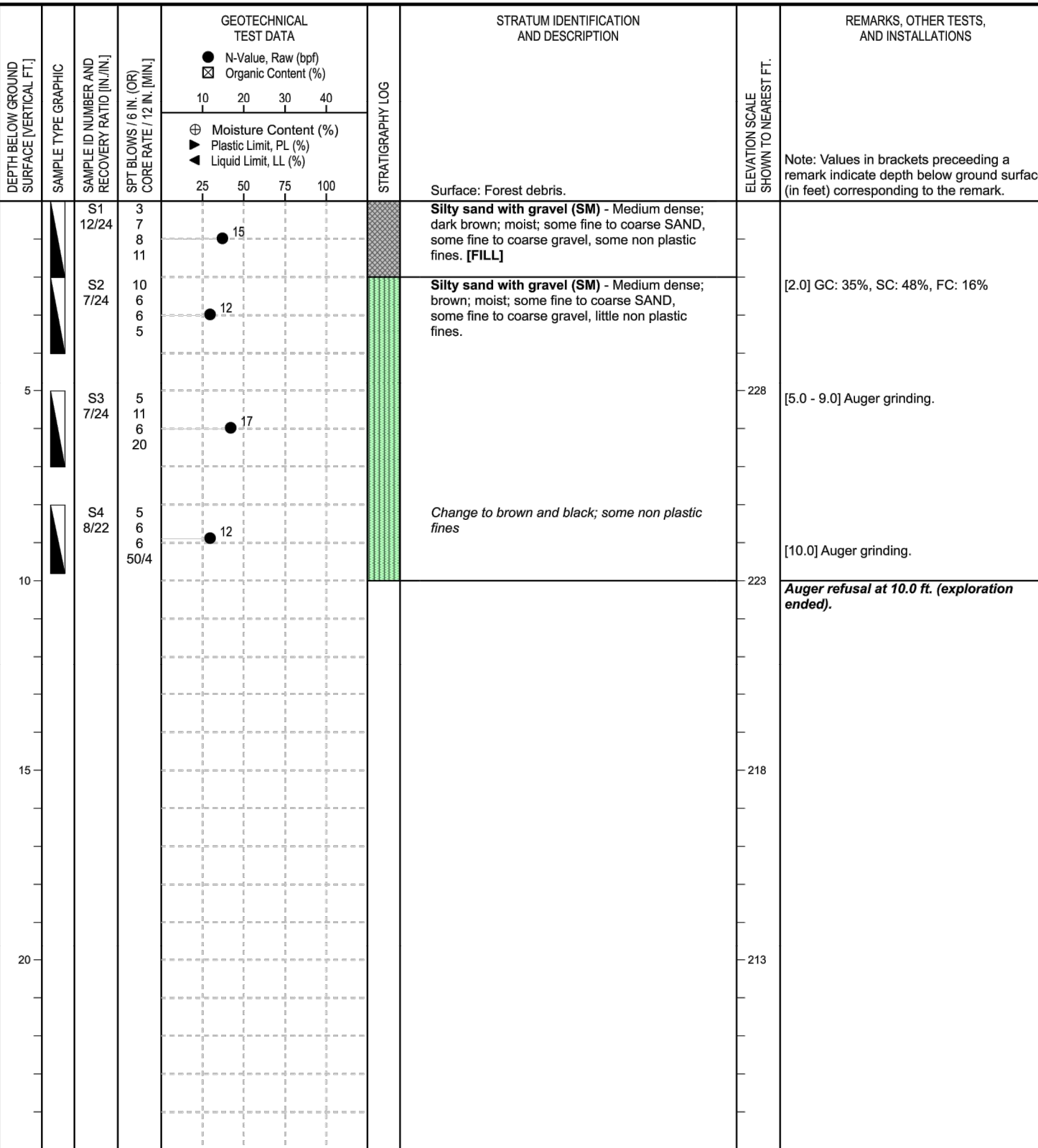
CONTRACTOR: Seaboard Drilling, Inc.	BORING LOCATION: See Site Exploration Map	DATE START: January 26, 2023
FOREMAN: Jeff Nitsch	ADVANCE METHOD: Hollow-Stem Auger Drilling	DATE FINISH: January 26, 2023
LOGGED BY: Geno Mirabella	AUGER DIAMETER: 4-1/4" ID (Stem), 7-5/8" OD (Flights)	GROUND EL: 233.0 ± (NAVD88)
CHECKED BY: S. Spink, P.E.	SUPPORT CASING: N/A	FINAL DEPTH: 13.1 ft. (Refusal)
EQUIPMENT: Diedrich D-50, ATV Mounted	CORING METHOD: N/A	GRID COORDS: N/A
SPT HAMMER: Automatic (140-lb.)	BACKFILL MATERIAL: Drill Cuttings	GRID SYSTEM: Not Available



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WSE Project: ENG22-1336

CONTRACTOR: Seaboard Drilling, Inc.	BORING LOCATION: See Site Exploration Map	DATE START: January 26, 2023
FOREMAN: Jeff Nitsch	ADVANCE METHOD: Hollow-Stem Auger Drilling	DATE FINISH: January 26, 2023
LOGGED BY: Geno Mirabella	AUGER DIAMETER: 4-1/4" ID (Stem), 7-5/8" OD (Flights)	GROUND EL: 233.0 ± (NAVD88)
CHECKED BY: S. Spink, P.E.	SUPPORT CASING: N/A	FINAL DEPTH: 10.0 ft. (Refusal)
EQUIPMENT: Diedrich D-50, ATV Mounted	CORING METHOD: N/A	GRID COORDS: N/A
SPT HAMMER: Automatic (140-lb.)	BACKFILL MATERIAL: Drill Cuttings	GRID SYSTEM: Not Available



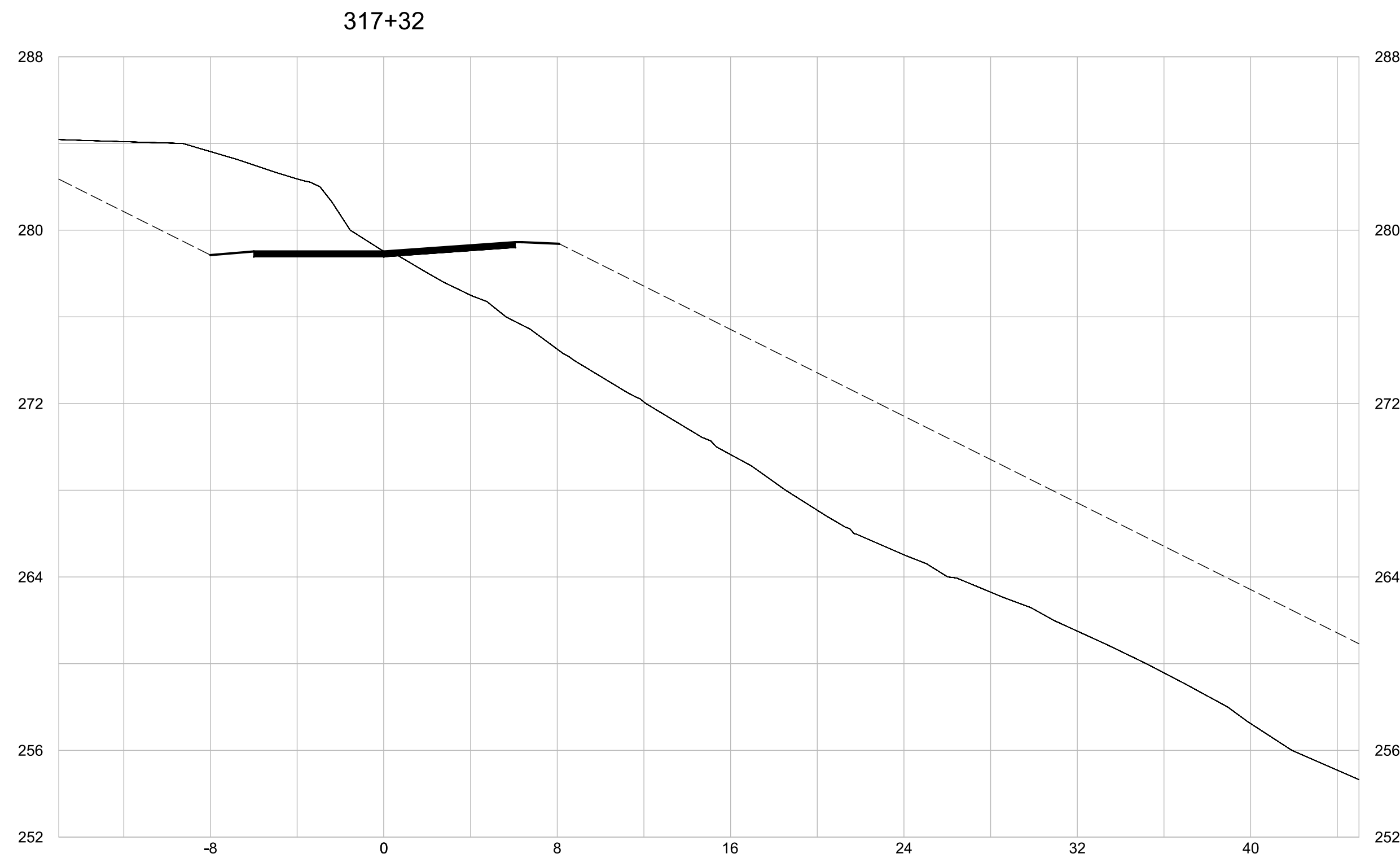
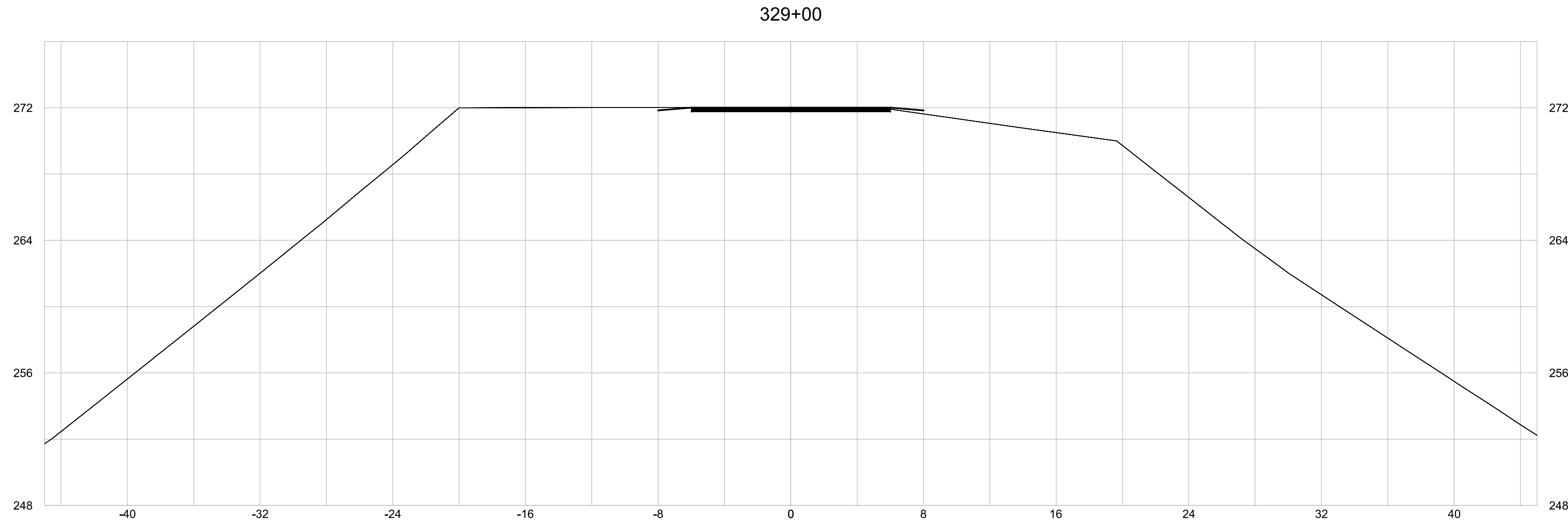
Refer to the attached index sheets for important information about this log including general notes, legends, and guidance on description methods and procedures.

WSE Project: ENG22-1336

CONTRACTOR: Seaboard Drilling, Inc.	BORING LOCATION: See Site Exploration Map	DATE START: January 26, 2023
FOREMAN: Jeff Nitsch	ADVANCE METHOD: Hollow-Stem Auger Drilling	



\\w031\land\10\Projects\22\Putnam CT\Main - 712 Brook St\11-126\2022\w031\proj\p01\p01.dwg




**PROFILE**

0 2.5' 5' 10'

HORIZONTAL SCALE: 1"=5'

VERTICAL SCALE: 1"=5'

Project:  
TAP PROJECT NO. 111-126



AIR LINE TRAIL

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Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ---

Drawing Title:

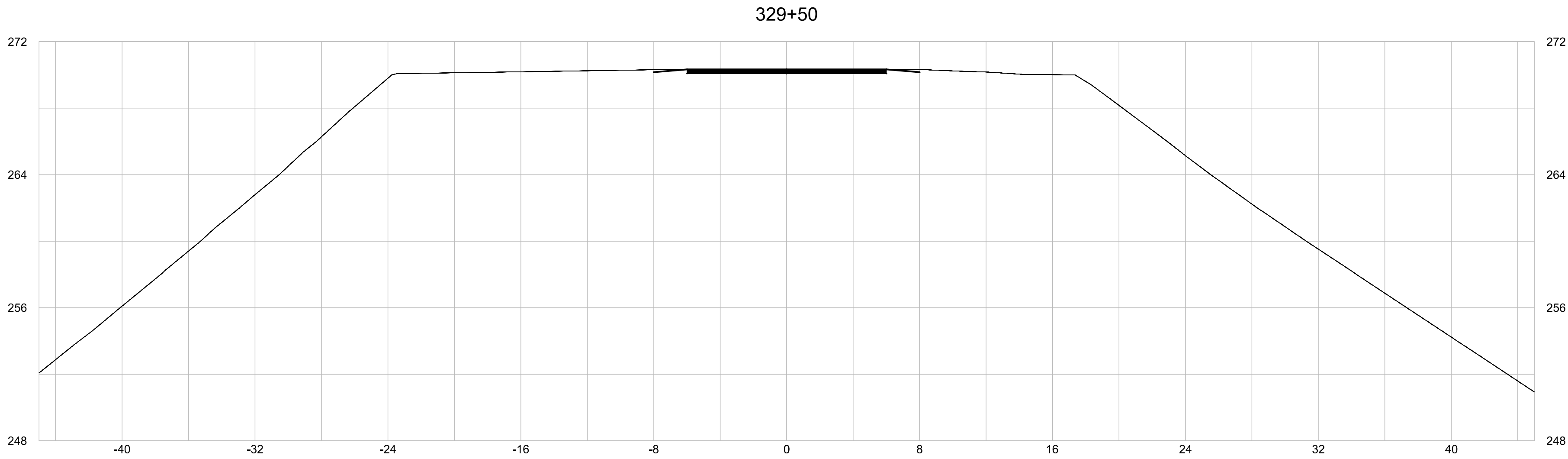
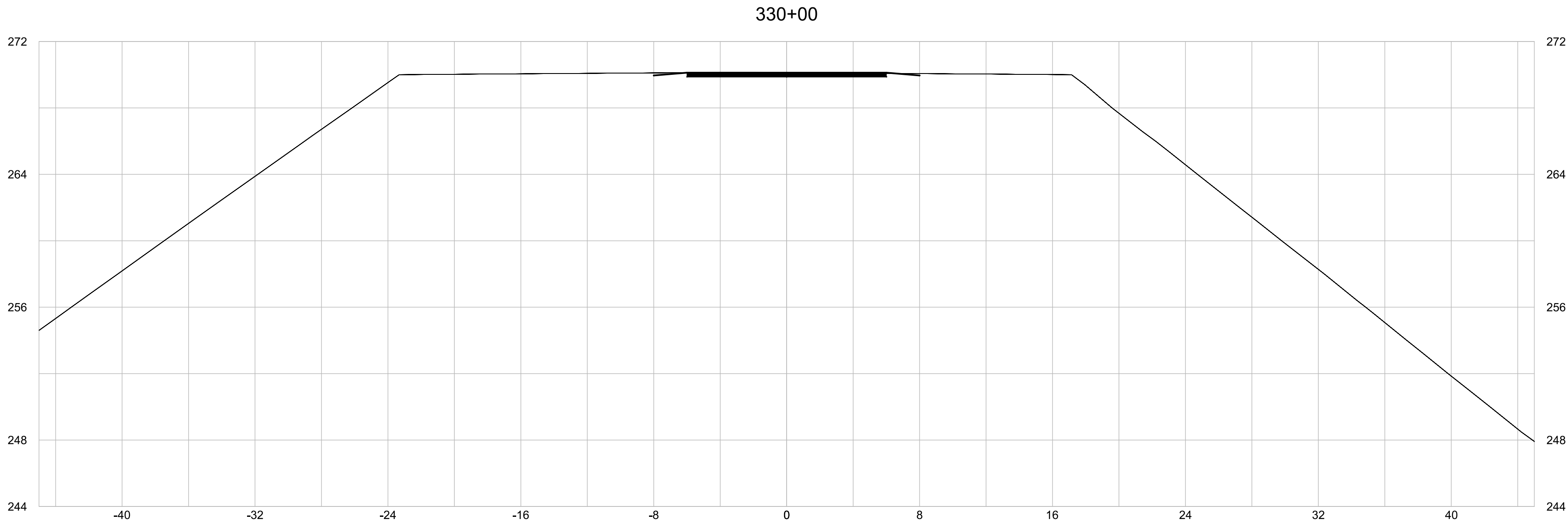
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Sheet Number:

**L801**



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**PROFILE**

0 2.5' 5' 10'

HORIZONTAL SCALE: 1"=5'

VERTICAL SCALE: 1"=5'

Project:  
TAP PROJECT NO. 111-126

AIR LINE TRAIL

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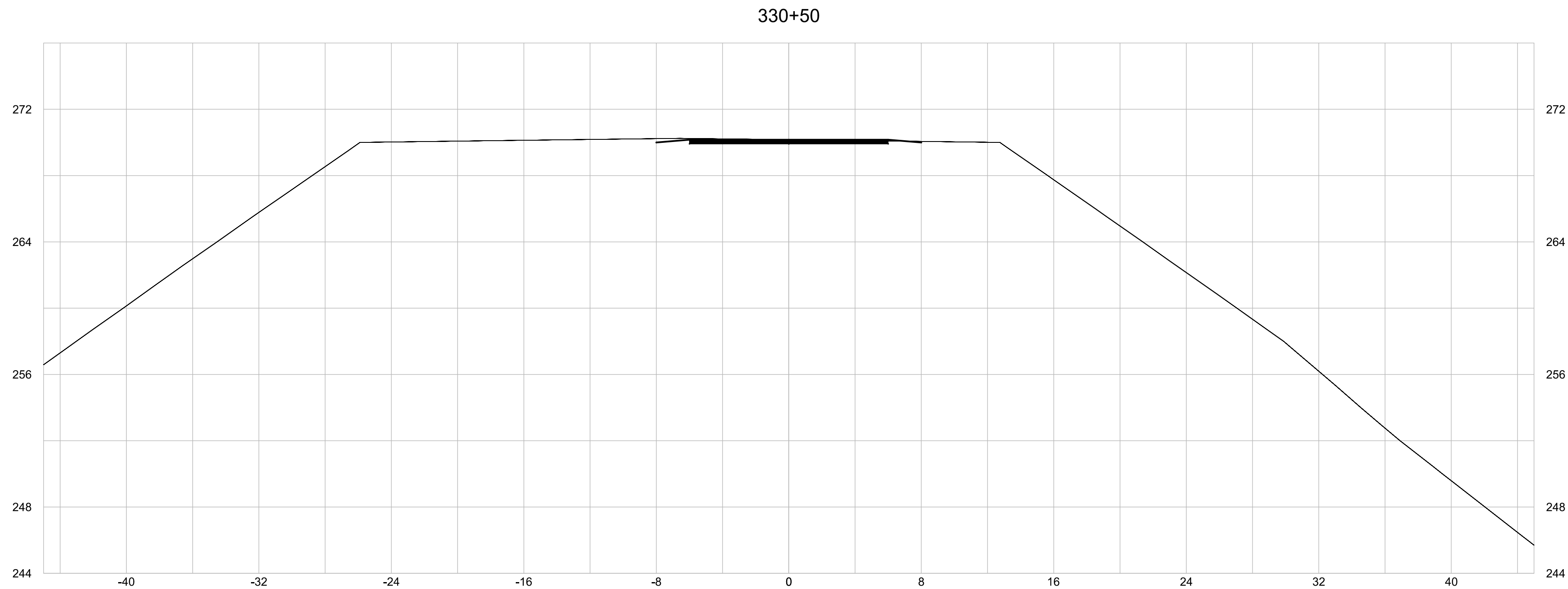
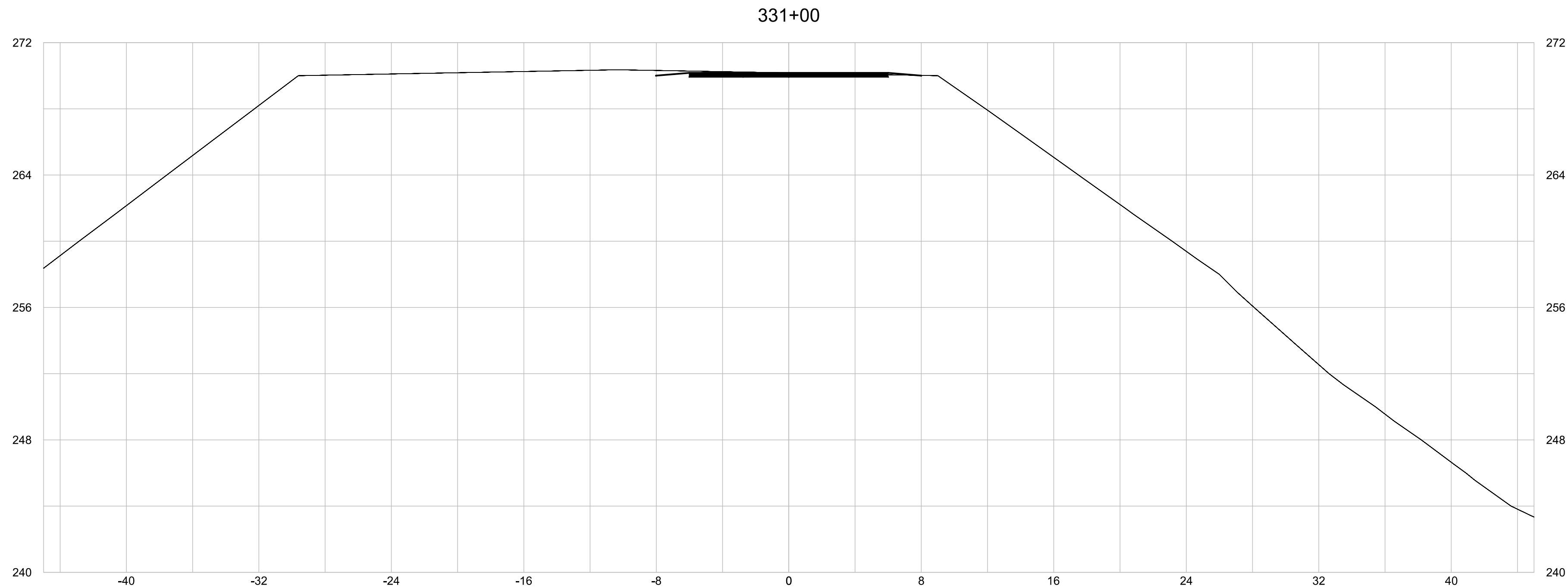
Date: August 28, 2023  
Drawn By: AMN  
Reviewed By: RC  
Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ---

Drawing Title:  
**EXISTING PROFILE**

Sheet Number:  
**L802**



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**PROFILE**

0 2.5' 5' 10'

HORIZONTAL SCALE: 1"=5'

VERTICAL SCALE: 1"=5'

Project:  
TAP PROJECT NO. 111-126

AIR LINE TRAIL

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W&S File No.: ---

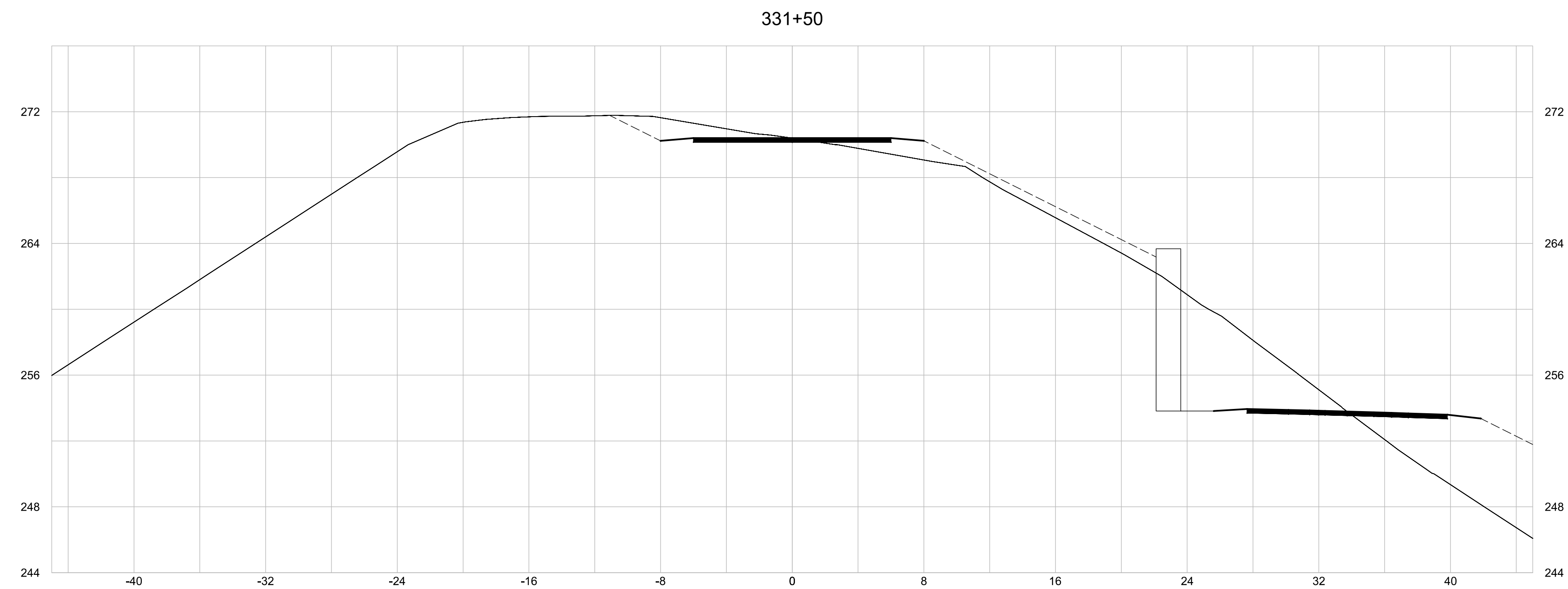
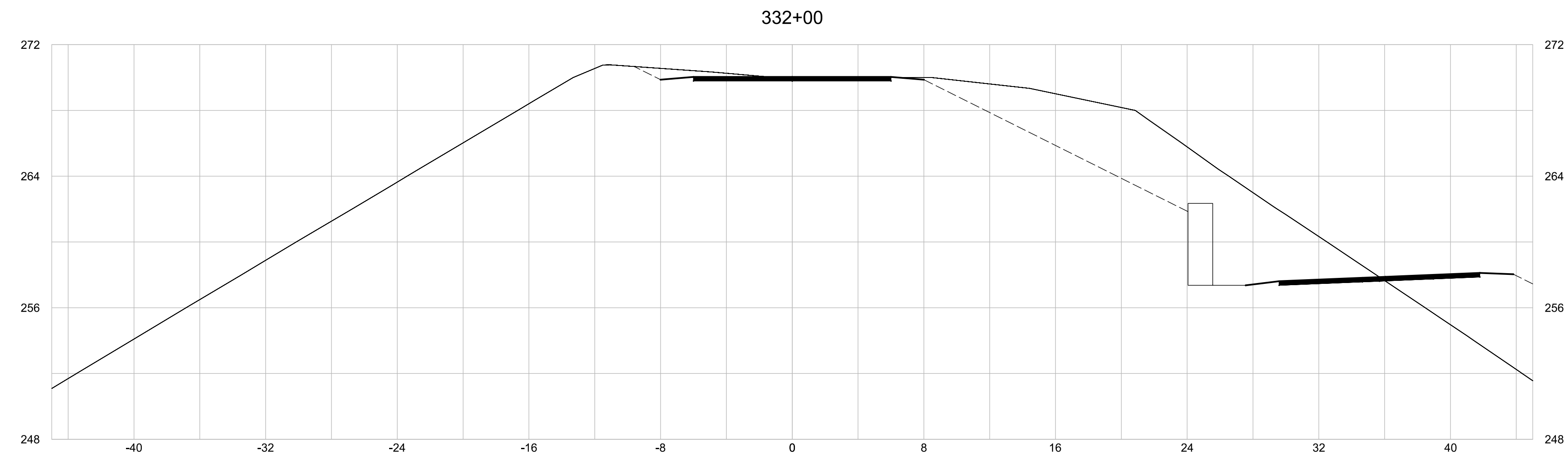
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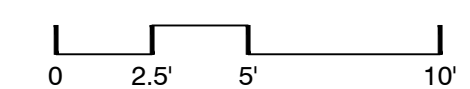
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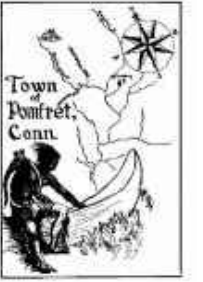


## PROFILE



HORIZONTAL SCALE: 1"=5'  
VERTICAL SCALE: 1"=5'

Project: TAP PROJECT NO. 111-126



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Approved By:	MJJ

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W&S File No.: ----

Drawing Title:

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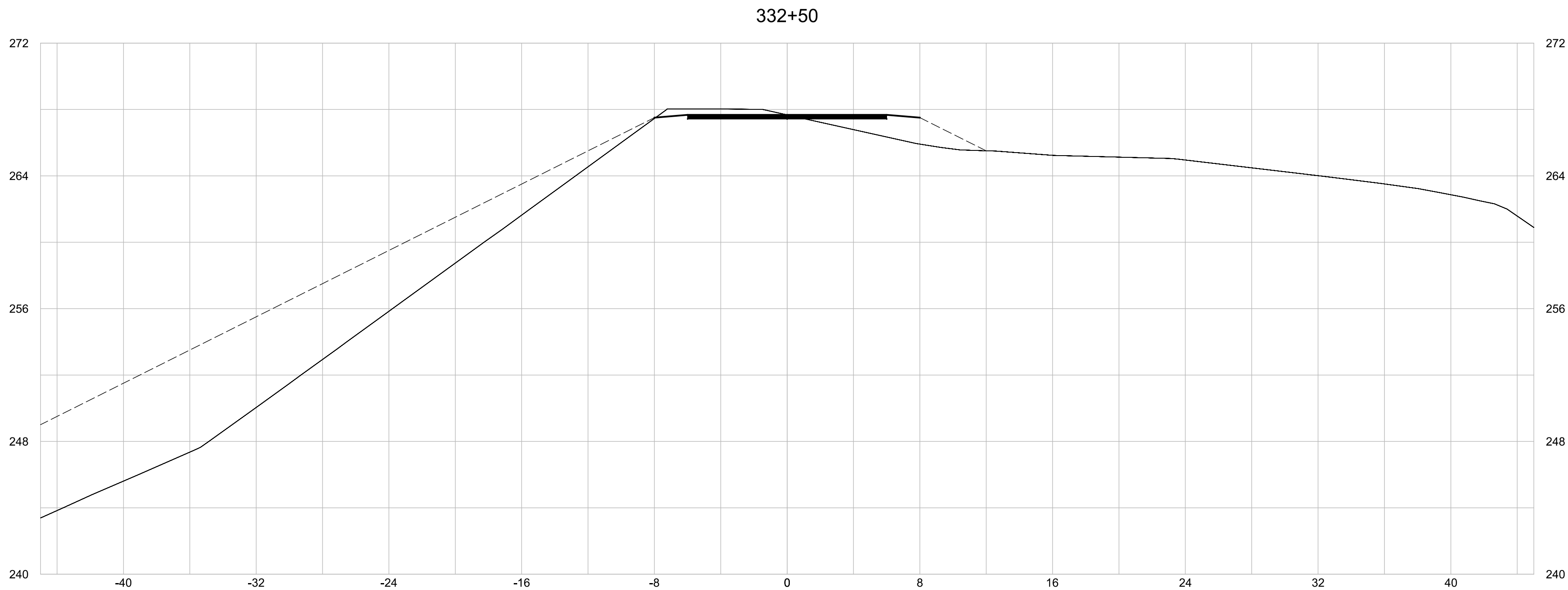
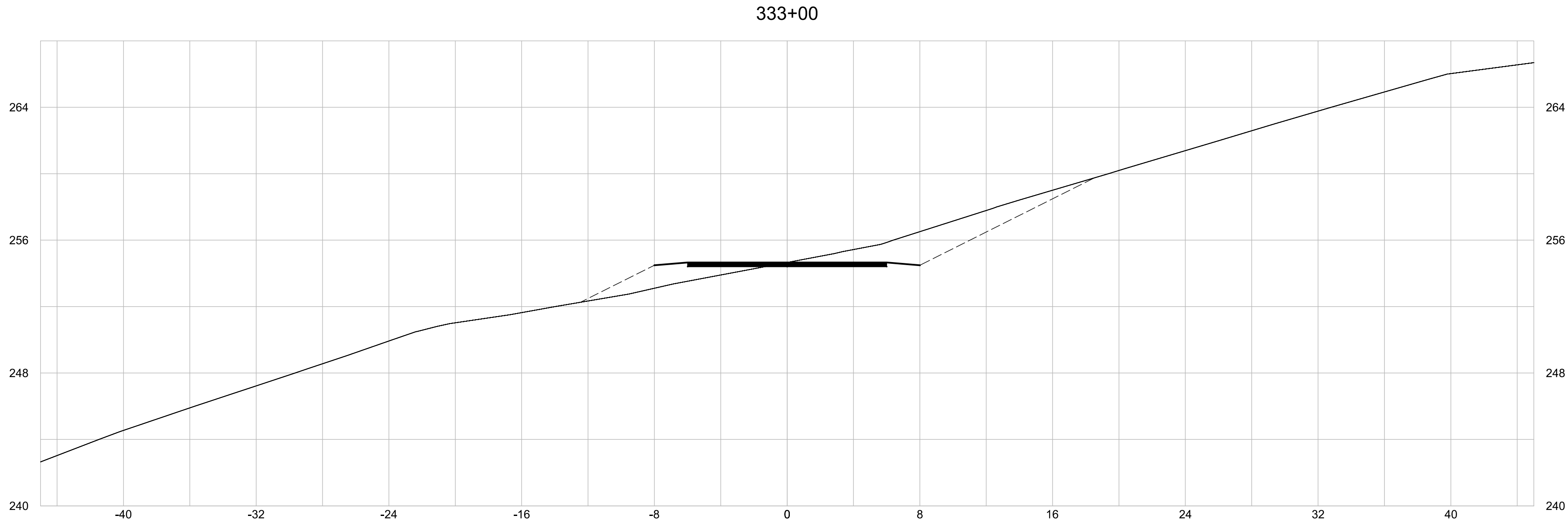
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**PROFILE**

0 2.5' 5' 10'

HORIZONTAL SCALE: 1"=5'

VERTICAL SCALE: 1"=5'

Project:  
TAP PROJECT NO. 111-126

AIR LINE TRAIL

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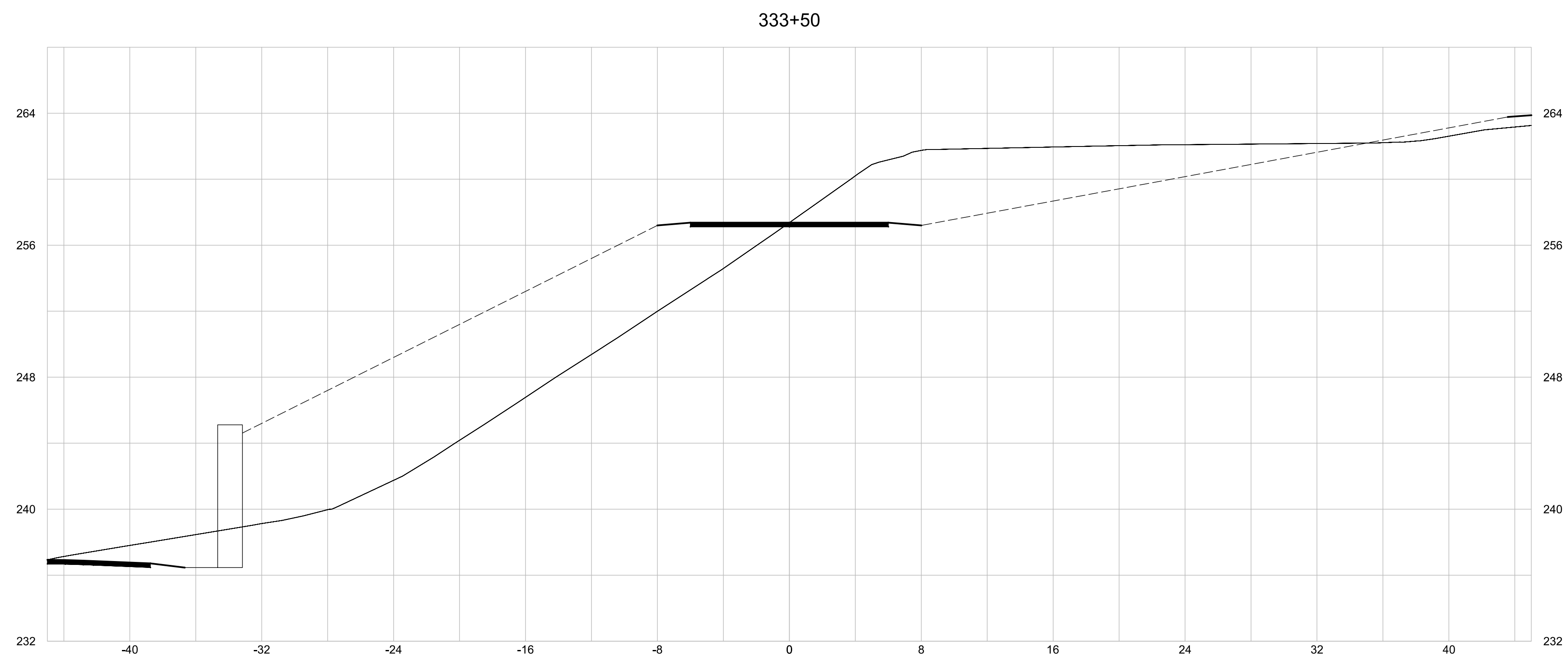
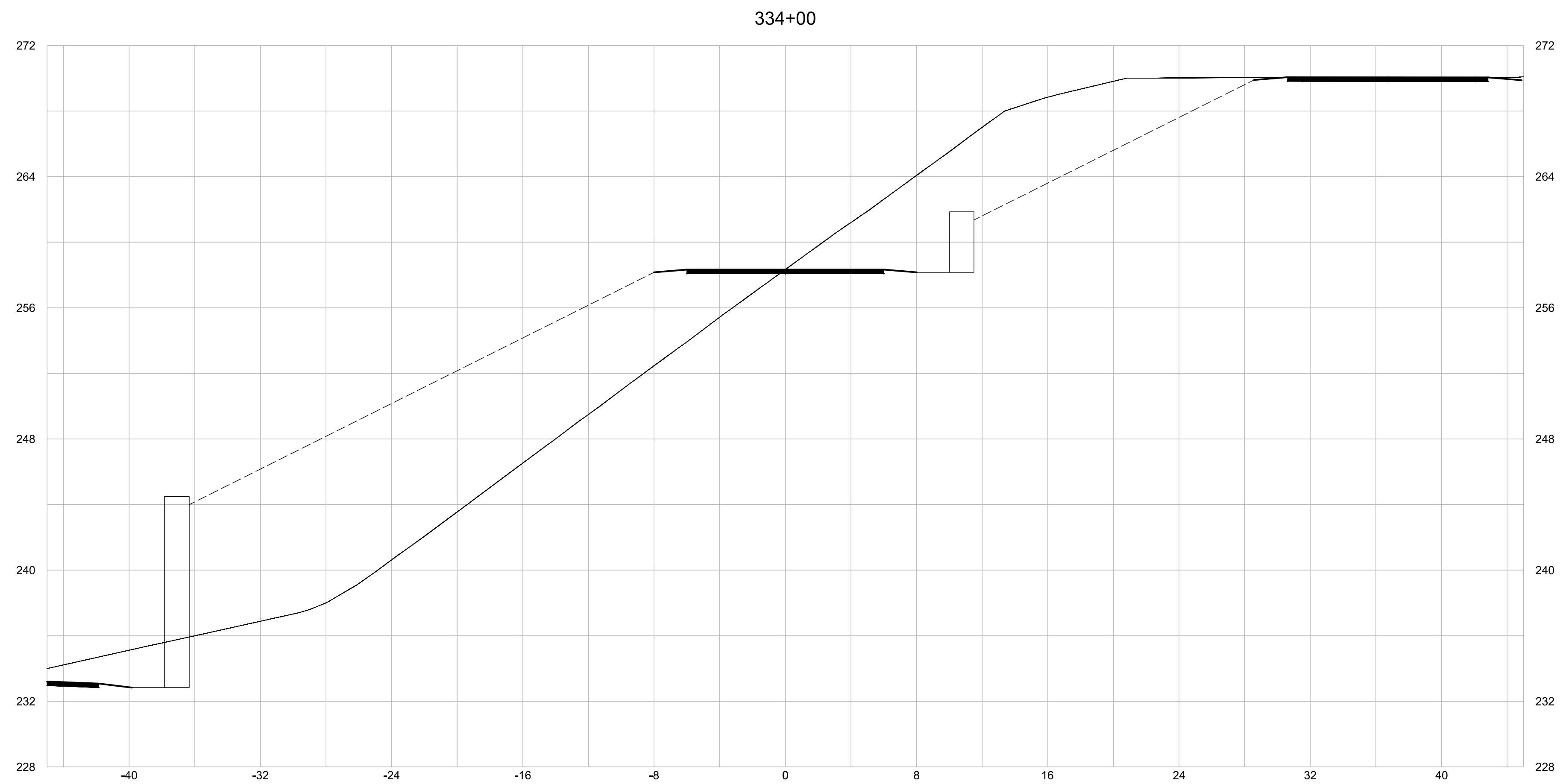
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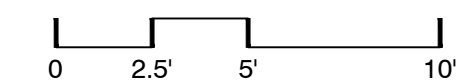
Sheet Number:

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## PROFILE



HORIZONTAL SCALE: 1"=5'  
VERTICAL SCALE: 1"=5'

Project: TAP PROJECT NO. 111-126



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Approved By:	MJJ

W&S Project No.: ENG22-1336  
W&S File No.: ----

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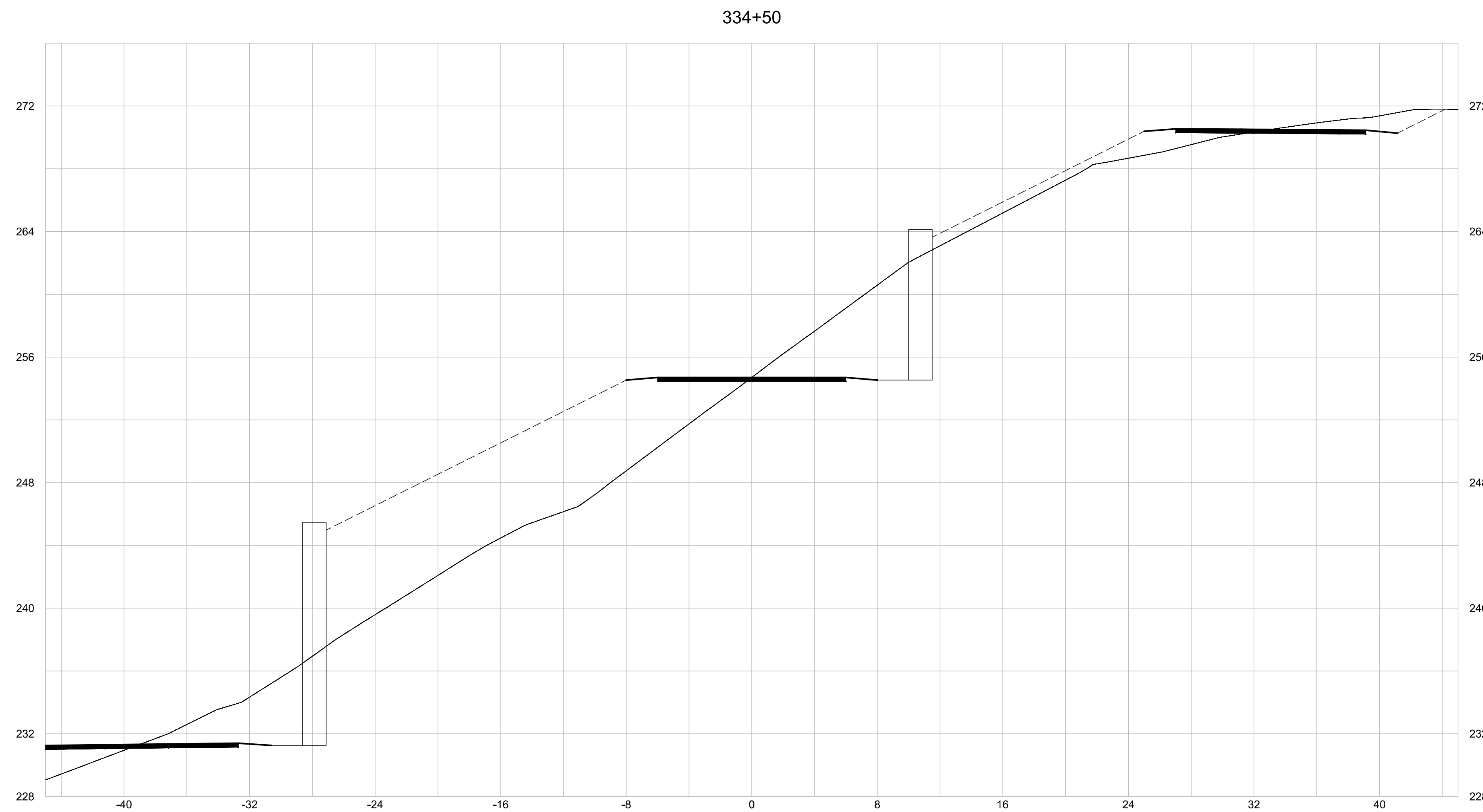
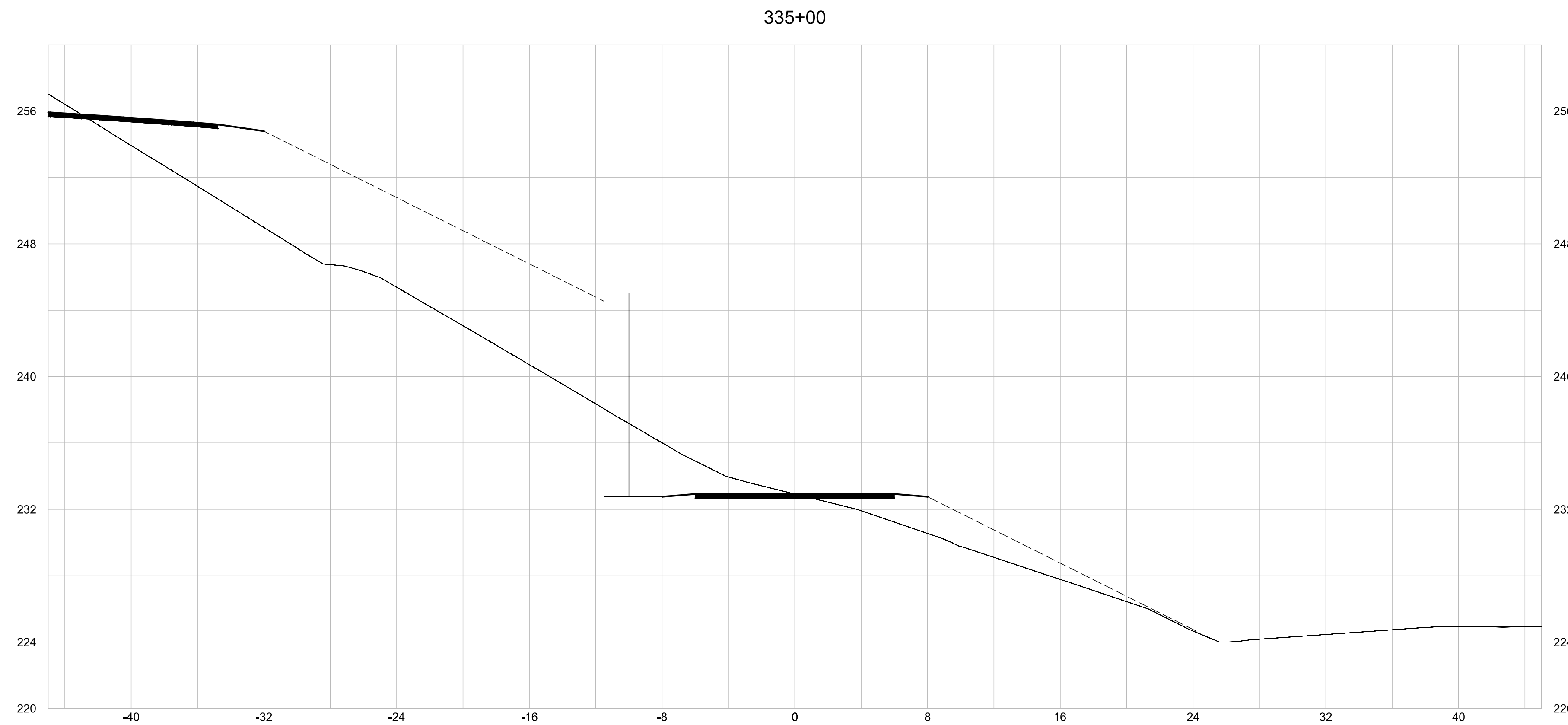
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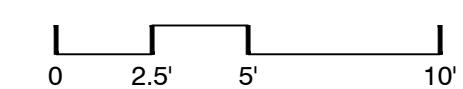
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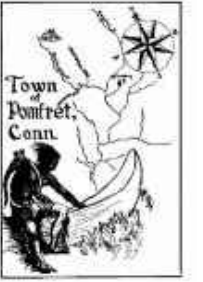


## PROFILE



HORIZONTAL SCALE: 1"=5'  
VERTICAL SCALE: 1"=5'

Project: TAP PROJECT NO. 111-126



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## EXISTING PROFILE

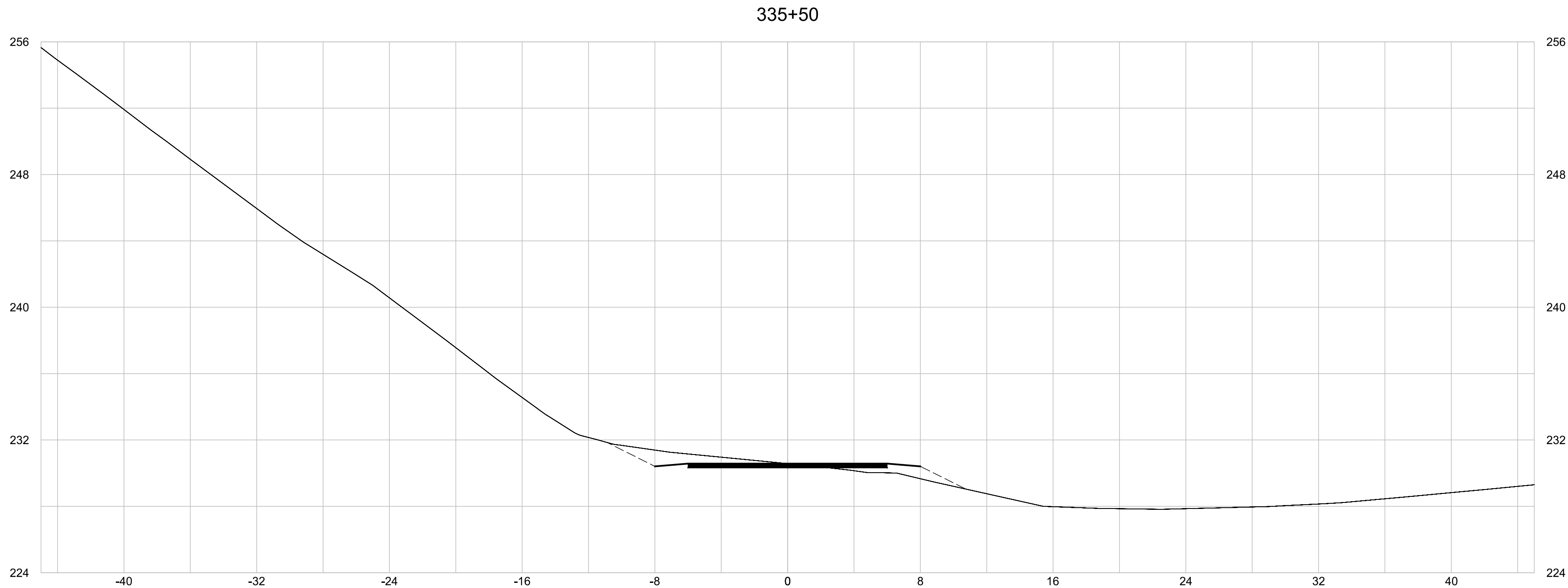
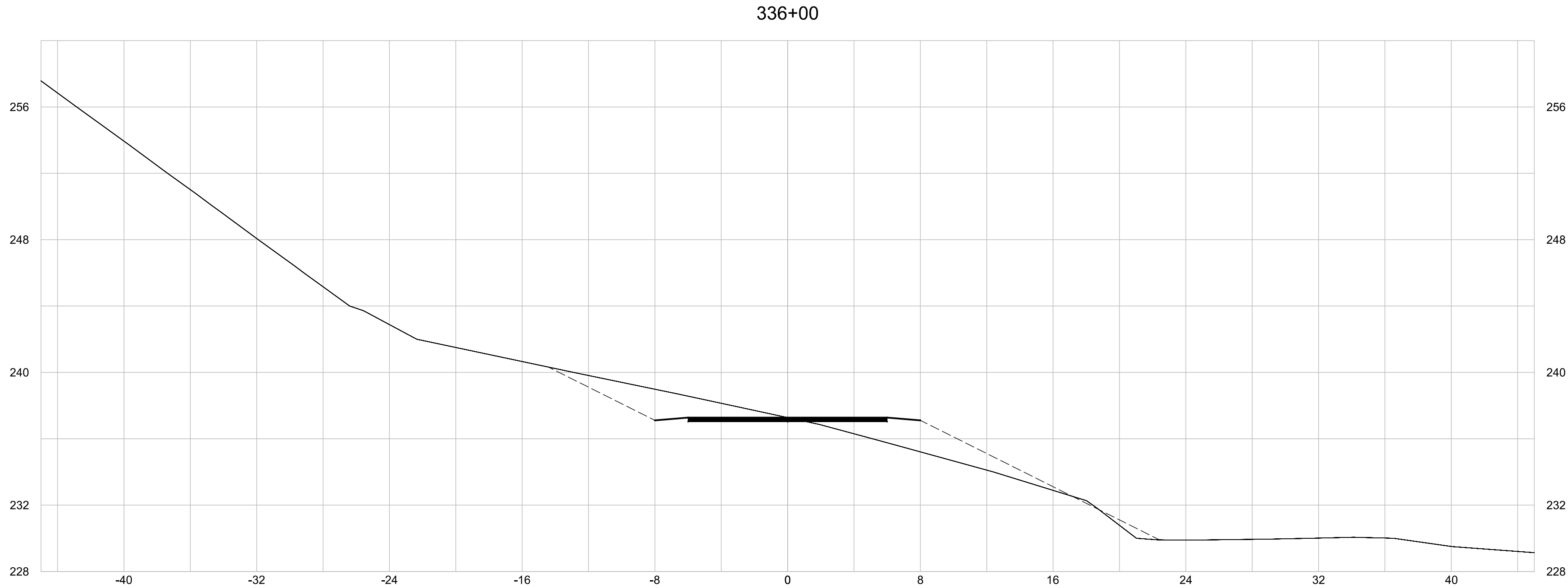
Sheet Number:

# L807

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**PROFILE**

0 2.5' 5' 10'

HORIZONTAL SCALE: 1"=5'

VERTICAL SCALE: 1"=5'

Project:  
TAP PROJECT NO. 111-126

AIR LINE TRAIL

TOWNS OF PUTNAM AND  
POMFRET, CT

**Weston & Sampson**

Weston & Sampson Engineers, Inc.  
712 Brook Street  
Rocky Hill, CT 06067  
978.532.1900 800.SAMPSON  
www.westonandsampson.com

Consultants:


Revisions:

No.	Date	Description

COA:

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Date: August 28, 2023  
Drawn By: AMN  
Reviewed By: RC  
Approved By: MJJ  
W&S Project No.: ENG22-1336  
W&S File No.: ---

Drawing Title:

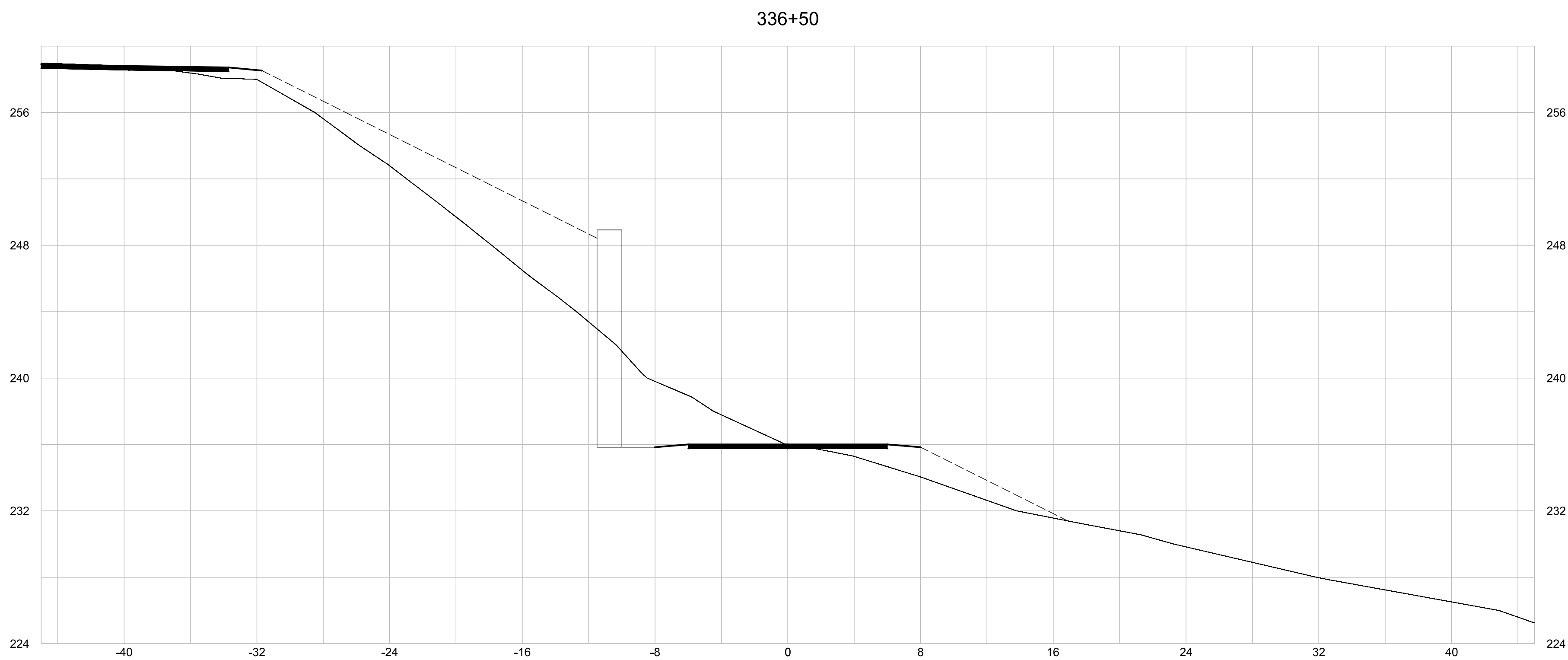
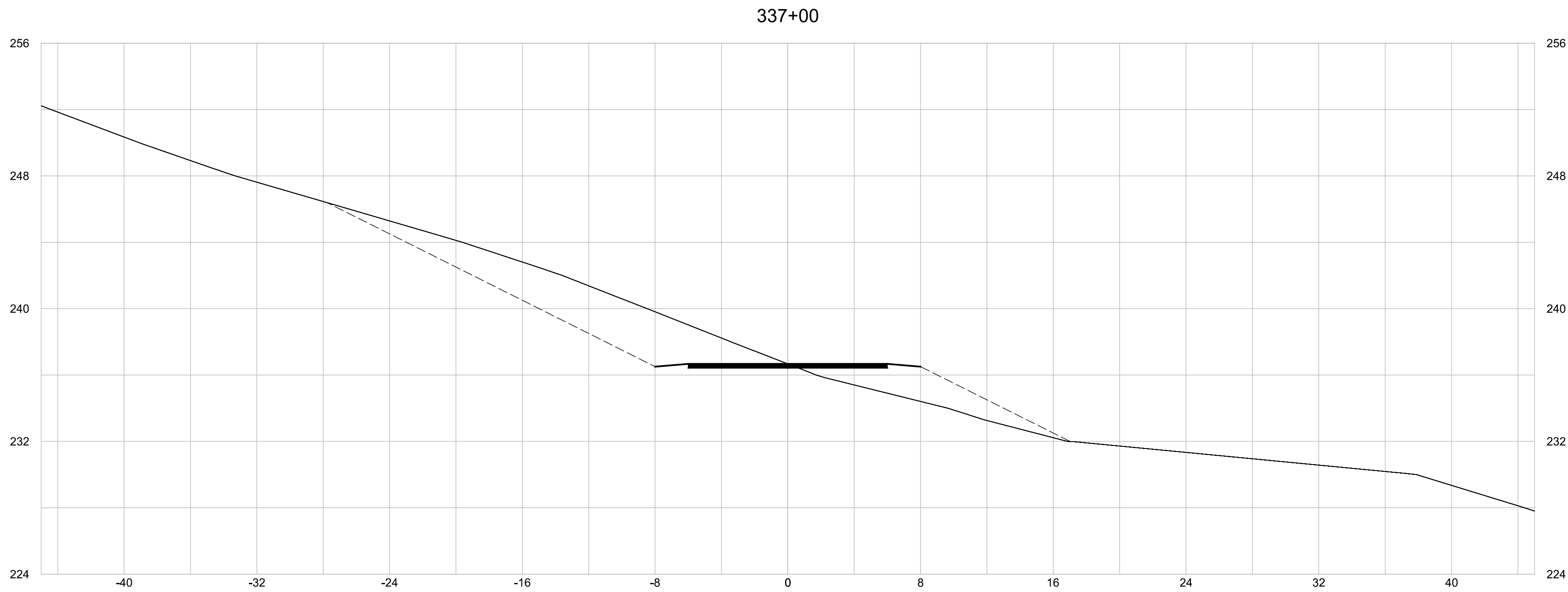
**EXISTING PROFILE**

Sheet Number:

**L808**



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**PROFILE**

0 2.5' 5' 10'

HORIZONTAL SCALE: 1"=5'

VERTICAL SCALE: 1"=5'

Project:  
TAP PROJECT NO. 111-126

AIR LINE TRAIL

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